

ORDINARY COUNCIL MEETING

AGENDA

25 MARCH 2015

at 4:00 PM

COPACC Meeting Rooms

Next Meeting:
22 April 2015 at 4.00pm
COPACC Meeting Rooms, Colac



Our Vision

Council will work together with our community to create a sustainable, vibrant future.

Our Mission

Council will work in partnership with our community and other organisations to provide:

- Effective leadership, governance and financial accountability
- Affordable and effective services
- An advocacy and engagement approach to sustainably grow our community

Our Values

Council will achieve its Vision and Mission by acting with:

- Respect
- Integrity
- Goodwill
- Honesty
- Trust

Our Strategic Direction

The four pillars of our Council Plan indicate our key strategic direction for 2013-2017.

An underlying principle in the development of the Council Plan was to more effectively integrate service delivery.

Pillar 1: Good Governance

Pillar 2: A Planned Future

Pillar 3: A Place to Live and Grow

Pillar 4: A Healthy Community and Environment

Our Councillors

Cr Frank Buchanan (Mayor), Cr Brian Crook (Deputy Mayor), Cr Michael Delahunty, Cr Stephen Hart, Cr Lyn Russell, Cr Chris Smith, Cr Terry Woodcroft.



Ordinary Meeting of Council

Welcome

Welcome to this Meeting of the Colac Otway Shire Council

Council Meetings are an important way to ensure that your democratically elected representatives are working for you in a fair and transparent way. They also allow the public to be involved in the decision making process of Council

About this meeting

There are a few things to know about tonight's meeting. The agenda itemizes all the different parts to the meeting. Some of the items are administrative and are required by law. In the agenda you will also find a list of all the items to be discussed this evening.

Each report is written by a Council officer outlining the purpose of the report, all relevant information and a recommendation. Council will consider the report and either accept the recommendation or make amendments to it. All decisions of Council are adopted if they receive a majority vote from the Councillors present at the meeting.

In accordance with Local Law 4, agenda items will be considered as follows:

- The item is introduced by the Mayor and Councillors are invited to ask questions of relevant officers
- A mover and a seconder of a motion is called for and if there is any Councillor who wishes to oppose the motion.
- The mover will then be invited to speak to the motion, followed by the seconder and then, if required, the Councillor who opposed the motion.
- Remaining Councillors will be given the opportunity to speak for or against the motion.
- If any Councillor speaks against the motion, the mover will be given the right of reply.
- There will be no further discussion of the item once the vote has been declared.

Public Question Time

Provision is made at the beginning of the meeting for general question time from members of the public. Matters relating to routine Council works should be taken up with Council's Customer Assist Staff.

Up to thirty minutes may be provided for Question Time. This is at the discretion of the Mayor.

Residents are encouraged to lodge questions in advance so that a more complete response can be given.

Questions can be submitted in writing up until 5.00pm on the Monday prior to each Council meeting. There is also provision for questions to be asked from the gallery. If you would like to ask a question during Question Time, it would be appreciated if you could please fill in the blue "Public Question Time – Council Meetings" form located in the meeting rooms and hand to the COPACC Duty Supervisor.

Questions relating to a topic on the agenda may be taken on notice and responded to after the meeting. Responses to questions taken on notice will be tabled at the following meeting and included in the minutes of that meeting.

Hearing of Submissions

Any person who has made a written submission on an item and requested that she or he be heard in support of that written submission pursuant to section 223 of the *Local Government Act 1989* will be entitled to address Council.

When the relevant item is listed for discussion, the Mayor/Chairperson will call your name and ask you to address the Council. The length of time available to each speaker is five minutes.

Recording of Meetings

All Council and Committee meetings are audio recorded, with the exception of matters identified as confidential items in the Agenda. This includes the public participation sections of the meetings. Audio recordings of meetings are taken to facilitate the preparation of the minutes of open Council and Committee meetings and to ensure their accuracy. In some circumstances a recording will be disclosed to a third party. Those circumstances include, but are not limited to, circumstances, such as where Council is compelled to disclose an audio recording because it is required by law, such as the Freedom of Information Act 1982, or by court order, warrant, or subpoena or to assist in an investigation undertaken by the Ombudsman or the Independent Broad-based Anti-corruption Commission.

Council will not use or disclose the recordings for any other purpose. It is an offence to make an unauthorised recording of the meeting.

COLAC-OTWAY SHIRE COUNCIL MEETING

25 MARCH 2015

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COLAC OTWAY SHIRE COUNCIL ORDINARY MEETING OF COUNCIL

NOTICE is hereby given that the next **ORDINARY COUNCIL MEETING OF THE COLAC-OTWAY SHIRE COUNCIL** will be held in COPACC Meeting Rooms on 25 March 2015 at 4.00pm.

1. OPENING PRAYER

*Almighty God, we seek your
blessing and guidance in our
deliberations on behalf of the
people of the Colac Otway Shire.
Enable this Council's decisions to be
those that contribute to the true
welfare and betterment of our community.*
AMEN

2. APOLOGIES

3. DECLARATION OF INTEREST

Pursuant to Sections 77, 78 and 79 of the Local Government Act 1989, direct and indirect conflict of interest must be declared prior to debate on specific items within the agenda; or in writing to the Chief Executive Officer before the meeting. Declaration of indirect interests must also include the classification of the interest (in circumstances where a Councillor has made a Declaration in writing, the classification of the interest must still be declared at the meeting), ie:

- a) direct financial interest
- b) indirect interest by close association
- c) indirect interest that is an indirect financial interest
- d) indirect interest because of conflicting duties
- e) indirect interest because of receipt of an applicable gift
- f) indirect interest as a consequence of becoming an interested party
- g) indirect interest as a result of impact on residential amenity
- h) conflicting personal interest.

A Councillor who has declared a conflict of interest, must leave the meeting and remain outside the room while the matter is being considered, or any vote is taken.

Councillors are also encouraged to declare circumstances where there may be a perceived conflict of interest.

4. WELCOME & ACKNOWLEDGEMENT OF COUNTRY

Colac Otway Shire acknowledges the traditional custodians and law makers of this land, their elders past and present and welcomes any descendants here today.

Colac Otway Shire encourages community input and participation in Council decisions. Council meetings provide an opportunity for the community to ask Council questions, either verbally at the meeting or in writing.

Please note that Council may not be able to answer some questions at the meeting. These will be answered later.

Council meetings enable Councillors to debate matters prior to decisions being made. I ask that we all behave in a courteous manner.

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Thank you. Now 30 minutes is allowed for question time. Please remember, you must ask a question. If you do not ask a question you will be asked to sit down and the next person will be invited to ask a question. This is not a forum for public debate or statements.

1. Questions received in writing prior to the meeting (subject to attendance and time),
2. Questions from the floor.

5. QUESTION TIME

6. TABLING OF RESPONSES TO QUESTIONS TAKEN ON NOTICE AT PREVIOUS MEETINGS

These responses will not be read out but will be included in the minutes of this meeting.

7. PETITIONS/JOINT LETTERS (if required)

8. CONFIRMATION OF MINUTES

- Ordinary Council Meeting held on the 25/02/15.

Recommendation

That Council confirm the above minutes.

As per Governance Local Law No 4, clause 34:

34. Objection to Confirmation of Minutes

- (1) If a Councillor is dissatisfied with the accuracy of the minutes, then he or she must:
 - (a) state the item or items with which he or she is dissatisfied; and
 - (b) propose a motion clearly outlining the alternative wording to amend the minutes.
- (2) Except where sub-clause (1) applies, no vote shall be recorded against a motion to adopt the minutes.

OM152503-1 CEO'S PROGRESS REPORT TO COUNCIL

AUTHOR:	Rhonda Deigan	ENDORSED:	Sue Wilkinson
DEPARTMENT:	Executive	FILE REF:	F11/3291

EXECUTIVE

Meetings and events attended by the CEO and/or Councillors during the past month included:

- G21 Board meeting at the Borough of Queenscliff
- Meeting with the Director Barwon South-Western Region (Health) and Manager Health Information and Health Programs
- Community Conversation at Gellibrand, Cressy, Wye River and Kennett River
- Strategic Planning Workshop with Councillors and Executive Management Team
- International Women's Week Celebrations
- G21 Board Delegation to Canberra
- Youth Council Induction Ceremony
- Beeac Red Cross Branch 100th Annual Meeting.

Colac Otway Shire Youth Council

An Induction Ceremony for the eight members of the 2015 Youth Council was held at COPACC on Monday 16 March 2015. The eight inspiring young citizens included 3 students from Colac Secondary College, 3 students from Trinity College Colac, and 2 former Colac Secondary College and Colac Specialist School students who now work within Colac Otway Shire. The Induction Ceremony also recognised the contribution from last year's Youth Councillors and Mentors.

The Youth Council has now convened two meetings discussing project plans for the year and learning Council meeting procedures. Youth Councillors will also attend an overnight orientation trip to Bimbi Park, Cape Otway on 30 and 31 March which will include team building activities and discussions about Youth Council's focus for 2015.

CORPORATE & COMMUNITY SERVICES

HEALTH & COMMUNITY SERVICES

Children & Family Services

Family Day Care Focus Group Meeting

A focus group meeting was held with Family Day Care Educators which included Council's Environmental Health Officer presenting to the Educators about Food Safety and Hygiene. The Educators found the refresher helpful as it reaffirmed the importance of what they are teaching children and parents around correct hygiene practices.

Maternal and Child Health (M&CH)

M&CH nurses have had a busy month with a higher than average 26 birth notifications and are ensuring all the new families are settling in well. Our participation rates in February increased for the older age group children at 18mths, 2yrs and 3 1/2 yrs. There was a

doubling of the number of additional visits for mums and babies so we are looking forward to being able to use our new consulting space with the arrival of a new portable office/consulting room at the rear of the Queen Street, Colac centre.

Statistics – February 2015

- 26 infants enrolled from birth notifications.

Key Ages and Stages Consultations					
Home visits: 16					
2wks	17	4wks	16	8wks	14
4mths	14	8mths	16	12mths	16
18mths	25	2yrs	19	3.5yrs	21

Other services provided included:

- 113 additional consultations
- 30 phone consultations
- 3 opportunistic immunisations
- 20 Child and Family Referrals with the main issues being vision, auditory and maternal physical & emotional health.
- 52 Child and Family Counselling sessions provided. The majority of counselling was for nutrition and maternal emotional health.
- New Parents Groups - 4 sessions in Colac.
- This month we commenced with 47 cases open for our vulnerable families requiring Enhanced Home Visiting service (Vulnerable and at risk families). We opened 6 new cases and closed 10.

RECREATION ARTS AND CULTURE

EVENTS

2016 Colac Otway Shire Australia Day Celebrations

The Expression of Interest process to host 2016 Colac Otway Shire Australia Day celebrations opened on Friday 27 February. Colac Otway groups that have the passion and drive to plan, organise and execute the Australia Day event, in partnership with the Colac Otway Shire events team, are being encouraged to submit an Expression of Interest for the 2016 Celebrations. The Australia Day event provides the opportunity for the successful host to showcase their town, local venues, artists and performers to a diverse range of residents and visitors. Submissions close at 5:00pm on Friday 10 April 2015.

Calendar of Events Project

The Colac Otway Shire Calendar of Events offers a free advertising opportunity for not-for-profit community events held within the Colac Otway Shire. The Calendar of Events Flyer is printed quarterly and is accompanied by free newspaper advertising.

The 2015 Autumn Calendar of Events, promoting 11 Colac Otway Shire events and four community markets, is now available to the public. In late February 4,000 copies of the Colac Otway Shire Autumn Calendar were distributed throughout the Shire.

Upcoming events

Great Ocean & Otway Classic Ride – 18 April 2015

Annual cycling event for 3,000 riders from Torquay to Lorne, along the Cape Otway Road and then back to Torquay along the Great Ocean Road. This year's event includes an extension ride for 300 riders to continue through to Forrest, Skenes Creek and back along the Great Ocean Road to Torquay. No road closures will occur as part of this ride. Community information sessions will be held at the end of March 2015 at Forrest and Wye River.

Anzac Day – 25 April 2015

Planning is underway for dawn services in Colac, Apollo Bay and Beeac and a morning service in Beech Forrest. Council is producing advertising material that will be used during the month of April to promote the events and projects that were funded through the Anzac Centenary Grants.

RECREATION

Cororooke Open Space Landscape Master Plan

The development of a landscape master plan for the new open space at Cororooke is progressing well, with significant community input into the process to date. A draft plan has been prepared based on information gathered via a community survey and a community 'open house'. The draft plan has since been distributed to the community via a number of means and discussed in length at a recent community meeting. The feedback on the draft plan has been overwhelmingly positive. It is anticipated that the draft plan will be presented to Council in coming months.

2015-2016 Community Funding Program

The 2015-2016 Festival and Events Support Scheme and the 2015-2016 Council Community Funding Program (which includes Recreation facilities, Community Projects and COPACC Hire Assistance) opened on 20 March 2015. Two information sessions will be held in Colac (13 April) and Apollo Bay (15 April) to provide potential applicants with further details about the program.

Application forms and funding guidelines for both of these programs will be available at Customer Service Centres at Colac and Apollo Bay and on Council's website www.colacotway.vic.gov.au from 20 March 2015.

COPACC

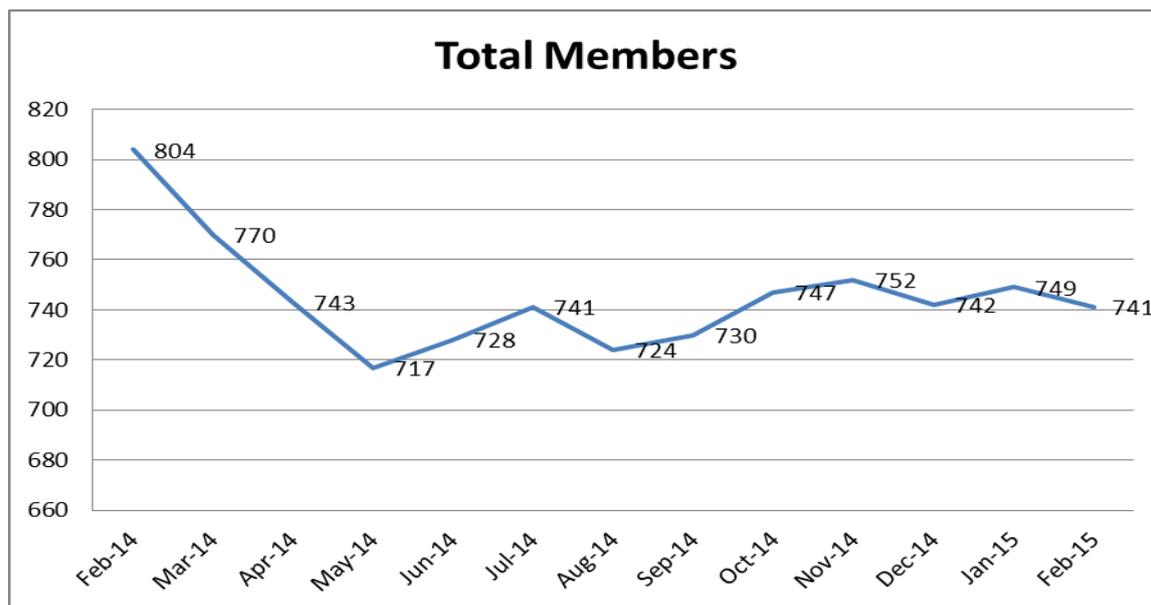
COPACC made a successful start to the year with almost 300 people attending the Colac Herald Theatre Season launch on 21 February 2015.

The Centre's first Morning Music concert for the year, Hot August Night & Me, a Neil Diamond tribute show, attracted an audience of close to 300.

BLUEWATER FITNESS CENTRE

Youth Club operations

Bluewater current membership total is 741 a slight drop from January 2015 (see graph below). After a decrease in early 2014 membership numbers have remained steady over the past 9 months.



Mission 8 of Bluewater Boot Camp has now finished, with 30 people participating. Due to the reduced morning light as we head towards winter, the next Bootcamp will take place in the new facility.

In place of Boot Camp we have commenced a new program called Group Training. The Group Training program will involve smaller groups of up to 10 people with one instructor undertaking similar Boot Camp style workouts.

Apollo Bay Pool

The 5 week condensed Learn to Swim program at the Apollo Bay Pool has now been completed. There were 80 places available in the program and a total of 65 enrolments, equating to 81% capacity. The feedback from the community was very positive. The program has been run for two seasons and looks to be sustainable.

The Apollo Bay Pool closed for the season following the Labour Day weekend (9 March 2015).

INFRASTRUCTURE & SERVICES

SUSTAINABLE ASSETS

ACTIONS	STATUS	% COMPLETION	PROGRESS COMMENTS
Work with the Finance Unit to complete Stage 2 of the Authority Asset Management Module implementation	In Progress	30%	The first scheduled visit for Stage 2 has been completed. This visit covered the set up phase for Works and Maintenance Management. The next visit has been scheduled for the week beginning 27 April 2015. This will allow Council Officers to complete data acquisition and verification. The final two visits, which relate to the implementation of the Strategic Asset Management (SAM) Module, have been scheduled for mid-August and early September 2015.
Monitor the performance and condition of Council's various asset types through programmed inspections and data collection.	In Progress	50%	The Level 2 Bridge Inspections have been completed and Council expects to receive the contractor's report by the end of March 2015. Defects were identified in six structures which warrant further investigation. Council is awaiting a quotation from the contractor for Level 3 inspections of these structures. Work has commenced on developing the quotation documentation for the next round of sealed road condition surveys. Data collected from the ongoing asset condition audits are used to develop long term maintenance and renewal programs.
Implement the asset renewal and maintenance programs.	In Progress	67%	The 2014/15 asset renewal and maintenance programmes are in progress at various stages of planning and implementation. The reconstruction of Richmond Street, Colac has been awarded to R Slater & Sons with work due to commence in early April 2015. Tenders for the reconstruction of a short section of Costin Street between Seymour Crescent and Montrose Avenue are presently being evaluated.
Bridge Reconstruction Program.	In Progress	15%	The rehabilitation of both the Sand Road bridge and Cape Otway Road bridge are presently at tender stage. Preliminary investigations for the rehabilitation of F Pearces bridge (Apollo Bay) and Watsons Access bridge (Glenaire) have commenced.

Complete specific incident inspections to identify and address safety/risk issues associated with elements of Council's road and footpath networks; including rail crossings and in response to reported incidents.	In Progress	67%	Incident inspections are completed as required in line with Council's Road Management Plan.
Manage building maintenance programme to address programmed and reactive maintenance needs.	In Progress	67%	Routine building maintenance activities have been completed over the past month to maintain functionality and ensure user safety.
Footpath Renewal Program.	In Progress	63%	Renewal works are in progress. During the month of February footpaths were replaced in William Street, Quamby Ave, Bruce St, Polwarth South Street (partial), Wallace St (partial) all of which are located in Colac. Also completed was the replacement of a 100m section of footpath along the Great Ocean Road in Apollo Bay.
Complete road and footpath network inspections to meet frequencies prescribed by Council's Road Management Plan (RMP).	In Progress	67%	All inspections are conducted in accordance with Council's Road Management Plan. Inspection of footpaths in the Colac Level 1 Area, which incorporates the CBD and other areas which generate significant foot traffic, have been completed. Routine inspections of Access Roads in Colac and the Coastal area townships of: Wye River; Kennett River; Skenes Creek; Marengo; and Apollo Bay have also been completed.
Building Renewal Program.	In Progress	67%	<ul style="list-style-type: none"> - Waterproofing of Colac Otway Shire Radio Communication housing tanks has been completed. - Quotations are currently being reviewed for the replacement of the Beech Forest Hall Floor. Components of the project will be awarded shortly. - Final plans are currently being reviewed for Pirron Yallock Recreation Reserve Clubrooms.

Coordinate inspections and reporting of Essential Safety Measures (ESM's) relating to nominated Council buildings.	In Progress	67%	Programmed inspections of essential safety measures in nominated Council buildings are ongoing and is in line with statutory requirements. Any non-compliance issues identified are addressed in accordance with priorities and available budgets.
Implement the 2013/14 Kerb & Channel Renewal Programme.	In Progress	25%	Tenders for the 2014/15 Kerb and Channel renewal program closed 23 February 2015. Submissions are being evaluated.

CAPITAL WORKS

ACTIONS	STATUS	% COMPLETION	PROGRESS COMMENTS
Review and update the environmental protection and management policy and procedure relating to infrastructure activities.	In Progress	90%	A number of documents and procedures related to environmental management of infrastructure projects are being reviewed. A new Construction Environmental Management Plan template and an Environmental Assessment template have been completed. A Council Roadside Reserve Environmental Management Plan (CRREMP) Code of Practice is nearing completion, pending release of the final road agreement with DELWP. The environmental protection and management policy and procedure and are in initial stages of review.
Facilitate the construction of the public off street car park at Pascoe Street Apollo Bay.	In Progress	95%	Works have continued on the Pascoe Street Car Park. The contractors have been required to vacate the site for the week both sides of Easter, but will return to complete asphaltting and finalise the construction of the waste enclosure by mid-April.

Bridge Reconstruction Program

- **Sand Road Bridge** – Works consist of repair of most of the structural elements of the bridge structure and is expected to proceed in April and May 2015.
- **Cape Otway Road Bridge** – Two tenders were received for the bridge rehabilitation works. Both tenderer's prices exceeded the available budget. As such, the project scope has been reviewed, and tenders will again be invited based on the reduced scope.
- **Phalps Road Bridge** – Procurement for the service of rehabilitating the bridge to T44 loading (recommended design standard) has begun. Works are valued at an estimated \$100,000 and consist of the placement of 17 parallel flange channels. The works are expected to proceed in April/ May 2015.
- **Watsons Access Bridge** – Concept designs have been reviewed for the rehabilitation of this bridge. Detailed design of the preferred option is progressing.
- **Pearces Access Bridge** - Procurement for the service of rehabilitating the bridge has begun. Works are valued at an estimated \$80,000 and consist of the placement of

strengthening plates to the structure's beams. The works are expected to proceed in April/May 2015.

- **Wilson Street pedestrian bridge** – The contract for supplying and placing a pedestrian bridge at Wilson Street, Colac across the Barongarook Creek has been awarded. Works are expected to be completed prior to the end of May 2015.

Gellibrand Street Rainwater Garden Project

A contract has been awarded for a feasibility study and detailed design of the Gellibrand Street Rainwater Garden. The project scope is to provide a rainwater garden and stormwater capturing system for water reuse in the Gellibrand Street toilet block at the western edge of Memorial Square. The first stage of the project is to complete a feasibility assessment to ensure the works are achievable for Council to construct and maintain before being designed. Funding for the project is provided by the Office of Living Victoria.

Footpath extension program

The contract for the extension of concrete footpaths in Colac and Apollo Bay has been awarded.

The works consist of approximately 1,700m of new footpath linkages servicing school districts in Colac and Apollo Bay.

Richmond Street Reconstruction

Construction has commenced for reconstruction of Richmond Street, Colac, as part of the 2014/15 Road Reconstruction Program, following awarding of the contract to R Slater & Sons. The reconstruction, between McKenzie Street and Hearn Street, includes 6m-width asphalt road reconstruction including kerb and footpath, upgraded driveways and drainage, and replaced street trees.

Costin Street Reconstruction

The contract has been awarded for the Costin Street road reconstruction in Apollo Bay. The reconstruction, a continuation of the Seymour Crescent works completed in 2013, extends from the edge of Seymour Crescent to the intersection of Montrose Avenue. The reconstruction will widen the corner near Seymour Crescent to improve vehicle turning as well as widen the seal to 6.5m from the existing 5.2m.

Filling in the Gaps of Colac & Apollo Bay's Bicycle Lanes

A tender has been advertised for the Filling in the Gaps project, part of the 2012 Active Transport Strategy's recommendations. This project, which will aim to define some 10km of Colac's bike lanes and 1.5km of Apollo Bay's, is anticipated to be completed by the end of the financial year, weather permitting.

MAJOR CONTRACTS

Waste Management

Transfer station tour

Students from St Brendan's Primary School paid a visit to the Alvie Transfer station. As part of the tour, the group discussed recycling and why it is important and how recycled material is made into other items saving resources for the future. The children also visited the organics sorting facility where composting was discussed and related to their experiences at home where compost was used for the home garden or on the farm.

Detox your home

Colac Otway Shire has signed off on a memorandum of understanding with Sustainability Victoria (SV) to ensure residents have an ongoing service for disposal of household chemicals. The previous Detox your Home collection program which took place on a two year cycle will be replaced with:

- An annual collection of household chemicals in October each year to take place at the Colac Regional Saleyards (actual day to be confirmed).
- A permanent Detox your Home site in Colac where residents will be able to drop off, at no charge, items such as paint, batteries and compact fluorescent lamps.

Tenders

The reporting period is from 9 February 2014 to 8 March 2015.

Tenders opened since the last reporting period:

- 1501 – Cape Otway Road Bridge Rehabilitation
- 1502 – Footpath Extension Program – Colac
- 1503 – Footpath Extension Program – Apollo Bay
- 1507 – Consultancy Services – Stormwater Reuse System Feasibility Study & Design
- 1504 – Supply & Install Prefabricated Pedestrian Bridge – Wilson Street, Colac
- 1505 – Costin Street Reconstruction
- 1508 – Engineering Consultancy Services
- 1509 – Sand Road Bridge Rehabilitation
- 1511 – Concrete Works – Kerb & Channel Renewal

Tenders awarded since the last reporting period:

- 1431 – Organics Mobile Bin Supply – to Trident Plastics (SA)

Tenders advertised since the last reporting period:

- 1512 – Bike Lane Signage and Linemarking, closing 8 April 2015
- 1515 – Phalps Road Bridge Rehabilitation, closing 8 April 2015
- 1516 – Pearces Access Bridge Rehabilitation, closing 8 April 2015

Annual Bituminous Spray Sealing Works

Bituminous spray sealing works are fully completed including linemarking. The contract this year progressed well with Council receiving the benefit of a program of works which commenced early in December 2014.

Subdivision Works

The following table shows the current status of various subdivisional works which will be handed over to Council when completion is approved:

Subdivision	Status
32 Tulloh St, Elliminyt 11 Lot Subdivision	All civil works have been completed but before a compliance certificate is issued the developer must recertify a drainage easement to comply with underground drainage installed.
Queen Street, Colac (Stage 1B & 2) 18 Lot Subdivision	Construction works are continuing in this subdivision with drainage and kerb and channel installed. Road construction and footpath is expected to be completed within the next couple of weeks

COSWORKS

Works undertaken by Cosworks during February 2015 are as follows:

Minor Patching

Minor patching works are ongoing on sealed road repairs to keep up with potholes and edge breaks. This month the focus has mainly been in Apollo Bay Streets, Coastal townships and Apollo Bay rural roads.

Road Construction

The Colac-Lorne Road rehabilitation works have completed on a 1550 metre section that included pulverisation of existing pavement, reshaping and compacting. This was followed by adding base layer, reshaping, compacting, sealing, linemarking and guide post installation.

The Eurack Road rehabilitation works have commenced on a 2200 metre section which included pulverisation of existing pavement, reshaping and compacting. This will be followed by adding base layer, reshaping, compacting, sealing, linemarking and guide post installation and be completed by late March 2015.

Road Regrading

Road regrading was conducted on the following roads and areas:

- Coulstone Street
- Bullock Swamp Road
- Short's Road
- Johnstone Estate Road
- Callahan's Road
- Dewing's Road
- Roseneath Road
- Dunlop's Road
- Hargreaves Road
- Hordern Vale Road
- Green's Access
- Clancy's Access
- Gellibrand East Road
- Southern Road
- Beauchamp Falls Road
- Amiets Track
- Gellibrand Township
- Trigg's Road
- Aerodrome Road
- Poomeet Station Road
- McDonnell's Road
- Raffertys Road
- Warncoort Cemetery Road
- Birregurra Yeodene Road
- Mooleric Road
- Melanesia Road
- Hendrickson Track
- Rifle Range Road
- McDonalds Access
- Lardners Track
- Jackson Track
- Binns Road
- Hopetoun Falls Road
- Birregurra Street's

Gravel Road Resheeting

6km of Resheeting works have been completed on:

- Kelly's Lane
- Barholm Lane
- De La Rues Road

Linemarking

Statutory Control (Statcon) line-marking has commenced in Apollo Bay on bike lines, bike symbols, statcons and pedestrian crossings. Council's contractor has commenced rural centre line marking. Linemarking is also ongoing in Colac on car parks, statcons and school crossings.

Major Drainage Works

During February 2015 works were completed on:

- Morris Access
- Hickey's Cutting
- Murroon Road
- Gravel Pit Road
- Broughton Access
- Division Road
- Barholm Lane

Routine Drainage Works

During February 2015 routine drainage works were completed in the following townships:

- Apollo Bay
- Kennett River
- Skenes Creek
- Wye River
- Separation Creek
- Marengo

Council's drain cleaning contractor has commenced work in the Colac area to remove tree roots from underground drainage system and to unblock pits.

Landslip Capital Works

Works on the Old Beech Forest slip site at the 7 kilometre mark have been completed. Included in the works was a 25m retaining wall with subsoil drainage. A 15m surface drain was also added with concrete matting placed to help stop erosion.

Bridge Maintenance

During February 2015 bridge maintenance was completed as follows:

- Nalangil Bridge - new guard rail posts installed
- Phalps Road Bridge - guard rail maintenance completed
- Wilson Street Bridge - a new anchor for guardrail installed
- Chapel Street Bridge - painted concrete spalling repaired.

Roadside Slashing

During February 2015 slashing works were carried out on the following:

- Morris Track
- Carlisle Gellibrand Road
- Walls & Skinners Road
- Moomowroong Road
- Monty's Vista
- Lyness Access
- Burrupa Road
- Devondale Road
- Eurack Road
- Troy's Road
- Normans Road
- Beal's Road
- Buckley's Road
- Woods Road
- Bullivant Road
- Collins Road
- Edwards Road
- Crabbe's Road
- Cashins Road
- Larson's Vista
- Tuckers Orchard Road
- Lucas Road
- Karacays Road
- Egan's Track
- Sand Pit Road
- Kings Track
- Mingawalla Road
- Corbett's Road
- Beeac Warrowie Road
- Sunnyhills Road
- Sproules Road
- McMasters Road
- Drapers Road
- Trigg's Road
- Trasks Road
- Roseneath Road

- Collyer's Road
- Old Yeo Road
- Irrewarra School Road
- Pyles Road
- McKay's Road
- Bourke's Road
- Warrowie Road
- Armstrong Street
- Sinclair South
- Queen Street
- Wamcoort Cemetery Road
- Old Friends Road
- Hardbattles Road
- Ryan's Road
- N Dennis Road
- Ramsay's Road
- Pound Road
- Irrewillipe Road
- Harris Road
- Airey's Street

Vegetation Control

Vegetation works were undertaken on:

- Binns Road
- Pipeline Road
- Boundary Road
- Porcupine Lane

Township Mowing

Township mowing has been completed at:

- Apollo Bay
 - Beech Forest
 - Forrest
 - Beeac
 - Birregurra
 - Colac
 - Gellibrand
 - Barwon Downs
 - Carlisle
 - Cressy
 - Kwararren
- 2.**

New areas of mowing have also been completed in:

- Alvie Township
 - Swan Marsh
 - Cororooke Township
- 3.**

Storm Damage

Over the last month Parks and Gardens teams have been cleaning up storm damage from January 2015, especially in the Yeodene and Barwon Downs areas.

Sport Ovals

During February 2015, the mowing of ovals has continued. The Lake Colac oval was vertically cut to thin out the couch grass. The Central Reserve was fertilized in anticipation of the upcoming playing season.

Colac Township Parks

Works continues on trees being trimmed and lifted. Mowing of our parks is well underway in Colac. General rubbish removal has been undertaken in all Colac parks.

Tree Maintenance

Tree maintenance works were undertaken in February 2015 including:

- Lifting tree's in the medium strip of Murray Street
- Lifting and weight reduction of trees in playground and rotunda areas of Memorial Square
- Pruning has been completed in the Rex Norman Park
- Pruning has been completed on Knowledge Track

Old Beechy Rail Trail

Tree trimming works have occurred during February along the new section from Dinmont towards Beech Forest.

SUSTAINABLE PLANNING AND DEVELOPMENT

Planning and Building

Colac 2050 Plan

The proposed project structure milestones have been agreed in principle with Regional Development Victoria (RDV) and officers have updated the project charter accordingly. Work will shortly commence on the initial parts of the project, which will initially focus on retail commercial and industrial strategies.

Domestic Wastewater Management Plan (DWMP)

Consultant firm Whitehead & Associates is currently finalising the Technical Document (Background Report) and Operational Document (DWMP) for submission to Council shortly. These documents will be reviewed by Council and other parties such as the relevant water authorities and the EPA. The draft report will be then be considered by Council prior to community consultation on its recommendations.

Planning Scheme Amendment C74 (Apollo Bay Settlement Boundary and Urban Design Review)

This amendment came into effect when notice of approval was published in the Government Gazette on Thursday 19 February 2015. The amendment updates urban design initiatives for the Apollo Bay shopping centre, confirms the location of the settlement boundary for Apollo Bay and Marengo, rezones part of the Apollo Bay industrial estate in response to its proximity to residential areas, rezones and applies a Development Plan Overlay (DPO) to land at the northern end of Apollo Bay for residential development, amends the DPO schedule for the Mariners Vue estate, applies a common DDO schedule to both the Mariners Vue estate and the proposed new residential area, and lists the Apollo Bay Settlement Boundary and Urban Design Review 2012 as a reference document.

Economic Development

Small Business Van

The State Government's Small Business Van will be visiting Colac in June 2015 and will be located at the Memorial Square. The van will be offering five free mentoring sessions with a mentor from the Small Business Mentoring Service. Existing and new business people have the opportunity to receive information from the van on topics such as marketing, cash flow, how to start a new business and succession planning.

Film Friendly guidelines

The Economic Development Unit (EDU) is working with Council's Public Relations and Local Laws departments to ensure Council has the appropriate process for filming in the region. Film Victoria has contacted each Council and set out guidelines to assist film makers when having to lodge a filming application. Council has produced guidelines and an application form for film makers in line with Film Victoria's regulations. The guidelines, form and key information are located in the Economic Development section of Council's website.

Regional Victoria Living Expo

EDU is currently working on marketing material for this year's Regional Victoria Living Expo. This is the final year of the Expo which is aimed at attracting those living in Melbourne to move to regional areas. While there may be very few people who visit the Expo and choose to move, it is an opportunity to encourage people to visit and then perhaps consider moving or investing in the Shire. Council received a \$10,000 grant from the Victorian Government to assist with the development of marketing collateral for the Expo. The Expo will be held at the Melbourne Convention and Exhibition Centre from 1-3 May 2015.

Business Events

EDU is currently working with Crowe Horwath on the next 'Secrets of My Success' event which will be held in May 2015. Based on the success of last year's event with over 60 people attending, this year's event will have the same focus with a range of speakers from different industries. More information will be available closer to May.

Barwon Downs Combined Community Centre

A \$240,000 development of the Barwon Downs Combined Community Centre has been completed. The project was a collaborative effort with Council, CFA, the local CFA group, the Barwon Downs Community Group and the Barwon Tennis Club all working together to complete the project. The new centre was achieved through funding of \$20,000 cash and \$15,000 in-kind from the Barwon Downs groups, a \$25,000 Council Small Town Improvement Program (STIP) grant and a \$180,000 Victorian Government grant. The new centre will assist the local CFA group to hold meetings and conferences and provide club rooms for the Barwon Downs Tennis Club. The Barwon Downs community group will also use the centre for their monthly meetings.

Tourism

Colac Visitor Information Centre (VIC)

Colac VIC February visitation was down 12% from February 2014 however there was an increase of the Love Our Region (LOR) membership. The majority of visitors to the Centre surveyed were international (47%) with a large percentage coming from China (25%) due to the Chinese New Year holiday period.

Colac VIC retail sales were down 8% compared to February 2014. An increase of Otway Fly ticket sales (70%) can be attributed to increased marketing by the Otway Fly.

Accreditation is underway with coordinators from both the Colac and Great Ocean Road centres working together. This is a great opportunity to review the Centre's business and marketing plans, licenses and insurances. This process should be completed by early April.

	2014	2015	% Change to previous year
Walk – Ins	3144	2762	-12%
Phone Calls	172	113	-34%
Otway Fly	\$360	\$611	70%
Retail Sales	\$3653.40	\$3358.85	-8%
New Res Kits	13	4	-69%

Great Ocean Road Visitor Information Centre (GORVIC)

GORVIC visitation was up 5% from February 2014 with 15,433 counter enquiries. Phone calls were also up 24% and email enquiries 193%.

Visitation along the Great Ocean Road during the Chinese New Year has been very high. GORVIC visitor surveys identified that 54% of customers arriving were internationals mainly from China, UK and Germany. Interstate visitors were arriving mainly from NSW, QLD & SA.

February 2015	2014	2015	% Change
Walk-ins	14,712	15,433	+5%
Phone calls	1,640	2,031	+24%
Email enquiries	27	79	+193%
V-Line	\$5,317.70	\$5,603.80	+5%
Otway Fly - Walk	\$9,071.00	\$12,609.00	+39%
Otway Fly - Zip Line	\$2,898.00	\$3,640.00	+26%
Cape Otway Lightstation	\$3,941.00	\$6,773.00	+72%
Retail Sales	\$16,614.70	\$19,882.40	+20%

The Apollo Bay Seafood Festival was a great one day event well attended and enjoyed, with very positive customer feedback. The ongoing closure of Marriners Falls continues to be a regular customer complaint.

Great Ocean Road Regional Tourism (GORRT)

GORRT is continuing to work on several key projects for the region including development of the Great Ocean Road Advocacy Program, the Tourism Master Plan, the three year Strategic Marketing Plan and the Digital Strategy. GORRT continues to actively involve the community, businesses and Council in all of these processes.

GORRT continues to lobby for investment in the Great Ocean Road (GOR) Region via their advocacy program which was launched prior to last year's state election. The program aims to facilitate visitor management and to create a sustainable visitor economy. The program highlights the key issues and challenges within the region and the opportunity that the visitor economy presents in increasing employment within the region and reducing youth unemployment in particular.

The Tourism Master Plan commenced with the development of a series of action plans for key destinations within the region. These Destination Action Plans (DAPs) have now been completed and are ready for circulation. The plans have been formally presented to the leadership groups who participated in their development at industry workshops in March and will be formally presented to stakeholders.

Consultation is continuing in the development of the Tourism Master Plan for the region. Formal responses to the discussion paper continue. A Great Ocean Road Master Plan Project Steering Committee is also meeting with each individual Council, key land managers and industry bodies to discuss their submissions.

GORRT is working in partnership with Tourism Victoria to review the Great Ocean Road Master Brand and to establish a communication framework to facilitate the Board's greater

focus on destinations. The project is expected to deliver a communications framework for the region as well as for each of the key destinations. A series of workshops have been held throughout March to provide industry input into the branding project.

GORRT have appointed Bliss Media to develop the new GOR digital program. The program will include the development of a regional site and destination sites.

Council Tourism Marketing

The popularity of the GOR Visitor Information Centre's facebook page has increased during February and the facebook page now has 7,679 likes. The facebook page has become a key tourism destination marketing tool for Council.

Regional Development Victoria has offered to promote the Otways region to Victorian residents via the major prize at the 2015 Regional Victoria Living Expo. The Colac Otway Shire is aiming to showcase our local tourism product in the form of a Weekend Escape. This is a rare and valuable opportunity for our region. The Otways Weekend Escape will get top billing in the Herald Sun lift out for this year's Regional Living Expo on print collateral and the website. An array of products has been donated to the expo prize by local operators.

On 17 February 2015 the Cape Otway Lightstation hosted the Channel 7 Sunrise Weather team. Council's EDU assisted the Sunrise team with the telecast that showcased the GOR region, local businesses and the Apollo Bay Seafood and Local Producers Festival. The opportunity to host Sunrise was leveraged by GORRT.

Current GORRT media pitches include:

- An article in Apple Daily (Hong Kong's main newspaper) about the Great Ocean Road Region Eco Experiences
- An article in Outback Magazine on Victoria's Volcanos
- Articles about the 2015 Great Ocean Road Marathon in both Air Asia (inflight magazine) and Bharat Times Indian community newspaper.

Tourism Development

Council has applied to host the 2016 Rural Council Victoria (RCV) Summit. If successful, Council will host the RCV Rural Summit in Apollo Bay in June 2016. The purpose of the RCV Rural Summit is to equip rural councils, leaders and residents of rural communities with the necessary information, skills and capacity to respond positively to ever changing environments. RCV is offering a grant of \$60,000, along with the proceeds of ticket sales, to the successful council to host the 2016 Summit.

Small Town Improvement Program

The Gellibrand Community House renovation has been completed and was open to the public on 28 February 2015 during the Bluesberry Festival market in the Rex Norman Park. The renovation has opened up the space by removing an internal wall, and installing a kitchenette and desktop area which will allow space for internet use and a larger room for a meeting place.

Lake Colac Coordinating Committee

Expressions of interest have been received from community members who will work with the Advisory Committee to provide advice on issues related to the management of Lake Colac. These applications are being processed.

Environment and Community Safety

Fire Prevention

Council's Municipal Fire Prevention Officer is continuing to undertake inspections of rural properties to ensure that grass height and fuel loads are kept to the required standards during the fire season. Over 200 fire prevention notices have been issued and only five landholders failed to meet the requirements. While it is good news that there is generally a high rate of compliance and we are nearing the end of the Fire Danger Period it is important that landholders continue to maintain their properties. Due to the relatively mild conditions Council and the CFA have agreed to start issuing permits to farmers to burn off stubble. If you have questions about burning off please call Council and ask to speak to the Municipal Fire Prevention Officer.

Clean Up Day Events

Clean Up Australia celebrated its 25th birthday this year with an estimated 526,268 volunteers collecting 13,563 tonnes of rubbish across Australia. Colac Otway Shire participated with a full week of clean-up events. In the week leading up to Clean Up Australia Day Council's Environment Unit ran a number of events in Colac. Council staff participated in a clean-up event along Barongarook Creek and although a large amount of rubbish was picked up it was noticeably less than last year. This is a good sign that our efforts are steadily improving the health of the creek.

Council staff also worked with a number of schools to help clean up areas in the local vicinity of their school. VCAL students from Trinity College cleaned up along the Princes Highway, Colac Specialist School students cleaned up Wilson Street and a section around the railway line, Colac West Primary School students cleaned up along the banks of the Lake Colac foreshore, and Colac Primary School students cleaned up around the Bird Reserve and local playgrounds.

All students should be commended for getting their gloves dirty to make a positive difference to our environment. Two other groups, the 2nd Colac Sea Scouts and team members from McDonalds, also took to the shores of Lake Colac to make a difference.

Colac Otway Shire would like to congratulate all those who participated in Clean Up Australia Day this year. Groups are also encouraged to register formally with Clean Up Australia next year so all efforts are formally recorded and to take advantage of Council's support for disposing of any rubbish collected.

Animal Registrations

Animal Registrations are due on 1 April 2015. Reminder notices will be sent out by Council to all owners of animals that are currently registered. Registering animals is important for the welfare of the animal. If an animal is not registered it is impossible for it to be returned if it gets lost. Penalties exist for failing to register animals and Council will be undertaking follow up inspections to ensure all animals are registered.

Domestic Animal Management Plan

Under State Government legislation every council is required to develop a Domestic Animal Management Plan (DAMP) to set objectives relating to domestic animal management. The current 4 year (DAMP) was endorsed by Council on 24 July 2013 and is reviewed annually to ensure it remains current. The review of the DAMP has found that the current plan is addressing the needs and requirements of the community and does not need alteration or amendments at this stage. The following is a summary of the key areas assessed during the review process:

- **Animal Management Officer training:** All officers have attended and obtained accreditation in the identification of Restricted Breed Dogs (Canine Anatomy). Officers have also attended the mandatory course for the Auditing of a Domestic Animal Business.
- **Animal Registration & Identification:** Animal numbers remain static and as of June 2014 there were 6396 (Dogs 4863 / Cats 1533) animals registered. Authorised officers conducted over 600 inspections on premises where animals had not had their registrations renewed and further random inspections were conducted. 99 Infringements for failing to apply for registration were issued (the value of the infringements were \$289.00 each and this represented a total of \$28,611.00). There were a small number of prosecutions in the Magistrates Court for failing to apply for registration (10 in all).
- **Dogs and cats at large:** Many animals (dogs and cats) are returned home to their owners with appropriate warnings being issued. The ability to contact owners has been greatly enhanced since the mandatory micro chipping of new registrations. This has substantially impacted on the number of animals arriving at the pound. Habitual offenders have their animals impounded and they can also be issued with an Infringement Notice for animal at large. Dog impoundments remain static at about 270 per year. Increased patrols targeting animals at large has been extremely effective and has sent a clear message to the community about the consequences of not confining their animals. The biggest impact on animals at large is the community dealing with the issue themselves, by utilizing social media (Facebook, Gumtree, Colac Swap Buy and Sell, etc.) in all there are about 5 sites used by the public. The animal is reunited with its owner without Council attendance or intervention.
- **Dog attacks:** For the period of 1/7/2013 to 30/6/14 there were 14 dog attacks reported. Of these attacks, two were of a serious nature against animals and both matters were successfully prosecuted in the Colac Magistrates Court. Seven were not of a serious nature and were dealt with by way of an Infringement Notice. Five additional dogs (2 Dangerous and 3 Menacing), of which 2 have since been euthanized, were declared and entered on the Declared Dog Registry. The Registry has 14 dogs currently registered as alive and this amount has been constant over the last 2 years. The Registry has been audited and is up to date. There has been a substantial reduction in dog attacks from 17 to 9. Prosecution and enforcement in the Local Magistrate's Court has been proven to be a strong deterrent.

Attachments

Nil

Recommendation(s)

That Council notes the CEO's Progress Report to Council.

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**OM152503-2 HARRINGTON MEMORIAL PARK MANAGEMENT OPTIONS**

|             |                                |           |              |
|-------------|--------------------------------|-----------|--------------|
| AUTHOR:     | Ian Seuren                     | ENDORSED: | Colin Hayman |
| DEPARTMENT: | Corporate & Community Services | FILE REF: | F14/217      |

**Purpose**

To consider options for the future management of Harrington Memorial Park, Wye River.

**Declaration of Interests**

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

**Background**

Harrington Memorial Park, located at 1 McLellan Court Wye River, is a 1ha parcel of public open space owned by Council. The reserve is zoned Public Purposes and Recreation (PPRZ) and provides passive open space. The park is one of the only parcels of useable open space in Wye River/Separation Creek away from the foreshore. It also provides for off-road pedestrian movement from the top of Wye River to the foreshore. Harrington Memorial Park is an important parcel of public open space with which the local community has a strong affiliation.

As a Council owned reserve, Council is responsible for managing and maintaining the reserve. At present, Cosworks mow the reserve a number of times per year as well as undertaking some other minor maintenance.



**Harrington Memorial Park Advisory Committee:**

An Advisory Committee for the Harrington Memorial Park was formed and endorsed by Council in 2001. Further to this, in 2004 Council endorsed the membership of the Committee following expiration of the committee’s term. While the Committee continued to

be active, since that time there has been no further endorsement of the Committee or its members by Council. Councillors attended, or were invited to attend, Annual General Meetings (AGM) over the years.

More recently, the Advisory Committee has partnered with Council to improve the park from a bush block to a usable public reserve. Prior to the recent redevelopment project, the park contained a walking path, garden beds, seating and picnic furniture.

At its 23 October 2013 meeting, Council dissolved the Advisory Committee and endorsed the establishment of a Section 86 Committee of Management of local community members to manage Harrington Memorial Park.

Council then called for Expressions of Interest from individuals interested in being on the Harrington Memorial Park Committee of Management. Correspondence and an Expression of Interest form were sent to all landowners in Wye River and Separation Creek.

A total of nine community members registered their interest in being on the Harrington Memorial Park Committee of Management. All applications adequately met the selection criteria and provided a range of skills and experience beneficial to managing Harrington Memorial Park.

A council report was considered by Council at the 26 March 2014 Council meeting.

The purpose of the report was to consider the appointment of a Section 86 Committee of Management for Harrington Memorial Park and the Instrument of Delegation for the Committee.

The report was deferred to the In-Committee part of the meeting.

At the 26 November 2014 Council meeting Council passed the following resolution.

***That Council:***

- 2. Defers the establishment of the Section 86 Harrington Memorial Park Committee of Management until satisfactory resolution of the works program associated with the Multi Use Recreation Facility and consideration of a report to be provided to Council by March 2015 on the future management options of Harrington Memorial Park.***

**Council Plan / Other Strategies / Policy**

**Good Governance**

Means we care about and are responsive to the community, encourage democratic participation and involve people in decisions that affect them. We strive for excellence in financial management and council services, and always look for better ways to do things.

*Our Goal:*

Ensure transparency of governance practices, the capability of our organisation and effective resource management.

**A Place to Live and Grow**

Is a community where people feel cared for and supported; where buildings and spaces facilitate creativity, social activity and enrichment of life, and people have access to gain the skills and education needed to reach their potential.

*Our Goal:*

Improve access to buildings, spaces, services and education to support and enable quality of life.

This report is aligned to Council's Public Open Space Strategy.

**Issues / Options**

In February 2015, Council considered a report regarding the Harrington Memorial Park Redevelopment project. The report identified that the project had created significant conflict within the community over an extensive period of time and that it was necessary for Council to determine the preferred management arrangements of the park.

There a number of options for Council to consider. These include Council directly managing the park or establishing a committee to assist with its management. Information relating to the different types of Council appointed committees is detailed below.

**Advisory Committees**

A Council may establish Advisory Committees whose purpose is to look into and advise the Council on matters referred to it.

Unlike a Special Committee of Council, an Advisory Committee does not have any formal, delegated powers to act in place of the Council. Advisory Committee decisions or recommendations have no legal standing unless they are adopted by the Council at a formal meeting.

Although there is no delegation of powers, an Advisory Committee should operate under a "Terms of Reference" document, in which the Council sets out the Committee's purpose and how it will function. The Council retains control over the membership and purpose of the committee.

The main function of an Advisory Committee is to assist Council in the consultative process and provide valuable information to support the decision making of Council.

Advisory Committees can assist Councils by spreading a Council's workload, enabling a Council to recruit and utilise particular expertise to help the Council make its decision by engaging community resources and opinion.

Short term Advisory Committees (sometimes called "working groups" or "ad hoc groups") may be created for a particular purpose and disbanded when that purpose is achieved.

In some cases Advisory Committees consist of all Councillors or also include Council officers and community members. The Committees provide advice on a range of issues including: projects, planning, policy, resources, access, community amenity and other strategic community issues.

Advisory Committees have no other authority or purpose than to provide advice to Council to assist in its ultimate decision-making role.

A Council is not bound to accept a recommendation of an Advisory Committee.

The purpose of an Advisory Committee is to:

- Advise on program development and policy direction
- Provide information to Councillors
- Comment on issues relating to their area of expertise
- Develop and support committee member's skills
- Develop partnership between Councillors, participants and Council.

Colac Otway has a number of Advisory Committees, with Councillor and community representatives.

Current Advisory Committees:

1. Audit Committee
2. Australia Day Advisory Committee
3. Central Reserve Advisory Committee
4. Colac Livestock Selling Centre Advisory Committee
5. Festival and Events Support Scheme Advisory Committee
6. Friends of the Colac Botanic Gardens Committee
7. Grants/Community Funding Advisory Committee
8. Lake Colac Co-ordinating Committee
9. Small Town Improvement Program Advisory Committee.

### **Specific Purpose Committees**

Specific Purpose Committees are created for a discrete and specific purpose. Their role is to review an issue or undertake a project and report back to Council. These committees conclude/sunset at the delivery of the project brief.

### **Special Committees**

A special committee is any committee to which the Council delegates a duty, function or power.

When exercising a delegated power, duty or function, a special committee effectively "is the Council".

All special committees must comply with the rules for special committees in the *Local Government Act 1989* (the Act). This includes delegation limits, meeting arrangements and conflicts of interest.

### **Section 86 Committees**

Most Council special committees are specifically established under section 86 of the Act and are sometimes referred to as "Section 86 committees".

Council appoints Committees of Management under Section 86 of the Local Government Act 1989 as Special Committees to Council in accordance with rules and conditions of appointment adopted by Council. The purpose of these committees is to manage the operations of the facility and/or land. Council policy is that Committee of Management members are appointed for a three year term.

### ***The legal status of Special Committees***

The Special Committee of Council exists only as a Special Committee of Council. It is not a legal entity in its own right and therefore can only exercise the functions and powers and perform the duties that Council delegates to it. A Special Committee of Council cannot hold property in its name, nor can it be sued or initiate legal proceedings. A Special Committee of Council can enter into binding contracts provided the value of the contract is within the delegated authority, because Council gives its *Special Committees of Council* the power to do so under Council's standard *Instrument of Delegation*.

### ***Membership***

A special committee may include Councillors, Council staff or any other people as members. The membership of a special committee is a matter for the Council to decide. It is recommended that Councils regularly review the membership of its special committees.

### ***Appointing members***

No appointment process is specified in the Act for the appointment of community members to special committees. However, it is common for Councils to use an application form to collect information about candidates.

Where a council is appointing community members to a committee of management for a recreation reserve, public hall or other public facility with tenant sporting clubs or community organisations, it is encouraged to allow those clubs/organisations to nominate representatives to serve on the special committee. However, the Council remains responsible for making the formal appointment.

A Council may at any time remove a member from a special committee.

### **Options**

In relation to the future management of Harrington Park, there are a number of options available to Council which include:

1. Council to manage and maintain Harrington Memorial Park exclusively.
2. Re-establish the Harrington Memorial Park Advisory Committee to provide advice to Council.
3. Establish a Section 86 Committee of Management for Harrington Memorial Park and delegate the management of the park to the local community. This could include a number of sub-options:
  - a. To appoint all nominees as per the original process.
  - b. As per option a. but with the addition of a person to represent the interests of the Community Garden.
  - c. Restart the Expression of Interest process.
  - d. Appoint a community representative group (eg: the Wye River/Separation Creek Progress Association) as Committee of Management.

### **Recommended Option**

The recommended option is for Council to directly manage Harrington Memorial Park. This would be consistent with the management and maintenance of parks across the shire. Council directly manages all neighbourhood parks within the municipality although there are a number of parks where communities provide assistance with maintenance eg: Birregurra Town Park.

Council has always assisted in the maintenance of Harrington Memorial Park by mowing the grass and assisting with other maintenance items such as the clean-up of fallen tree limbs. With the development of the BBQ, shelter and levelled gravel area there would be a requirement for some additional minor maintenance.

Formal use of the park for activities such as community events or activities (eg: Petanque competitions) would be required to go through Council's normal process for reserve bookings and/or obtaining an event permit. In addition, a regular user group could enter into a formal agreement with Council which is similar to the process used for Colac recreation reserves which are managed by Council.

If Council was to support the recommended option, it would be required to rescind the resolution from its 23 October 2013 meeting, this being:

***“That Council:***

***1. Endorses the establishment of the Section 86 Harrington Memorial Park Committee of Management.”***

***Community Garden***

It should be noted that there is a need to formalise the process around the community garden. The community garden is currently being overseen by a local community member.

As the garden is located on Council land, there does need to be formalised control established for it eg: policy/guidelines for size, access, types of plantings etc. It was initially intended that the proposed S86 Committee of Management would control this; however Council will need to control and establish a separate process for the management of the space and access to garden beds.

Council officers are currently investigating appropriate guidelines and processes to manage the community garden.

**Proposal**

That Council:

- Rescinds its resolution at the 23 October 2013 meeting: “That Council endorses the establishment of the Section 86 Harrington Memorial Park Committee of Management.”
- Determines to directly manage Harrington Memorial Park.

**Financial and Other Resource Implications**

Historically, Cosworks have undertaken mowing and minor maintenance of Harrington Memorial Park. In 2014/15, expenditure is estimated to be approximately \$4,000 which is within the allocated operational budget.

According to Cosworks, having full management responsibility of the park, without an Advisory Committee or Committee of Management in place, would not result in a substantial increase in costs. Cosworks would include the park into their coastal maintenance regime, which would be cost-effective. There may be a requirement for maintenance of the gravel surface, estimated to be \$500 per year.

An additional cost to Council will be the cleaning of the new single BBQ. At present, a local contractor is monitoring and cleaning the BBQ and picnic furniture. BBQ cleaning costs will

be factored into Council's shire-wide BBQ cleaning budget and would be an approximate cost of \$1,000 per annum.

As per all Council parks, maintenance costs may depend on requests from the community and seasonal conditions. Determining and communicating a level of service for park maintenance to the community will be important.

### **Risk Management & Compliance Issues**

Council directly managing the park provides greater control over park maintenance and in turn possibly reduces the level of risk to park users.

### **Environmental and Climate Change Considerations**

Harrington Memorial Park is an important parcel of public open space. Management and maintenance of the reserve will incorporate relevant environmental considerations.

### **Community Engagement**

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method selected would be inform. The community would be advised of the decision via correspondence to members of the community who had previously expressed an interest in being on a S86 Committee of Management. The broader community would also be informed via a range of mediums including the Community Noticeboard and the Progress Association's newsletter.

There has been significant community interest in Harrington Memorial Park, particularly during the recent redevelopment project. It is clear that there was, and still are, conflicting views on the need for the project and how the project was managed. There are also conflicting views on how the park should be managed into the future. Members of the former Advisory Committee have expressed strong views that the park should be managed by the community. Recent correspondence from the Wye River & Separation Creek Progress Association states that they support Council's previous resolution to establish a S86 Committee of Management for the park.

Of importance is that Council needs to communicate clearly and openly with the community about the future operation and management of the park. In addition, any future use or development of the park should be guided by an extensive and inclusive community engagement process facilitated by Council.

Council officers will engage community members interested in having access to the community garden in the development of policies and guidelines.

### **Implementation**

Council has previously sought Expressions of Interest from the community to nominate for the Harrington Memorial Park S86 Committee of Management. Should the recommendation in this report be supported, Council would write to the previous nominees informing of the decision for Council to directly manage the park. Furthermore, Council would also inform the broader community of its decision through a range of mediums.

Council will also commence the development of policies and guidelines for the management of the community garden.

**Conclusion**

Harrington Memorial Park is owned by Council and is an important parcel of public open space for the coastal communities of Wye River and Separation Creek. Council directly managing Harrington Memorial Park is in line with the management and maintenance of neighbourhood parks across the shire.

**Attachments**

Nil

**Recommendation 1**

***That Council rescinds its resolution of the 23 October 2013 meeting: "That Council endorses the establishment of the Section 86 Harrington Memorial Park Committee of Management."***

**Recommendation 2**

***That Council Determines to directly manage Harrington Memorial Park.***

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OM152503-3 CENTRAL RESERVE REDEVELOPMENT - PROJECT REFERENCE GROUP

AUTHOR:	Ian Seuren	ENDORSED:	Colin Hayman
DEPARTMENT:	Corporate & Community Services	FILE REF:	F14/6181

Purpose

To appoint a Councillor to the Central Reserve Redevelopment – Project Reference Group.

Declaration of Interests

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

Background

Central Reserve is Council's premier recreation reserve in Colac catering for a range of sport and recreation activities including AFL, netball, soccer and little athletics.

Central Reserve was developed around 1960 with its main grandstand facility constructed in 1962. The grandstand is a two-storey building encompassing home and away changerooms and amenities on the bottom level and tiered spectator viewing on the second level.

Central Reserve is currently home to the Colac Football Netball Club which participates in the Geelong Football League. The reserve also caters for the Colac & District Football Netball League finals series. In addition, the reserve is used for a range of sport and recreation activities including:

- Soccer
- Little athletics
- Cricket
- School sports
- Tennis
- Dog Obedience.

The Central Reserve Master Plan was completed in 2000 and provides guidance in improving facilities at Council's premier recreation reserve. The Central Reserve Redevelopment Project has been a priority project for Council for a number of years. The project aims to implement the key components of the master plan including:

- New grandstand building
- Refurbishment of the existing grandstand
- Netball changespace
- Netball court upgrades
- Competition lighting for the oval and courts
- Spectator amenities.

In the lead up to the 2013 Federal election, Sarah Henderson MP, Member for Corangamite, made a commitment of \$2.5m to the upgrade of facilities at Central Reserve. Council has lobbied the State Government extensively for matching funds for this project. Unfortunately the State Government is yet to commit funds to any components of the project.

Due to timelines associated with the Federal Government commitment, there is a need to commence detailed design for a number of project components.

Council Plan / Other Strategies / Policy
A Place to Live and Grow

Is a community where people feel cared for and supported; where buildings and spaces facilitate creativity, social activity and enrichment of life, and people have access to gain the skills and education needed to reach their potential.

Our Goal:

Improve access to buildings, spaces, services and education to support and enable quality of life.

Other related initiatives

This project implements the Central Reserve Master Plan. It also implements recommendations from Council's Public Open Space Strategy.

Issues / Options

Council has lobbied the State Government extensively for matching funds for this project, however to date a funding commitment has not yet been achieved. Notwithstanding the lack of State Government financial support, the project will progress using the Federal funding component to deliver a reduced project scope. Council officers will continue to seek funding from other levels of government for project components that fall outside of the current budget.

With significant funds committed, Council is eager to commence the detailed design process in order to seek tenders for construction. The detailed design phase will articulate what can be achieved for the budget available. It will also provide greater detail to assist with funding applications to other levels of government.

Council is currently seeking tenders for Design Services to undertake detailed design and specifications for the following project components:

- New grandstand building
- Refurbishment of the existing grandstand
- Netball changespace.

Tenders closed early March 2015 and it is anticipated that a design team will be appointed by the end of April 2015.

Design for lighting and netball court upgrades will be undertaken separately to the above components due to their specialist nature or Council's ability to complete in-house.

The project will be governed by a Project Team comprising Council representatives and led by Council's General Manager Infrastructure and Services. The role of the Project Team will be to ensure that the project is delivered to the agreed scope and to the budget available.

The improvement to facilities at Central Reserve has largely been driven by user groups of the reserve. Therefore, it is vital that the key users have extensive input into the design process. To support the Project Team, it has been determined that a Project Reference Group be established to assist in the design process which is representative of the key user groups, these being:

- Colac Football Netball Club
- Colac & District Football Netball League
- Colac & District Football League Netball Association

- Colac Otway Rovers (soccer)
- Colac & District Football Umpires Association.

The Project Reference Group would also include Council officer representation.

The role of Project Reference Group would be to provide advice to Council and to represent the views of the key user groups. It would report to the Project Team and provide guidance on the required design. It would not have decision making power.

Due to the significance of the project, it is proposed that the Project Reference Group be chaired by a Councillor to provide a conduit between the community user groups and Council and ensure Council's views are represented. It will also provide Council with a better understanding of the community's needs when delivering this project.

Proposal

That Council nominates a Councillor to Chair the Central Reserve Redevelopment Project Reference Group.

Financial and Other Resource Implications

There are no financial or resource implications in the setting up of the Project Reference Group.

Risk Management & Compliance Issues

There are no risk management or compliance issues in the setting up of a Project Reference Group.

A key risk will be ensuring that through the detailed design process the design reflects the budget available. It will be important that community expectations are managed sufficiently so that any proposed works are within available budget. This will be a key role of the Project Team as well the Councillor chairing the Project Reference Group.

Environmental and Climate Change Considerations

There are no environmental or climate change considerations in the setting up of a Project Reference Group.

In terms of the design process, Environmental Sustainable Design (ESD) will be an important consideration. An important principle of the project is to utilise an environmentally sustainable philosophy in a holistic manner as well as in a detailed way for energy and water usage. It is anticipated that leading edge ESD principles will be adopted into the design with the intention of achieving a 6 star green star rating for buildings.

Community Engagement

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method selected would be involve. Establishing a Project Reference Group representing the key user groups will ensure that the community has sufficient input into the design process. In addition, Councillor representation will better enable Council to understand the needs of the key user groups.

Further consultation will be undertaken with the Central Reserve Advisory Committee, which comprises a broader range of clubs and groups which utilise the Central Reserve.

Consultation will also take place with other current or potential users including AFL Barwon, AFL Victoria, the Victorian Football Federation (soccer), Netball Victoria and the Geelong Falcons.

Implementation

Once Council nominates a Councillor Representative and a design team has been appointed, a Project Reference Group will be established and commence its function.

Conclusion

The Central Reserve Redevelopment Project has been a priority project for Council for a number of years. The Federal Government funding commitment of \$2.5m enables Council to deliver some components of the project. A design team will be appointed in the near future which will enable Council to commence the detailed design process.

The project will be managed by a Project Team consisting of Council officers. Establishing a Project Reference Group representing key user groups will ensure that the community has extensive input into the design process. Having a Councillor chair the Project Reference Group will provide a conduit between the community user groups and Council and ensure that both Council's views and user groups' needs are represented.

Attachments

Nil

Recommendation(s)

That Council nominates Councillor to Chair the Central Reserve Redevelopment Project Reference Group.

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**OM152503-4                      PROPOSED NAMING OF UNNAMED ROAD  
DOWNES ROAD, GERANGAMETE**

|             |                                |           |              |
|-------------|--------------------------------|-----------|--------------|
| AUTHOR:     | Paul Carmichael                | ENDORSED: | Colin Hayman |
| DEPARTMENT: | Corporate & Community Services | FILE REF: | F14/217      |

**Purpose**

To gain Council's approval to commence statutory procedures to name an unnamed road extending south east off Dewings Bridge Road, Gerangamete as "Downes Road".

**Declaration of Interests**

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

**Background**

The owner of a property abutting the subject road has requested it be named to provide them with a Rural Road Number address that can be used by emergency services and utilities to locate the property.

At present the applicant's property is addressed as 485 Dewings Bridge Road, Gerangamete but this address is no longer appropriate as 485 Dewings Bridge Road commences on the northern side of 545 Dewings Bridge Road, whilst the applicant's recently constructed dwelling on this property is physically located abutting the unnamed road reserve to the south of 545 Dewings Bridge Road. Emergency services attending 485 Dewings Bridge Road would therefore have difficulty locating the applicant's dwelling as they would be unlikely to progress past 545 Dewings Bridge Road.

The unnamed road reserve in question extends south easterly from 545 Dewings Bridge Road, Gerangamete for approximately 1.8kms to Seven Bridges Road. The road reserve contains a gravel road for approximately 340 metres from Dewings Bridge Road and this provides access to the applicant's residence. The remainder of the road reserve is overgrown with trees. Despite this, it is considered the entire road reserve should be named in case the trees are removed and other lots abutting the road reserve require Rural Road Number addresses.

It is proposed to name the road reserve as "Downes Road". It was proposed to use this name for this road in 2008 as part of a Road Naming Project, but it was decided at the time that a name was not required as it was essentially an access track to paddocks associated with the farm at 545 Dewings Bridge Road. The Downes family had an historical link to land in this vicinity.

In order to apply names to roads, Council is required to undertake a statutory procedure prescribed by the *Local Government Act 1989*. The procedure requires Council to advertise its intention to apply names to the road in question and consider submissions that may be received in response to the proposed name. Council is also required to write to property owners abutting the road to be named to advise them of the proposal.

Submissions to the proposed names are able to be lodged in accordance with the provisions of section 223 of the *Local Government Act 1989*.

### **Council Plan / Other Strategies / Policy**

#### **Good Governance**

Means we care about and are responsive to the community, encourage democratic participation and involve people in decisions that affect them. We strive for excellence in financial management and council services, and always look for better ways to do things.

#### *Our Goal:*

Ensure transparency of governance practices, the capability of our organisation and effective resource management.

#### **Issues / Options**

Available options are to:-

- (a) take no action, or
- (b) commence statutory procedures to rename the road reserve in question.

Given the possible problems for emergency service in locating the dwelling on the property, taking no action is not a feasible option.

#### **Financial and Other Resource Implications**

Costs incurred by Council to undertake this procedure would include advertising of the proposal and gazettal of the new name (totalling approximately \$600).

There will also be a cost for the preparation and erection of a street nameplate sign once the name is applied to the subject road.

#### **Risk Management & Compliance Issues**

Nil

#### **Environmental and Climate Change Considerations**

Nil

#### **Community Engagement**

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method selected would be consult and include:

- advising abutting owners of the proposal
- advertising the proposal
- seeking and consideration of submissions.

The proposal will be advertised in the Colac Herald on 10 April 2015 and Birregurra Mail and a period of 6 weeks will be allowed for submissions to be lodged. Submissions will close on 22 May 2015.

This complies with section 223 of the *Local Government Act 1989* and Council's Community Engagement Policy.

If a Special Council Meeting is required to consider any submissions it will be held on Wednesday 3 June at 1.00 pm at COPACC.

**Implementation**

The statutory process is expected to take up to six months to complete.

**Conclusion**

There is a need to apply a new Rural Road Number address for a recently constructed dwelling located on part of 485 Dewings Bridge Road, Gerangamete. As the new dwelling is accessed from an unnamed road reserve, it is necessary to apply a name to this road reserve. It is therefore recommended Council commence the statutory process to name the road reserve as "Downes Road".

**Attachments**

1. Naming of Road - Downes Road

**Recommendation(s)**

***That Council:***

- 1. Commences statutory procedures pursuant to clause 5, schedule 10 of the Local Government Act 1989 to apply the name "Downes Road" to the unnamed road extending south easterly from alongside 545 Dewings Bridge Road to Seven Bridges Road, Gerangamete.***
- 2. Advertises in the Colac Herald on 10 April 2015 and Birregurra Mail and submissions be invited in accordance with the provisions of section 223 of the Local Government Act 1989 and Council's Community Engagement Policy.***
- 3. Notes that submissions will be accepted for a 6 week period closing on Friday 22 May 2015.***
- 4. Resolves to hear and consider any submissions received pursuant to Section 223 of the Local Government Act 1989 at a Special Council Meeting on Wednesday, 3 June 2015 at 1.00pm at COPACC.***
- 5. In the event that no submissions are received, authorises the Chief Executive Officer, or her delegate, on behalf of Council, to do all things required to finalise the renaming of the road.***

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OM152503-5

**BLUEWATER FITNESS CENTRE REDEVELOPMENT
PROJECT - PROGRESS REPORT**

| | | | |
|-------------|---------------------------|-----------|--------------|
| AUTHOR: | Adam Lehmann | ENDORSED: | Phil Corluka |
| DEPARTMENT: | Infrastructure & Services | FILE REF: | F13/7721 |

Purpose

The purpose of this report is to provide Council with an update on the progress of the delivery of the Bluewater Fitness Centre Redevelopment Project.

Declaration of Interests

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

Background

The transformation of the Bluewater Fitness Centre is one of the most significant projects undertaken by Council. The Centre is a focus for sport and leisure within the Shire, and an enhanced facility will allow quality recreational activities to be delivered to the community for many years to come.

Council Plan / Other Strategies / Policy

A Planned Future

Creates an attractive shire with quality buildings and spaces, accessible travel and transport, and a community that has the services and facilities it needs now and in the future; supports a prosperous economy where trade, manufacturing and business activity flourishes.

Our Goal:

Facilitate the growth, liveability and development of the shire and encourage innovation and efficiency in the local economy.

A Place to Live and Grow

Is a community where people feel cared for and supported; where buildings and spaces facilitate creativity, social activity and enrichment of life, and people have access to gain the skills and education needed to reach their potential.

Our Goal:

Improve access to buildings, spaces, services and education to support and enable quality of life.

A Healthy Community and Environment

Actively connects and includes people of all ages and backgrounds and promotes a healthy and vibrant community life in a clean, safe and sustainable environment.

Our Goal:

Respect cultural differences, support a diverse range of healthy and creative activities, foster community safety and promote environmental sustainability.

Issues / Options

This report is prepared to provide timely and accurate information specific to the delivery of the Bluewater Fitness Centre Redevelopment Project.

Construction delays have occurred due to the need for design re-work. The full effect of the delay is currently being assessed with the contractor. Once the assessment is completed a new completion date will be set.

Proposal

This report is for Council to note the progress of this project and provides details relating to performance and variance against schedule, budget, and quality parameters. The report also gives an indication of the key issues and risks which have the potential to affect project delivery outcomes.

Financial and Other Resource Implications

The project budget is \$11.86M. The project is presently being managed within budget tolerances.

Risk Management & Compliance Issues

All aspects of the project are being managed through an established risk register. There are a number of key risks which have the potential to deviate the project from schedule, budget, and quality objectives. The major known issues relate to latent conditions associated with the condition of the existing building structure.

Environmental and Climate Change Considerations

Environmental risks posed by construction activities on site are being managed in accordance with the Contractor's Environmental Management Plan.

Community Engagement

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method used is to inform including ongoing community and stakeholder engagement activities which will be implemented throughout the course of the project. Information about the project will be disseminated to the community through traditional and social media outlets.

Implementation

This report is provided as information for the benefit of Council and the community and gives a status update on the delivery of the Blue Water Fitness Centre Redevelopment Project which is presently at construction stage.

Conclusion

The intention of this report is to inform the community about the progress of the Blue Water Fitness Centre Redevelopment project. These ongoing reports focus on monitoring of budget and expenditure, progress of works and issues or variances which have the potential to impact on project delivery outcomes.

| Monthly Project Status Report - BWFC Redevelopment | | | |
|--|---|-------------------|---|
| For Period | 1 February - 28 February 2015 | | |
| Prepared By | Graeme Collins and Adam Lehmann | | |
| Date Prepared | 04-Mar-15 | | |
| Project Description | Redevelopment of aquatic and sports facility to incorporate warm water pool, additional basketball court and compliant run off areas and general facility upgrades and renewal. | | |
| Project Status | | | |
| Status Item | Current Status | Prior Status | Reason for Deviation |
| Overall Project Status | Caution | Caution | |
| Schedule | Caution | Caution | Potential delays likely to affect forecast completion date |
| Scope | Controlled | Controlled | |
| Budget | Controlled | Controlled | Project presently within budget tolerances |
| Project Risk | Caution | Caution | Potential for further delays and associated costs |
| Overall Summary/Comments | | | |
| External wall cladding to the stadium is nearing completion. This scope of works has taken longer than originally planned. Other achievements over the past month include; completion of the concrete concourse around the new pool; and commencement of the installation of joinery in dry programme areas. | | | |
| Budget Performance | | | |
| <p>Building Contract Expenditure Summary</p> <p>Contract Sum</p> <p>\$10,955,860</p> <p>Contract Works Complete</p> <p>\$6,506,283</p> <p>Approved Variations Completed</p> <p>\$315,373</p> <p>Total Approved Works Completed</p> <p>\$6,821,656</p> <p>Contract Contingencies Credits and Contract Contingencies Remaining</p> <p>\$850,000 \$534,627</p> | | | <p>Project Expenditure Summary</p> <p>Total Project Budget</p> <p>\$11,855,860</p> <p>Total YTD Project Expenditure</p> <p>\$7,828,689</p> <p>Percentage Budget Spent</p> <p>66.0%</p> |
| <p>Expenditure Forecast vs. Actual</p> | | | |
| Comments | | | |
| Expenditure over the month of February shows a continued trend of increased site activity across the whole site with the expectation that this will continue over coming months. | | | |

| Schedule Performance | | | | | | |
|---|--|--|--|--|---|--------------------|
| Project Element | Design (including Contract Administration) | Relocation of Dry Programmes | Construction | Equipment Purchases for Redeveloped Facility | Landscaping | Project Management |
| Percentage Complete | 100.8% | 100.0% | 62.3% | 0.0% | 0.0% | 108.2% |
| Construction Correspondence | | | | | | |
| No. of Requests For Information (RFI) to Date | 320 | | No. Variation Quotations (VQ) Submitted | 152 | | |
| No. Unresolved RFIs No. Site Instructions Issued (SI) | 55 | | No. VQs Approved | 118 | | |
| 607 | | | | | | |
| Accomplishments Since Last Report | | | | | | |
| External wall cladding to stadium nearing completion
Wall linings in the stadium continue
Completion of concrete concourse around the new warm water pool
Painting of existing internal structure of the pool hall completed | | | | | | |
| Upcoming / Next Steps | | | | | | |
| Construction of subfloor within stadium area
Further assessment of existing concrete slab in foyer due to elevated moisture content and finish quality
Erection of pool hall steel framework
Continuation of high level work in stadium including lighting, fire services, wall linings, and windows | | | | | | |
| Key Risks that May Affect the Project | | | | | | |
| RFI response times | | | | | Current Status
Caution | |
| Latent conditions within existing building | | | | | Caution | |
| Possible delays due to seasonal change and extent of works exposed to weather | | | | | Controlled | |
| Integrity of existing pool structure and associated services | | | | | Caution | |
| Key Issues that Currently Affect the Project: | | | | | | |
| RFI pending response and current timeframes for finalisation | | | | | Current Status
Caution | |
| Documented design for roof over dry programme area - Constructability issues | | | | | Controlled | |
| Project schedule impacted by redesign and redocumentation delivery | | | | | Caution | |
| Progress of stadium cladding works | | | | | Caution | |
| Key Upcoming Milestones | | | | Target Completion Date | | |
| Completion of high level scope in stadium | | | | 31-Mar-15 | | |
| Erection of steel framework for pool hall | | | | 13-Mar-15 | | |
| Plaster linings to internal areas | | | | 31-Mar-15 | | |
| Key | | | | | | |
| Controlled | | - Project or issue is within established milestones, scope, budget and is on schedule | | | | |
| Caution | | - Project has slightly deviated from the plan.
- Issue/risk has the potential to deviate project from defined time, cost, quality objectives. | | | | |
| Critical | | - Project not tracking to established milestones, or has taken on tasks out of scope or budget.
- Issue or risk likely to significantly impact on schedule and project costs. | | | | |

Site Photos



Preparation of concrete concourse around warm water pool



Painting within existing pool hall



Internal timber lining to stadium



Completed concrete concourse

Attachments

Nil

Recommendation(s)

That Council notes the monthly status report for the Blue Water Fitness Centre Redevelopment Project covering the period to 1 February 2015 to 28 February 2015.

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**OM152503-6                      STRATEGIC ROAD REVIEW**

|             |                           |           |              |
|-------------|---------------------------|-----------|--------------|
| AUTHOR:     | Paula Gardiner            | ENDORSED: | Phil Corluka |
| DEPARTMENT: | Infrastructure & Services | FILE REF: | F11/1809     |

**Purpose**

To present the draft Colac Otway Shire Strategic Road Network Review to Council and seek resolution to commence public consultation.

**Declaration of Interests**

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

**Background**

Council has identified the need to conduct a review of the strategic freight routes within the Colac Otway Shire to assist in programming future strategic improvements of Council managed roads.

GHD Pty Ltd were commissioned to complete the review which involved consultation with freight operators, identification of strategic roads and the development of a report and cost estimate for the improvement of identified roads to a suitable standard. The consultation stage sought feedback from a range of industries including timber, construction, agriculture and tourism. The findings provided by this report are to be referenced in the development of Council's future capital works programs.

**Council Plan / Other Strategies / Policy**

**A Planned Future**

Creates an attractive shire with quality buildings and spaces, accessible travel and transport, and a community that has the services and facilities it needs now and in the future; supports a prosperous economy where trade, manufacturing and business activity flourishes.

*Our Goal:*

Facilitate the growth, liveability and development of the shire and encourage innovation and efficiency in the local economy.

**Issues / Options**

The Strategic Road Review has been developed to identify Council managed roads that are of significant strategic importance for a range of industries within the Colac Otway Shire. This document will assist Council in providing roads of a standard appropriate for commercial vehicles using identified routes in Council's road network.

It is considered important for Council to have strategic direction with respect to road network improvements to ensure consistency in the development and delivery of projects which will improve the standard of the road network.

**Proposal**

That Council reviews and endorses the Colac Otway Shire Strategic Road Review for public consultation.

### **Financial and Other Resource Implications**

Contained within the report is a list of five (5) routes recommended for road improvement works. It is proposed that these works be considered for funding within Council's 10 year Capital Works and Major Projects Program, however funding from State and Federal Government may be possible for some upgrades.

### **Risk Management & Compliance Issues**

The hazards associated with Council managed roads include insufficient clear zones, narrow seal widths, lack of road shoulders and insufficient pavement strengths for commercial vehicles. This document will assist Council in better managing these associated risks.

### **Environmental and Climate Change Considerations**

The report identifies removal of roadside vegetation and significant earthworks required to meet minimum required road widths specified in relevant road design guidelines. Environmental issues will be considered and appropriate controls implemented as each project is delivered.

### **Community Engagement**

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method selected would be consult and include exhibiting the draft Strategic Road Network Review and inviting comments and submissions from the public.

Advertisements will be placed in the local media on 3 April 2015 inviting the public to comment on the draft document. The public review period will be six (6) weeks, beginning on 3 April 2015 and concluding on 14 May 2015. The draft document is to be placed on Council's website, with hard copies in the Colac and Apollo Bay Customer Service Centres and the Colac Community Library and Learning Centre.

Submissions received by Council during this review period will be assessed by Council officers and incorporated into the document where considered appropriate.

### **Implementation**

Following Council's consideration of the draft Strategic Road Network Review, Council officers will commence the public consultation process by placing copies of the draft document in the Colac and Apollo Bay Customer Service Centres and the Colac Community Library and Learning Centre. The document will also be made available on Council's website. Advertisements within the local media will be placed to ensure the public are aware that the draft document is available for comment through the consultation period. A minimum of six (6) weeks will be given for lodgement of written submissions.

### **Conclusion**

The intention of the Strategic Road Network Review is to identify the Council managed roads that are most important to freight movements within the Shire and possible improvements to these strategic road linkages. This will enable Council to focus funding towards improving access and transportation for industry within the Shire.

By endorsing this document for public exhibition and comment, Council will provide the opportunity for community input to further refine the document's recommendations.

### **Attachments**

1. Colac Otway Shire Strategic Road Network Review

**Recommendation(s)**

***That Council:***

- 1. Endorses the draft Strategic Road Network Review for public consultation; and***
- 2. Places the draft Strategic Road Network Review on exhibition for a period of six (6) weeks commencing 3 April 2015 and concluding 14 May 2015.***

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OM152503-7 CONTRACT 1433 - SUPPLY AND DELIVERY OF GRADER

AUTHOR:	Andrew Kavanagh	ENDORSED:	Phil Corluka
DEPARTMENT:	Infrastructure & Services	FILE REF:	11/96582

Purpose

Tenders have been received for Contract 1433 – Supply and Delivery of Grader which requires Council approval to award the contract.

Declaration of Interests

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

Background

Tenders closed on 28 January 2015 for the supply and delivery of a grader.

A Caterpillar 120H grader registered TGN 882 purchased in December 2004 is proposed to be traded for the new plant.

The following tenders were received:

Tenderer	Make/Model
CJD Equipment	Volvo G930
Komatsu Australia	GD555_5
Hitachi Construction Machinery (Australia)	John Deere 670G
Hitachi Construction Machinery (Australia)	John Deere 670GP

Tenders were evaluated and a recommendation made in accordance with Council's *Procurement Policy and Tenders/Quotations and Purchasing Procedure*.

All tenders were evaluated and scored using the following selection criteria:

<u>Criteria</u>	<u>Weighting</u>
Tendered price	50
Suitability of vehicle tendered	10
Performance of vehicle tendered	10
Authorised distributor of product	10
Vehicle availability	5
Spare parts – availability and support	5
Warranty offered	5
Local content	5

The Tender Evaluation Panel (TEP) consisted of the following members:

Cosworks Manager

Cosworks Team Leader – Maintenance and Construction

Cosworks Supervisor Mechanic
Contracts Officer.

The TEP recommended that the **Hitachi Construction Machinery (Australia)** submission for the **John Deere 670GP** at a cost of \$390,176.50 (excluding GST, stamp duty and registration costs) provided the best value and is the highest scoring tender. The vehicle conforms fully with the tender specifications.

The TEP further recommended accepting the trade-in offer of \$65,000 (excluding GST) from Hitachi Construction Machinery (Australia). Council was unable to secure an auction estimate for the Caterpillar grader offered for trade-in at the time the request for tender was issued. However, using the trade-in offers from the other tenderers as a market indicator the trade-in offer by Hitachi was assessed as offering best value to Council.

Council Plan / Other Strategies / Policy Good Governance

Means we care about and are responsive to the community, encourage democratic participation and involve people in decisions that affect them. We strive for excellence in financial management and council services, and always look for better ways to do things.

Our Goal:

Ensure transparency of governance practices, the capability of our organisation and effective resource management.

Issues / Options

The existing plant needs to be replaced due to age and condition. Ongoing maintenance and reliability issues will become worse over time. This will impact on the operations of Cosworks and road maintenance works.

Proposal

It is proposed that Council dispose of the existing Caterpillar grader and purchase a suitable replacement as recommended. It is further proposed that Council accept the trade-in offer of the preferred tenderer.

Financial and Other Resource Implications

The change-over cost for the grader is within and is in line with the plant replacement budget requirements.

Total plant replacement program includes allowance for replacement plant and income from plant traded or sold at auction.

The change-over costs of the proposed new plant are within the available funds.

Risk Management & Compliance Issues

Purchase of the new plant is in accordance with Council's occupational health and safety requirements where equipment has been specified and reviewed. Necessary documentation is to be provided with the new plant on delivery.

Environmental and Climate Change Considerations

No outstanding environmental issues have been raised.

Community Engagement

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method selected was to inform. The tender was advertised in The Age and Colac Herald on 5 and 6 December 2014. The tender was also advertised on the Colac Otway Shire website and via Tenderlink.

Implementation

Upon Council's approval the Contract will be awarded and the new plant purchased. Delivery is expected within six (6) weeks after notifying the successful tenderer.

Conclusion

A recommendation is made to award the contract to Hitachi Construction Machinery (Australia) for purchase and trade-in. The changeover sum is within the budget.

Attachments

Nil

Recommendations

That Council:

- 1. Awards Contract 1433 for Supply and Delivery of one John Deere 670GP at the changeover price of \$325,176.50 (excluding GST, stamp duty and registration costs) inclusive of trade-in.***
- 2. Delegates authority to General Manager, Infrastructure & Services to sign the contract documents following award of Contract 1433.***

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**OM152503-8                      CONTRACT 1509 - SAND ROAD BRIDGE REHABILITATION**

|             |                           |           |              |
|-------------|---------------------------|-----------|--------------|
| AUTHOR:     | Roger Fox                 | ENDORSED: | Phil Corluka |
| DEPARTMENT: | Infrastructure & Services | FILE REF: | F11/3291     |

**Purpose**

Council approval is required to award Contract 1509 – Sand Road Bridge Rehabilitation.

**Declaration of Interests**

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

**Background**

The Sand Road Bridge is a 75 metre multi span timber bridge spanning the Aire River in Glenaire. It connects Sand Road with the Horden Vale Road and services a Parks Victoria camp ground while also forming a critical link in the Great Ocean Walk. The bridge is currently subject to a two tonne load limit and is nearing the end of its serviceable life. The bridge was closed for a period in 2014 following a failure of a structural member of the fifth pier. The bridge was subject to a five tonne load limit at the time.

Council undertook repair works designed by GHD Pty Ltd which facilitated the reopening of the bridge at the lower load limit of two tonne. Council also erected barriers to prevent the passing of larger vehicles such as four wheel drives and coaches over the bridge.

Further works are required due to the ongoing deterioration of critical structural elements as identified by the GHD report. It is proposed that the bridge be rehabilitated to a five tonne load limit with a ten year design life. This provides Council with the best economic value from a number of available options. Council will need to close the bridge to all users in the near future unless identified reinstatement works are undertaken due to the risks posed by its failing condition.

Tenders for the work closed on 25 February 2015. The contract is a lump sum which involves the rehabilitation or replacement of various structural members on the sub-structure and super structure to extend the life of the existing bridge. The deck has previously been repaired and needs no further works. Specific repairs are to 14 piles, 16 capwales, 12 girders and eight corbels as well as the splicing of two piles.

Practical completion is to be achieved by 30 June 2015.

A provisional sum of \$10,000 is included for day works which will be deducted from the contract sum if not utilised.

A tender was received from a single contractor:

**Timber Restoration Systems Pty Ltd**

The tender was evaluated and a recommendation made in accordance with Council's Procurement Policy and Tenders/Quotations and Purchasing Procedure.

The Tender Evaluation Panel consisted of the following members:  
Contracts Coordinator  
Manager Sustainable Assets  
Development Engineer.

Tenderers must also satisfy Colac Otway Shire Council that they meet requirements for:

- Occupational health & safety
- Financial viability.

The submission by Timber Restoration Systems Pty Ltd was assessed by the tender evaluation panel as an acceptable tender and ensured the price was accurate and reflected a fair value of the works to be undertaken. An independent estimate of the price of works showed the tendered price to be within eleven percent of the value of works initially proposed by Council's consulting engineers, GHD Pty. Ltd. The tender price is considered to represent value for money.

The methodology Timber Restoration Systems Pty Ltd proposed to undertake the works is different to that as specified in the documents but is considered equally suitable to what was initially intended for the contract. The approach proposed has been developed in response to the GHD design using methods in which the contractor specialises.

The methodology proposed by the contractor involves the contractor's in-house design team utilising expert procedures to measure and restore structural integrity of the bridge.

The design approach is checked and refined against Australian Standard AS5100 - Bridge Design Code. GHD has indicated that the methods meet industry standards and that the method proposed is appropriate.

The tenderer has a proven track record of expertise in successfully restoring timber structures with particular experience in timber bridges. The tenderer also offers guarantees on all work including a load rating and durability guarantee.

Timber Restoration Systems has indicated that it would start works by 13 April 2015 with three or four weeks to completion.

### **Council Plan / Other Strategies / Policy**

#### **Good Governance**

Means we care about and are responsive to the community, encourage democratic participation and involve people in decisions that affect them. We strive for excellence in financial management and council services, and always look for better ways to do things.

#### *Our Goal:*

Ensure transparency of governance practices, the capability of our organisation and effective resource management.

Tenders were evaluated in accordance with Council's Procurement Policy and Tenders/Quotations and Purchasing Procedure.

This is a project contained within the Council 2014/15 Capital Works Program and is in accordance with Council's long term plans for road asset renewal.

#### **Issues / Options**

Only one tender submission was received.

It is considered necessary to award the contract and proceed with rehabilitation works of the bridge as failure to do so would result in the enforced closure of the bridge. This is due to the advanced deterioration of several critical structural members. Council advertised for the services of providing the preferred option which was selected from a suite of options provided by GHD which included a bridge replacement option valued at approximately \$3 million. The preferred option was assessed as being best value to Council. The five tonne load limit will provide an improved level of fire-fighting service to the area as it allows the safe passage of a "slip-on vehicle" between Sand Road and Horden Vale Road. Full replacement of the bridge is not financially feasible at this point. A new pedestrian bridge is valued at \$2 million which includes the cost of removing the existing bridge. Due to poor ground conditions, rehabilitation past the five tonne limit is not an option.

The submission is within the 2014/2015 budget for the works and is in compliance with Council's commitment to maintain existing assets to a serviceable level.

### **Proposal**

It is proposed to accept the tender and engage the recommended contractor to complete the rehabilitation works this year.

The tenderer has indicated an ability to commence works from 13 April 2015 with site-works completed in three to four weeks.

The Tender Evaluation Panel selected Timber Restoration Systems Pty Ltd as the recommended tenderer as their submission was best value for Council.

### **Financial and Other Resource Implications**

The tender of Timber Restorations Pty Ltd at \$435,890.45 (excluding GST) is within the available budget of \$500,000 (excluding GST).

This project forms part of Council's Bridge Renewal Program and is being funded from Council's allocation under the State Government's Country Roads and Bridges Program.

### **Risk Management & Compliance Issues**

The condition of the Sand Road Bridge poses a significant risk to Council. Delaying or deferring the proposed works exposes Council, as the asset custodian, to possible damage to reputation and liability claims relating to the use of the bridge. Council has an obligation to provide safe and functional infrastructure to our community.

The project planning has included consultation with Parks Victoria, the Corangamite Catchment Management Authority (CCMA), the CFA and Kuuyang Maar Aboriginal Trust.

The preferred tenderer, Timber Restorations Pty Ltd has a good track record of achieving projects on time and within budget while managing the risks. The company has provided details to confirm they have in place an effective, suitable occupational health and safety system.

### **Environmental and Climate Change Considerations**

The Contractor is required under the contract to provide a suitable, approved environmental plan which will actively prevent incidents and occurrences of detrimental impacts to the environment.

The contractor has been provided with environmental background information specific to the site as part of the tendering information.

Prior to tendering Council's Infrastructure Environment officer undertook an investigation into the environmental risks and protection required.

**Community Engagement**

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method selected was to inform. The tender was advertised in the Colac Herald 23 January and the Geelong Advertiser on 24 January 2015. The tender was also advertised on the Colac Otway Shire website and via Tenderlink.

Prior to commencement of works on site and the closure of the bridge for the duration of the works a public notice is to be placed in the local newspaper to advise of pending operations and appropriate signage on site to advise users.

Parks Victoria is aware of the arrangements and its impact on the camping grounds. The bridge is not required for private property access.

**Implementation**

Upon Council's approval, the Contract will be awarded and works will be programmed to commence. If successful, Timber Restorations Systems Pty Ltd proposes to commence works in early April 2015 following notification of award of the contract and to complete the works within approximately three to four weeks.

**Conclusion**

A recommendation is made to award the contract to Timber Restoration Systems Pty Ltd for the rehabilitation of the Sand Road Bridge, Glenaire.

**Attachments**

Nil

**Recommendation(s)**

***That Council:***

- 1. Awards Contract 1509 for Sand Road Bridge Rehabilitation to Timber Restoration Systems Pty Ltd at the lump sum tender price of \$435,890.45 (excluding GST).***
- 2. Delegates to the Chief Executive Officer authority to sign and place under Council seal the contract documents following award of Contract 1509.***

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**OM152503-9 SMALL TOWN IMPROVEMENT PROGRAM STIP 2014/2015 -
 ALTERNATIVE PROPOSALS FOR BIRREGURRA**

AUTHOR:	Katrina Kehoe	ENDORSED:	Doug McNeill
DEPARTMENT:	Sustainable Planning & Development	FILE REF:	11/95454

Purpose

The purpose of this report is for Council to consider a Small Town Improvement Program (STIP) replacement project in Birregurra. A resolution of the June 2014 Council meeting approved the carry-over of 2013/2014 STIP funding of \$20,000 for Birregurra and \$20,000 for Apollo Bay to be allocated to replacement projects in those towns to be completed in 2014/2015, subject to Council approval of the specific projects.

It is also proposed that Council reviews the STIP guidelines to guide the process for proposals which are not able to proceed within the year they are allocated.

Declaration of Interests

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

Background

The original proposal was to construct a pathway along Roadknight Street, Birregurra, between the railway station and Main Street - a distance of 930 metres. The Birregurra Community Group lodged a proposal with Council in 2012 that the path be constructed using funds from Council's Small Town Improvement Program (STIP). The money available from Council was well short of that required. In order to attract further State Government funding the proposal was increased beyond the 1.5m width of a standard footpath to a shared trail 2 metres in width. Regional Development Victoria (RDV) had indicated that municipal footpaths could not be funded, but funding might be available for shared trails. However, RDV did not support the grant proposal for a staged pathway, which was the only option open to Council with the amount of STIP funds available to use for matching funds.

The Apollo Bay funding was reallocated at the December 2014 Council meeting to support the Apollo Bay Fisherman's Co-operative to construct a viewing platform at the Harbour. The Birregurra community had not submitted an alternative proposal by the December meeting, but had been consulting Council officers about the use of the funds on various streetscape improvements in Main Street.

The STIP guidelines take into account whether a proposal has broad community support. As such, a community drop in session was held on Main Street Birregurra on Saturday 15 November 2014 to consult with the community over an alternative proposal being considered for streetscape improvements in the Main Street. Diagrams were on display with several styles of seating, bin enclosures, bike racks, line marking options and garden beds for community members to comment on. The line marking, garden beds and seating on the south side of Main Street have not been included in this application. The seating proposed is to upgrade the seating in the median garden strip on the north side of the street in a similar style to the existing seats.

Thirty two written responses were received with their preferred styles of street furniture recorded. Of these responses, seven did not support the proposal and stated their priority is the Roadknight Street pathway.

A petition was subsequently received by Council containing 219 signatures advocating that the STIP funds be redirected to the pathway project, and opposing the use of the funds for changes to the Main Street streetscape. The petition called for any future changes to the streetscape to undergo thorough consultation with stakeholders and Council's heritage adviser. The petition was tabled at the 26 November 2014 Council meeting.

A report was submitted to the December 2014 Council meeting to seek approval of the alternative project for Birregurra for use of the \$20,000 STIP funds and to note the petition. The resolution made in regards to the Birregurra project was as follows:

"That Council:

1. Allows the Birregurra community to submit an alternative proposal to use the \$20,000 which was carried forward from the 2013/2014 budget subject to Council approval of the specific project. A proposal should be received at Council offices by the end of January 2015, to allow the STIP referral processes to be followed and a report to be presented to the March 2015 Council meeting.
2. Notes the petition and writes to the organiser of the petition advising that:
 - a. Council acknowledges that the construction of a footpath along Roadknight Street is a priority for the Birregurra community.
 - b. There is no funding available for the construction of the entire footpath at this time and any future consideration would need to be in the context of council's future budget processes.
 - c. Council will continue to seek future external grant opportunities to fund the Roadknight Street footpath".

The Roadknight Street pathway project in Birregurra was to construct a shared path between the railway station and Main Street in an exposed aggregate finish consistent with the outcomes of the adopted Neighbourhood Character Study (the estimated cost of the pathway is up to \$317,000). Council was unable to attract the significant external funding required to construct the path, or for a shorter section of path if the project was staged over time.

Significant additional funding would have been required from Council to attract a State Government grant. The additional funding required was unbudgeted and would have been more than the value of the entire STIP program. Despite the footpath being one of the clear priorities for the Birregurra community, Council could not proceed with the project under the STIP program.

Council's position was to offer the community of Birregurra the opportunity to develop an alternative project to utilise the 2013/2014 STIP funding as an alternative to abandoning the funding opportunity.

Council Plan / Other Strategies / Policy

A Planned Future

Creates an attractive shire with quality buildings and spaces, accessible travel and transport, and a community that has the services and facilities it needs now and in the future; supports a prosperous economy where trade, manufacturing and business activity flourishes.

Our Goal:

Facilitate the growth, liveability and development of the shire and encourage innovation and efficiency in the local economy.

Issues / Options

Streetscape improvements

The Birregurra Community Group had been working on a proposal to install seating, bin enclosures and bike racks along the Main Street between Strachan and Austin streets (ie: the main commercial area). Some members of the community have strongly indicated they did not support this proposal as they believe it takes resources away from a possible footpath to the railway station and may interfere with heritage elements of the streetscape. Council's heritage advisor has met with these community members and indicated to them that there is no concern from a heritage perspective with installing new, appropriately chosen, street furniture.

The streetscape application links with two strategic documents for Birregurra

- The Birregurra & Forrest Township Infrastructure Plan 2012 (refer section 4.1)

“4.1 Birregurra

1. Construction of a paved footpath to the train station.
2. Landscaping enhancements at the thresholds to the township.
3. Pause place to define the western threshold to the town centre.
4. Streetscape enhancements in front of shops to include seating, bicycle racks and landscaping.
5. Part of school site utilised for a community garden.²

2 The community garden was identified as a priority during the community engagement process. However, it is not supported by the Public Open Space Strategy and other Council strategies and plans.

- The Birregurra Structure Plan 2013 (refer section 14.6)
 - “Improve the pedestrian environment along Main Street, including the introduction of seating, lighting, rubbish bins etc.
 - Provide bike racks in Main Street in convenient and accessible locations. Bike racks should be functional but also innovative in design”.

The Birregurra community were extensively consulted during the development of these plans, and both received relatively strong community support at the time.

The proponents of the both the alternative STIP applications received, were members on the Community Reference Group during the development of the Birregurra Structure Plan.

Part Construction of Roadknight Street footpath

A separate STIP application was also received from the “Birregurra Traders & Friends Association” for Stage 1 of a footpath along Roadknight Street from Main Street to the Railway Station. This would mean that a footpath to the value of \$20,000 would be constructed (approx 103m if in gravel, 69m if in plain concrete, or 58m in exposed aggregate finish), leaving the significant majority of the 930m distance between the station and Main Street without a footpath connection.

This is not considered a realistic or viable option for Council as it would only achieve a very small section of footpath, and it is clear that the funding to achieve the full path is not available to Council from external sources, and there is no Council budget for the significant funds required for that project in 2014/15. This issue was considered at the June 2014 Council meeting and again at the December 2014 Council meeting when Councillors considered the petition relating to the footpath, and Council resolved to direct the funds to an alternative project.

The construction of a gravel surface footpath along Roadknight Street to the Railway Station is not supported by the Infrastructure Design Manual that Council applies as its standard for infrastructure provision, along with a significant number of other rural and regional Victorian Councils. This Manual ensures good practice in both tendering and compliance with regulatory instruments such as the Disability Discrimination Act (DDA) 1992. The advice concerning footpaths in the Manual draws on material found in Australian Standard 1428 parts 1 and 2, and overseas guidelines and standards.

For example, the direction (or standard) in the Manual draws on Figure 2 of AS1428.2 that shows the minimum width required for 2 people using wheelchairs to pass each other is 1.8m.

The Manual is not stating that compliance with the DDA can only be achieved by providing footpaths this wide, but rather a footpath of this minimum width would provide amenity for all users, especially in areas of high pedestrian traffic such as High Street shopping areas, and would likely avoid the possibility of successful complaints that could incur significant legal and rectification costs for Council.

Section 13.3 of the Manual sets out the requirements of footpaths as follows:

“13.3 Requirements

The following requirements apply to footpaths and pedestrian accesses:

- The minimum footpath width to be 1.5m in residential areas and 2.0 metres in commercial areas.
- Footpaths must be constructed of either concrete or asphalt, and must conform to Australian Standards *AS/NZS1428.4, 2009 'Design for Access and Mobility – Means to Assist the Orientation of People with Vision Impairment – Tactile Ground Surface Indicators'* for surface finish.”

The Australian Standard does not specify a particular surface finish for footpaths, but Council's Manual has specified concrete or asphalt as a surface which would satisfy the universal access requirements of the Standard. It is worth noting that the manual has been developed in consultation with disability access advocacy groups and is therefore well guided by the principles and requirements of the Disability Discrimination Act.

Council is able to consider the merits of funding the footpath from its capital works program in future years, against the range of other infrastructure priorities, including the priorities for pathways in other towns such as Apollo Bay and Colac.

The “Birregurra Traders & Friends Association” has indicated that the organisation is an independent unincorporated group with a particular focus on preserving the heritage precinct of Main Street Birregurra. Officers were not aware of this group prior to receiving its submission and the proponent did not furnish any further details as to membership of this group despite requests to do so.

Options

The following options are open to Council regarding this funding:

- Option 1 – Fund the development of Streetscape Improvements as proposed by the Birregurra Community Group (recommended).

This option is consistent with the community support element of the guidelines and can be delivered within time and budget constraints.

The project has strategic support, having been identified as a priority in the recent Birregurra Community Infrastructure Plan for Birregurra.

- Option 2 – Fund the partial construction of a footpath on Roadknight Street (not recommended).

While community support has been demonstrated for a footpath in Roadknight Street the STIP program can only fund an inconsequential portion of the path that will deliver negligible community benefit. Partial construction would also represent the most expensive way to deliver this path. Funding for the full path could be considered by Council through future budget processes.

- Option 3 – Rollover the STIP allocation to next financial year (not recommended). I.e; carry over the funds to increase the 2015/16 allocation for STIP from \$100,000 to \$120,000, and consider new projects through the normal allocation process.

Given that there is an alternative project that is well supported and can be delivered in full and within time and budget constraints, this option is not supported. There would be no guarantee that the rolled over funds would be allocated to a Birregurra project.

- Option 4 – Reallocate the unspent STIP funds back to the budget this financial year (not recommended).

Given the amount of community and Council time invested this financial year in examining alternative projects it would not be desirable to return these funds to the broader budget (and away from a community project in Birregurra). However, if Council does not support the streetscape improvements proposal, this is a valid option.

Whilst Council has the option of funding a small section of footpath in Roadknight Street, this is not considered a viable option. Officers recommend that the STIP funds be allocated to the streetscape improvement works project.

Suggested Changes to the STIP Guidelines

A key learning from the STIP process in the past twelve months has been that where a project has not proved viable after allocation of the funds by Council, there have been difficulties in working with the community to establish a clear priority/scope for an alternative project within a time span that enables proper consideration of the proposal by officers and completion of the works within that financial year. It is suggested that the STIP guidelines could be amended to include the following suggested changes:

“Should a project not be able to proceed, for example, where an application for external grant funds has been unsuccessful, the funds shall be returned to the budget and the community can apply for new STIP allocations in the following year”.

This will avoid the situation where the community is trying to develop an alternative proposal in a short time span and reducing the time available to deliver the alternative project if approved.

Proposal

Two proposals have been received from different groups within the Birregurra community for the use of the STIP funds of \$20,000 which Council held over for Birregurra to complete a project in 2014/2015.

Whilst Council has the option of funding a small section of footpath in Roadknight Street, that project is not considered viable, and the recommended option is to support the streetscape improvement works advocated by the Birregurra Community Group, which have strong community support in Council led strategic studies over recent years.

It is also proposed that the STIP Guidelines be amended as suggested to address circumstances where a funded project is unable to proceed.

Financial and Other Resource Implications

STIP funds of \$20,000 have been carried over in the budget from 2013/2014 to enable an alternative project to be undertaken within the previously funded community of Birregurra.

The proposal for the upgrade to the rotunda and seating, along with the installation of bike racks and bin enclosures is ready to proceed if approved.

The proposal for the Roadknight Street footpath to stage the construction of a footpath would only allow for approximately 58m of footpath if constructed in exposed aggregate and 69m if constructed in concrete (as calculated against the full cost estimates of the footpath).

Risk Management & Compliance Issues

All projects undertaken as part of the Small Town Improvement Program follow Council's Risk Management processes, including preparation of a Risk Assessment Form and a completed and signed Job Safety Analysis/ Safe Work Method Statement from suitable qualified contractors engaged to complete the work on Council's behalf. Planning and building requirements, as well as cultural heritage and environmental issues, are considered and where required, appropriate permits are sought prior to implementation.

Environmental and Climate Change Considerations

All works to be undertaken will be completed following discussions with Council's Environment and Community Safety, Infrastructure, Building and Planning departments to ensure all issues are assessed.

Community Engagement

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower. The method selected is to inform, consult or involve, depending on the type of project.

Birregurra Streetscape improvements

A community 'drop in' session was held on 15 November 2014 to gain the community's input into a proposal which was being developed for the style of seating, bin enclosures and bike racks along the Main Street.

The Birregurra Community Group has further sought input from the community through the Birregurra Mail, a newsletter circulated by email to group members and notices placed on the community noticeboards at the General Store and the Post office.

The proposal has been referred to the Shire's Heritage Advisor who is supportive of the proposed styles and colours of bins and bike racks, and has suggested that they are placed in discrete locations so they do not obstruct views to significant features of the precinct. The proposed upgrade to the rotunda and existing seating is supported from a heritage perspective as they will improve the overall appearance of the streetscape.

The Birregurra Community Group is represented in the community with seven subgroups – Festival and Markets, Aged Friendly Community Project, Business and Tourism Group, Cycling and Walking, Land Care and Gardens, Country Women's Association and the Arts Group.

It should be noted that these works have been identified as a priority for Birregurra in several strategic plans/studies, including the Birregurra Structure Plan 2013 and the Birregurra Community Infrastructure Plan 2012, thus having strong community buy-in and strategic basis.

Roadknight Street footpath

As noted earlier, a petition was received by Council advocating that the STIP funds be redirected to the pathway project, and opposing the use of the funds for changes to the Main Street streetscape. The petition was considered at the 17 December 2014 Council meeting, with the resolution as noted above in the 'background section' of this report. It is noted that since that date, requests have been received from some individuals to remove their name from the petition.

The pathway along Roadknight Street is an identified priority in a number of local plans for the Birregurra community, however Council funds from the Small Town Improvement Program (STIP) are well short of the amount required to have a path constructed. It is clear now that this program is not suitable to fund this project as a staged approach is not viable or supported.

Implementation

The Economic Development Unit will coordinate the project in consultation with the community group, other applicable Council departments and State and Federal Government agencies.

Conclusion

It is recommended that Council approves the application for the upgrade to the rotunda and existing seating and the installation of bike racks and bin enclosures. The Birregurra community has had the opportunity to have input into the style of the bike racks and bin enclosures and the Heritage Advisor has been consulted. The rotunda and seating will be upgraded in a similar style to that which exists now. The proposal is consistent with recommendations within the Birregurra Community Infrastructure Plan 2012 and the Birregurra Structure Plan 2013, which were undertaken with community engagement.

The Birregurra community were allocated \$20,000 in 2013/2014 for the Roadknight Street pathway project, which was unable to commence due to an inability to obtain the external grant funds required to proceed. If the streetscape improvement proposal is not supported, there is no other viable project for Council to consider, and Birregurra may lose the opportunity for use of these funds.

The Roadknight Street footpath has previously been considered at the Council meeting held on 17 December 2014. There is no realistic option to redirect the carryover funds for Birregurra back to the footpath project as it is clear that the funding to achieve that project is not available to Council from external sources at this time and there is no Council budget for the significant funds required for that project in 2014/15. Council is able to consider the merits of funding the footpath from its capital works program in future years, against the range of other infrastructure priorities. Council will ***continue to seek future external grant opportunities to fund the Roadknight Street footpath.***

Amendments to the STIP guidelines are recommended to provide clarity for future STIP processes.

Attachments

1. Small Town Improvement Guidelines 2015

Recommendation One

That Council writes to the applicants of the Roadknight Street footpath proposal advising that:

- a) Council acknowledges that the construction of a footpath along Roadknight Street has been identified as one of a number of priorities for the Birregurra community.***
- b) The maximum \$20,000 STIP allocation is insufficient to cover the significant cost of a footpath along Roadknight Street Birregurra and STIP is not a suitable program to fund this proposal at this time or in the future. There are no other sources of capital works funding available for the construction of a footpath at this time and any future consideration would need to be in the context of Council's future budget processes.***
- c) Council will continue to seek future external grant opportunities to fund the Roadknight Street footpath, as it will with other footpaths identified as priorities across the Shire.***

Recommendation Two

That Council:

- a) Notes that the proposal by the Birregurra Community Group, to upgrade the Rotunda, existing seating and installation of bike racks and bin enclosures is consistent with the outcomes of the Birregurra Community Infrastructure Plan and the Birregurra Structure Plan which were both endorsed by Council after extensive community consultation; and***
- b) Notes that the elements of the Birregurra Community Group proposal have been the subject of further community consultation conducted by the Birregurra Community Group and through a drop in session conducted by Council officers, wherein the majority of those consulted continued to support the project elements identified in the proposal; and***
- c) Notes that if the Birregurra Community Group proposal is unsuccessful then the STIP funding will not be allocated.***
- d) Approves the application by the Birregurra Community Group, to upgrade the Rotunda, existing seating and installation of bike racks and bin enclosures, as an alternative project for expenditure of the \$20,000 STIP funds for Birregurra.***

Recommendation Three

That Council endorses changes to the Small Town Improvement Program (STIP) Guidelines as follows:

"Should a project not be able to proceed, for example, where an application for external grant funds has been unsuccessful, the funds shall be returned to the budget and the community can apply for STIP allocations in the following year".

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**OM152503-10                      PLANNING SCHEME AMENDMENT C73 PANEL REPORT  
(APOLLO BAY HARBOUR)**

|             |                                       |           |              |
|-------------|---------------------------------------|-----------|--------------|
| AUTHOR:     | Blaithin Butler                       | ENDORSED: | Doug McNeill |
| DEPARTMENT: | Sustainable Planning<br>& Development | FILE REF: | F13/4504     |

**Purpose**

To consider the Panel Report received for Planning Scheme Amendment C73, which proposes to apply the Special Use Zone Schedule 2 (SUZ2) to the Apollo Bay Harbour and surrounds and provide a coherent planning framework for the detailed master planning exercise to follow.

**Declaration of Interests**

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

**Background**

Redevelopment of the harbour, to provide improved boating and recreational facilities for residents and visitors whilst protecting the heritage, landscape and public realm assets that make the harbour attractive, has been a long term goal for Council and the community. Amendment C73 seeks to facilitate the redevelopment of the Apollo Bay Harbour by rezoning the harbour from part Public Park and Recreation Zone (PPRZ) and Public Conservation and Resource Zone (PCRZ) to Special Use Zone, with Schedule 2 (SUZ2) giving specific direction on how such redevelopment should occur.

Amendment C73 was initially exhibited in late 2013, with a Master Plan for the harbour to be an Incorporated Document and the harbour being rezoned to SUZ2. In response to submissions received to that exhibition, Amendment C73 was significantly revised and re-exhibited for six weeks from August to September 2014. Following consideration of submissions, Council changed the amendment to remove reference to the Master Plan, delete the potential for accommodation, allow greater flexibility within the Special Use Zone (with capacity to consider a swimming pool, and health and wellbeing uses), and to require the preparation of a Development Plan detailing how the precinct would be developed.

Thirty-five submissions supported the re-exhibited amendment and/or requested a change, and five objected to it. It was clear from the submissions that there is widespread community support for the re-exhibited amendment on the basis it maintains the prohibition on the hotel/accommodation and allows Council to consider the possibility of an indoor pool and health and wellbeing facility in the future (i.e. it would no longer be prohibited).

Council resolved at its October 2014 meeting to refer all submissions to an independent Planning Panel. Ten of the submitters elected to be heard at the Panel, which was held in December 2014. The resulting Panel Report was submitted to Council on 20 February 2015. Council is required to make a decision on the Panel's recommendations within 40 business days of receipt of the Panel Report (i.e. 22 April 2015). Council is also required to publicly release the panel report within 28 days of its receipt (i.e. by 20 March).

## **Council Plan / Other Strategies / Policy**

### **A Planned Future**

Creates an attractive shire with quality buildings and spaces, accessible travel and transport, and a community that has the services and facilities it needs now and in the future; supports a prosperous economy where trade, manufacturing and business activity flourishes.

#### *Our Goal:*

Facilitate the growth, liveability and development of the shire and encourage innovation and efficiency in the local economy.

### **A Place to Live and Grow**

Is a community where people feel cared for and supported; where buildings and spaces facilitate creativity, social activity and enrichment of life, and people have access to gain the skills and education needed to reach their potential.

#### *Our Goal:*

Improve access to buildings, spaces, services and education to support and enable quality of life.

## **Issues / Options**

This report outlines the findings and recommendations set out in the Panel Report on Amendment C73, and assesses the merits and implications of each of the recommendations.

The Panel concluded there is strong State and local planning policy that supports the redevelopment of the harbour precinct and has recommended that Amendment C73 be adopted with changes. Changes to the Municipal Strategic Statement were considered non-controversial and supported by the Panel. All changes proposed by the Panel would be to the proposed Schedule to the Special Use Zone (SUZ2).

The Panel also noted that *“many of the key elements of the harbour’s future still appear to be in considerable dispute”* and Council *“may simply have delayed the difficult consideration of some of the more controversial elements and issues”*. These elements include an indoor pool/health and wellbeing centre, and height and built form issues.

The Panel made recommendations on a number of issues raised in submissions, as summarised below:

### 1. Visual Analysis and Impact

The Panel Report notes:

*“In the Panel’s view the Council’s decisions to cluster development in the south western corner of the precinct; to relocate the harbour operations area so as to maximise public access to the harbour’s southern edge; to cluster parking into nodes; and to seek to define height and built form controls that ensure that development does not dominate the existing southern skyline, are appropriate. However the Panel shares the view expressed by some submitters that reliance upon simple land use, height and materials controls is unlikely to ensure a satisfactory outcome”.*

It is noted that the use of prescriptive measures to control building height was a response to submitters’ concerns about the visual impact of any development. It is also acknowledged

that prescriptive measures can ultimately be a reasonably ‘blunt tool’ that do not allow for design flexibility.

The Panel has identified the need for an urban design analysis to support the Development Plan. In particular the following change to the SUZ2 at clause 4.0 (Supporting Requirements of the Development Plan) has been recommended:

*“An Urban Design analysis that responds to the Development Principles and Guidelines in Clause 5.0 and the proposed Key Projects to ensure a coherent basis for the Development Plan and harbour development. This analysis should include internal harbour precinct design considerations and options for creating a clear visual and pedestrian link between the Apollo Bay town centre, the foreshore reserve and the harbour precinct.”*

It is considered that an urban design analysis would result in more site specific recommendations for the development of the harbour than prescriptive height limits and, as such, it is recommended that Council support these changes.

## 2. Built Form, Height and Design Excellence

The Panel agreed that most built form should be located in the south-western corner of harbour and there needed to be a gradual decrease of bulk and height from the west to the east across the southern shoreline. The Panel also found that Council should encourage development of a small number of multi-purpose buildings, rather than allowing the construction of a large number of smaller, single purpose, structures.

The Panel has not supported the prescriptive use of height limits but has placed emphasis on the need to clearly set out the design outcomes sought via the development principles of the SUZ2, recommending two changes. The first is the deletion of prescriptive height requirements under Clause 3.0 (Buildings and Works). The second is the insertion of the following in Clause 5.0 under Built Form and Siting:

- *Built form should be of a height which does not intrude upon the southern skyline of the harbour precinct as viewed from the north of the harbour and the town centre. The height of development should taper down towards the east, in line with the taper of the landscape.*
- *A small number of multi-use or agglomerated buildings is to be preferred to development of a larger number of single purpose, stand alone, structures.*

It is recommended that Council support these changes.

## 3. Car Parking

The Panel acknowledged that a precinct based approach to parking is appropriate and there is a degree of certainty that parking will be mainly provided at the existing sites (Mothers Beach and boat ramp). The Panel found that the Development Plan should also consider options for managing parking demand at peak times.

The Panel considered that a detailed parking analysis for all uses, including a potential aquatic centre, would be an important step in preparing the Development Plan. On this basis the Panel found that Council should delay final design and construction of the parking area until the completion of the Development Plan. The Panel recommended that the redevelopment of the Mothers Beach car park and the boat trailer/car park be removed from the list of developments exempt from the requirement for a permit specified in Clause 3.0 (Buildings and Works). It is considered that the implication of this recommendation requires consideration, as the deletion of these items would impact on Council’s ability to carry out redevelopment works (other than if exempted from the requirement for a permit under clause 62 of the Planning Scheme). This issue should also be considered in light of any current or

future budget provision for the re-development of the parking areas. It is noted that the exhibited SUZ2, while exempting the need for a planning permit, still required the parking areas to be generally consistent with a Development Plan. On balance, whilst the Panel's reasoning is understood, it is considered that the redevelopment of the Mothers Beach car park and the boat trailer/car park should not be deleted from the list of developments exempt from the requirement for a permit, to ensure that Council has flexibility to carry out such work without being fettered by any potential delays in the completion of the Development Plan (e.g. as a result of community engagement).

It is not recommended that Council support this change.

#### 4. Linkages to the Town Centre

While Amendment C73 does not cover the area between the harbour and the town, there is clear strategy to enhance linkages through this area in the current amendment. The Panel referred to a submission that sought a visual link between the main street/foreshore reserve to the harbour that provided an iconic view of a boat on the harbour slipway via the Mothers Beach car park and Great Ocean Road close to the Great Ocean Road Hotel. To create this, some vegetation removal, earthworks and relocation of the fuel tank would be required. The Panel supported this outcome and, in line with its general desire to remove prescription, proposed to delete the reference to the alignment of new paths, replacing it in Clause 4.0 with:

*“New linkages between the Harbour and town centre that consider modes and capacity, environmental and cultural values and the opportunity for vistas”.*

The Panel stated that it considers that there would be significant benefit, as part of the Development Plan process, in further exploring the scope for creating more direct visual and pedestrian linkages between the harbour and the Apollo Bay town centre, and noted that this would require cooperation between Council, the Otway Coast Committee and the Golf Course Management.

It is considered the most important aspect of linking the town centre with the harbour will be well designed and located pathways, way-finding signage and marketing. It is noted that the harbour view-shed is very broad from the town centre.

The text above does not specifically refer to a visual connection, so it is recommended that Council support this change.

#### 5. Cultural heritage

An existing Aboriginal well and middens are protected under State legislation and recognised as significant by Aboriginal Affairs Victoria (AAV). The Panel supported the references in the SUZ2 to Aboriginal heritage but emphasised they do not modify the existing legal protections. The Panel also noted the existing application of the Heritage Overlay (HO300) to recognise the important social role of the harbour. The Panel recommended a minor change to the development principle in Clause 5.0 of the SUZ2 to refer to both heritage and history.

It is recommended that Council support this change.

#### 6. Aquatic/Wellness Centre in the Harbour Precinct

The Panel considered the issues were:

- *“The community desires a new indoor aquatic centre for year round use by residents and visitors which may include other elements such as sea baths and a ‘wellness centre’*
- *Council is supportive in principle but is concerned about the capital and operating costs*

- *There is disagreement about the best location, generally considered to be either the harbour or the P-12 College, where the existing pool is located; but Council's most recent position is to support a harbour location."*

The Panel noted that whether or not a pool should be constructed at the harbour was not a question for it to consider, except to the extent that the Amendment may provide for such a facility. The Panel referred to the wording of the October 2014 Council resolution as a demonstration that the most recent position of Council is to support a harbour location. As such, it proposed amending the SUZ2 to refer to it as a potential project:

*"Construct new Harbour Edge buildings located on the water's edge south of the boardwalk which could cater for a mix of retail/hospitality, aquatic based recreation, health and wellbeing facilities and multi-use event and meeting spaces for club, community and commercial activities."*

It is considered the Panel has misinterpreted the October 2014 resolution of Council, despite time being spent clarifying the intent of Council at the hearing. The resolution of Council was carefully drafted to avoid an interpretation that it now supports one location over another. The resolution of Council was directed at providing the ability to consider a pool at the harbour and the potential of it being included in the Development Plan process. However, the proposed changes to the text of the SUZ2 reflect Council's position where 'could' is considered to be the equivalent of 'potential'. On this basis, it is considered that Council should support this recommendation, albeit a decision on the pool location has yet to be made.

It is recommended that Council support this change.

#### Other changes to the SUZ2

The Panel recommended a number of other minor edits and updates to the SUZ2 (see section 5 of the Panel Report). Some notable ones are:

- General use of performance based measures in lieu of prescriptive outcomes
- A Construction Management Plan be required to support the preparation of the Development Plan
- A commitment to review the Development Plan after 5 years to ensure it retains currency
- Additional reference documents
- Clarifying that the exemption from notice and review is only for projects considered under the SUZ2 and not other parts of the planning scheme

None of these are considered controversial, and will assist in the streamlining of what is a relatively long and detailed zone schedule without losing its intent and meaning.

It is recommended that Council support these changes.

One issue that requires further consideration is the Panel recommendation to delete the ability to prepare the Development Plan in stages. The October 2014 Council report justified the need to stage the Development Plan as follows:

*"As the redevelopment of the Harbour will take place over a 10-20 year period and rely heavily on public funds, the staged approval of a Development Plan should be considered to allow for a greater degree of flexibility and allow Council to react to funding opportunities that may be on offer.... A staged approach would allow Council the possibility of deferring a determination on whether an indoor pool should be located at the harbour when preparing the Development Plan, should the pool issue threaten the ability to agree on a plan. This would give Council the flexibility to then break the plan into parts that could proceed in the short term".*

It is recommended that Council retain the ability to stage the Development Plan, but acknowledge that this should not be an aspiration but an option to be considered. A Development Plan that covers the entire precinct would be preferable to a staged Development Plan; however missing external funding opportunities due to unresolved controversial issues would be unfortunate.

#### Options

Council is required to consider the recommendations of the Panel. Council has the option of adopting the recommendations of the Panel in full, in part or not at all.

This report recommends the adoption of the recommendations in part, as detailed above. Whilst it is considered that the majority of recommendations can be supported, it is recommended that the ability to undertake the Development Plan in stages be retained. In addition, whilst the Panel Report recommends that that Council should delay final design and construction of the parking area until the completion of the Development Plan, it is considered that the flexibility for Council to carry out such works should be retained. As such, it is recommended that works for car parks be included in the list of developments exempt from the requirement for a permit.

If Council resolves to adopt the recommendations in part or not at all, it is required to inform the Minister for Planning of the basis on which this decision has been made.

#### **Proposal**

It is recommended that Council resolves to adopt most of the recommendations from the Panel Report. However, it is recommended that the ability to stage the Development Plan not be deleted, and that the redevelopment of the Mother's Beach car park and the boat trailer/car park be retained in the list of developments exempt from the requirement for a permit, for the reasons outlined in the report.

It is important that Council embark upon the Development Plan process as a demonstration of its ongoing commitment to resolve a range of issues and set a development framework for the harbour. Further discussions with Council will need to occur to determine how this may be achieved.

#### **Financial and Other Resource Implications**

All costs associated with Amendment C73 are being funded from the State Government grant under the Creating Better Places program, and a contribution from Tourism Victoria. It is expected that the balance of funds not spent on the amendment process will be used to part fund the Development Plan process. Further funds will be required in the 2015/16 budget process for that Plan.

#### **Risk Management & Compliance Issues**

The risk in Council not proceeding with the planning scheme amendment is that it will not resolve the uncertainty around the type of development that is appropriate at the Harbour. As the planning scheme amendment process is a forerunner to a bid for State Government funds and private investment for implementation of the Plan, not proceeding would also send a message to the State Government that, despite the funds spent on the planning process to date, Council does not support redevelopment of the Harbour.

Without a Planning Scheme Amendment, under the current planning scheme provisions Council would have to consider the size, scale and design of any future development that may occur in the Harbour precinct on its individual merits, without an overall strategy guiding development. The planning scheme amendment aims to put in place clear safeguards that guide the preferred scale and intensity of development through the introduction of specific

planning controls, offering a greater level of long term certainty and protection to the community.

The amendment has been undertaken using State Government funds. There is a risk of reputational damage given the level of grant funds, and other support, that has been provided for this project if Council chose not to adopt the amendment with or without changes.

### **Environmental and Climate Change Considerations**

The former Apollo Bay Harbour Master Plan 2013 was informed by a Coastal Hazard Vulnerability Assessment (CHVA). This assessment found that the proposed infrastructure is at low risk of being adversely affected by future sea level rise, stronger storm surge activity or coastal erosion, but that any new buildings and works would need to ensure they are sited and designed to be protected from rising sea levels and storm surge events.

### **Community Engagement**

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower. The method selected was to inform and consult.

The community has had many opportunities to provide input into the forward planning for the redevelopment of the Apollo Bay harbour, notably through the initial exhibition of Amendment C73 in late 2013, the re-exhibition in August and September 2014, and the Panel Hearing in December 2014. A total of 40 submissions were received to the re-exhibited Amendment, with thirty-five submissions supporting the amendment and/or requesting a change, and five objecting to it. Ten submitters elected to be heard at the Panel Hearing.

### **Implementation**

If Council resolves to adopt Amendment C73, the final amendment documents will be forwarded to the Minister for Planning for approval.

### **Conclusion**

Amendment C73 has been reviewed by an independent Planning Panel which has recommended the amendment be adopted with changes. It is considered that most of the proposed changes to the exhibited amendment should be supported, except for those referred to in the report.

### **Attachments**

1. C73 Panel Report
2. Recommended Schedule SUZ2

**Recommendation(s)**

***That Council:***

- 1. Supports all recommendations of the Panel, except the recommendation to delete the ability to prepare a Development Plan in stages, and the deletion of the redevelopment of the Mother's Beach car park and the boat trailer/car park from the list of developments exempt from the requirement for a permit.***
- 2. Adopts Amendment C73 pursuant to Section 29 of the Planning and Environment Act 1987 with changes as outlined in the Special Use Zone – Schedule 2 (Attachment 1).***
- 3. Submits the adopted amendment, together with prescribed information, to the Minister for Planning for approval pursuant to Section 31 of the Planning and Environment Act 1987.***
- 4. Advises the Minister for Planning that Council accepts each of the Panel's recommendations except the deletion of the ability to prepare a Development Plan in stages and the deletion of the redevelopment of the Mother's Beach car park and the boat trailer/car park from the list of developments exempt from the requirement for a permit.***
- 5. Authorises the Chief Executive Officer (or delegate) to finalise the amendment documentation for Ministerial approval.***
- 6. Writes to all submitters, informing them of Council's decision and thanking them for their interest in this matter.***

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OM152503-11 ADOPTION OF FORREST MOUNTAIN BIKE TRAILS STRATEGIC PLAN

AUTHOR:	Michael Swanson	ENDORSED:	Doug McNeill
DEPARTMENT:	Sustainable Planning & Development	FILE REF:	F13/1516

Purpose

The purpose of this report is to recommend adoption of the Forrest Mountain Bike Trails Strategic Plan following receipt of public submissions on the draft plan.

Declaration of Interests

No officer declared an interest under the *Local Government Act 1989* in the preparation of this report.

Background

The township of Forrest has been undergoing economic and social transformation over the past five to ten years resulting from a reduction in logging and agricultural activities and the increasing focus on tourism and the Great Otway National Park. Factors in Forrest’s favour however are the designation of the Forrest to Apollo Bay Road as a priority north-south link providing access between the Great Ocean Road and the Princes Highway and the influence of ‘tree change’ lifestyle newcomers to the area. An increasing focus on tourism accommodation, product and experience was identified in the 2003 State Government ‘A New Future for the Otways Tourism Initiative’ which proposed to further stimulate local economies within the Otway Ranges through the development of tourism. This was to offset the losses in employment and local economic activity resulting from the cessation of commercial hardwood harvesting. The Forrest Mountain Bike Trails (MBT) is the product of this strategy.

Formal trail development commenced in 2004, and the network was opened in 2007. The project was led by the then Department of Sustainability and Environment (now the Department of Environment, Land, Water and Planning (DELWP), with input from Parks Victoria and Colac Otway Shire. DELWP has responsibility for maintenance, signage, and risk management. The Department convenes the Forrest Mountain Bike Trails Committee, which is a stakeholder committee convened to coordinate operational matters including maintenance, erosion control, alignment and possible new trails.

It is estimated that more than 22,000 riders per annum use the approximately 66km network of 16 trails. There are two trail heads with car parks, signs, toilets and shelter. The trails also link to the Tiger Rail Trail Stage 1, a shared access trail that may eventually link Forrest to Barwon Downs. The Forrest MBT is home to a number of major cycling events that attract a large number of competitors and their families to the township of Forrest.

The State Government through DELWP agreed to support a Council led strategic plan to establish a vision for the future development of the Forrest Mountain Bike Trails. Funding was secured through Regional Development Victoria and DELWP enabling the development of the strategic plan. Council engaged consultant Thompson Berrill Landscape Design to undertake the development of the plan.

The objectives of the project were to determine the value of the current infrastructure to key user groups as well as the broader community, identify issues and opportunities associated with the current trails and supporting infrastructure, future of trails development, increase in tourism development and visitation, and the development of an implementation plan with priority areas for action.

An extensive community consultation process was undertaken throughout the development of the strategic plan to gain input from local residents, local groups and committees, government agencies such as the former DEPI, Barwon Water and other key stakeholders. A number of consultation processes were used in the development of the plan including agency and stakeholder workshops, community drop in sessions, a local residents' newsletter, an online survey and a specialist mountain bike trails workshop. Overseeing the development of the strategic plan was a Project Steering Group that included Council staff, a Councillor representative (Cr Michael Delahunty), agency members, local Forrest community representatives and residents. The engagement throughout the consultation process was a fundamental step in developing the Issues and Opportunities Paper and ensuring continued feedback during the development of the strategic plan.

A Strategic Recommendations and Implementation Plan was developed with a number of key recommendations identified such as:

- The formation of an Advisory Group with key agency representation to focus on implementing the plan and providing direction for ongoing maintenance, management and funding options for the trails network.
- Establish new mountain bike trails in close proximity to the township of Forrest to ensure continued economic and tourism sustainability for the town.
- Establish a new skills development park at the Forrest Trail Head.
- Upgrade, undertake realignment and reclassification of existing trails to enhance the rider's experience.
- Upgrade trail signage, including directional, wayfinding and interpretation, across the Forrest and Yaugher trails network.
- Undertake detailed design for a tunnel underpass of Birregurra – Forrest Road to ensure safe crossing when riding to the Yaugher trails network.
- Undertake landscape improvements at the Forrest Trail Head and improve the overall trail entry including key visitor information, signage and a wash down facility.
- Retain and protect existing areas of remnant bushland and undertake revegetation works in targeted areas.

The strategic plan provides an overview of the importance of enhancing the township of Forrest and mountain bike trails to ensure future sustainability, and documents the ways in which Council and key agencies can lead the future development of Forrest. The strategic plan also includes a Benchmarking Analysis Report and Detailed Site Analysis Summary which detail the research and consultation outputs.

At the November 2014 Council Meeting it was endorsed that the draft Strategic Plan be placed on public exhibition for a minimum of eight weeks given the Christmas/New Year period, with the exhibition period closing on 22 January 2015.

Council received six (6) submissions to the draft Strategic Plan. Council has also received formal sign off from Barwon Water and DELWP of the draft strategic plan.

Council Plan / Other Strategies / Policy

A Planned Future

Creates an attractive shire with quality buildings and spaces, accessible travel and transport, and a community that has the services and facilities it needs now and in the future; supports a prosperous economy where trade, manufacturing and business activity flourishes.

Our Goal:

Facilitate the growth, liveability and development of the shire and encourage innovation and efficiency in the local economy.

Strategy 5 under this pillar is to: 'Grow tourism to support the local economy'.

Issues / Options

The six (6) submissions that were received on the draft Strategic Plan during the exhibition period have been reviewed by Council officers, resulting in the following suggested additions to the Plan.

A submission was received by a staff member from the National Disability Insurance Agency (NDIA) seeking an addition to the draft Strategic Plan in relation to trail use and trail design relating to inclusion of people with disabilities, looking specifically at dual-use trails for recumbent hand-cycles and recumbent mountain bikes. The NDIA have funding streams that targets projects that increase inclusion and demonstrate positive participation outcomes for people with disabilities, their carers and their communities through activities.

There is an opportunity when designing future trails as shown in the draft Strategic Plan to allow for riders that use hand cycles on a single track system and mountain bike riders. This would create an opportunity for a track or tracks to be designed with the potential for funding to assist with the implementation. The Forrest trails network was identified as the most appropriate network to investigate this design, as the trail head and facilities near the trail head are more suitable for people of all abilities.

There was a change to the draft strategic plan in section 2.2.1.2 on page 11 which states:

'Seek opportunity for multi-use track design to accommodate MTB trails suitable for people with all abilities, e.g. in the redesign of the Roller Coaster Trail'.

DELWP made a submission to the plan with a number of suggested changes. The plan now reflects the name change for the department throughout the document which was previously 'Department of Environment and Primary Industries'. The main changes related to the target marketing identified during the strategic plan development phase and the lack of reference to the Barwon South West Regional Trails Master Plan 2009. The target market section of the draft strategic plan has been expanded on after discussions with DELWP to state that women and families that may be new to mountain biking should be the focus of marketing material. The draft plan was amended to on page 20 to add the information on the target market, which states:

'The key target market based on survey feedback, consultation and gap analysis is women and families that may be new to the mountain biking experience. Opportunity to improve skill set via skill development clinics and general skill development via diversified skill development trails and a new skills park area. Growth in this target market will enhance the

economic outcome for the township and increase spending and visitation by diversifying the attraction and experience that Forrest has to offer’.

The Regional Trails Master Plan was highlighted as a document that had not been reviewed during the literature review. This has now been completed and referenced on page 7 of the draft strategic plan.

A number of submissions stated their support for a number of recommendations in the draft Plan. There were submissions that were very strong in supporting the underpass at Boundary Road given the risk of accidents to people crossing that road and the upgrade of services in the town. A change to the draft strategic plan was added based on the number of submissions received for the tunnel underpass and the effect the new infrastructure would have on events. The change was made on page 15 to reflect the submissions:

‘Completion of the tunnel underpass may reduce the need for road closures during events along Boundary Road’.

A number of submissions mentioned the need for an upgrade to the public toilets in Forrest. The submission from the Forrest and District Community Group in particular requested a change of the priority of the public toilet facilities from a ‘medium’ priority to a ‘very high’ priority. To ensure consistency in the plan we have made amendments on pages 13 and 22 and changed the priority to ‘medium – high’.

The other submission suggested liaising with Midway Plantations to discuss the potential for future mountain bike trails to be developed in their plantation areas. The draft Plan focusses on trails around the township of Forrest to increase economic activity in the town. Also, the majority of the trails suggested in the draft Strategic Plan are on DELWP or Barwon Water land, both of which have been heavily involved in the development of the draft Strategic Plan. The proposed trails for this plan will cater for mountain biking in the town for years to come, and therefore there is no suggested change to the plan following this submission. There were also submissions that put suggestions forward to the draft Plan that were not part of the scope of the project. These submissions were reviewed by Council officers in coordination with the consultant Thompson Berrill Landscape Design and detailed responses were provided to the submitters.

Both DELWP and Barwon Water have endorsed the plan which is a key step in finalising the draft strategic plan.

It is anticipated that some residents may not agree with the actions and outcomes of the strategic plan. However the plan has been developed with a comprehensive consultation process to ensure that it captured all issues and opportunities to inform the key recommendations to be delivered.

A key outcome of the strategic plan is to ensure that it secures and guides an economically sustainable future for the Forrest Mountain Bike Trails and the township of Forrest through nature based tourism. The plan aims to build on the current economic and tourism impetus the mountain bike trails have on the town of Forrest and to ensure that the trails remain highly competitive in the mountain biking industry.

Council has the options of:

Option 1 – Adopt the Forrest Mountain Bike Trails Strategic Plan.

Due to the high level of engagement from the community and key stakeholders throughout the development of the strategic plan, this would be the preferred option.

Option 2 – Seek further changes to the plan and release the revised plan for public exhibition.

It is not anticipated that further work needs to be completed on the strategic plan at this stage, given the input and feedback the plan has received to date. The public exhibition period was advertised, with the community, key agencies, and visitors given the ability to comment on the plan for an extended period of time.

Option 3 – Do not proceed with the project.

There is an expectation from the Project Steering Group which includes key agencies and the community that this plan will be adopted after the public exhibition period. If the plan is delayed or not adopted then this may put Council in jeopardy of not meeting the commitments of its funding agreement with Regional Development Victoria.

Considering the significant work that has been undertaken in preparing the strategic plan, (including several site visits, community meetings, key stakeholder meetings and surveys), it is recommended that Council adopts the strategic plan.

Proposal

It is proposed that Council adopt the Forrest Mountain Bike Trails Strategic Plan.

Financial and Other Resource Implications

Council was successful in securing funding of \$30,000 from Regional Development Victoria and \$20,000 from the former Department of Environment and Primary Industry. This was contingent on Council providing a contribution of \$25,000 towards the project, amounting to a total of \$75,000 for the project. Council's contribution was carried forward to 2014/15 to allow for the completion of the project. Many actions in the draft plan will require allocation of funding, mostly from State and Federal Government grants.

Risk Management & Compliance Issues

There is a risk if Council chooses not to proceed with the project that it would not meet the conditions of external grants, and could experience reputational damage with stakeholders.

Environmental and Climate Change Considerations

Environmental considerations are identified in relation to the inclusion of new trails, the modification of existing trails within the trails network and the natural landscape within the township of Forrest. Any changes to the trails network would be subject to a formal process coordinated by DEWLP which would ensure that all environmental aspects are considered.

Community Engagement

The community engagement strategy follows the recommendations of the Colac Otway Shire Council Community Engagement Policy of January 2010, which details five levels of engagement – inform, consult, involve, collaborate and empower.

The method selected was consult and includes significant communication from Council to the community released via local media and direct correspondence informing the community of the availability of the draft strategic plan for public comment. Copies of the document were available on Council's website and for viewing in the Colac and Apollo Bay Customer Service Centres, the Colac Community Library and Learning Centre and local businesses within the township of Forrest. A newsletter was completed and sent out to key agencies and community members to promote the exhibition of the draft Strategic Plan. The use of social media was also used with a post on Facebook promoting the Plan.

Implementation

Implementation of the recommendations within the strategic plan will only commence following Council’s adoption of the final Forrest Mountain Bike Trails Strategic Plan. Most actions will require allocation of funding, primarily from external sources. Should Council adopt the strategic plan, the next stage of this project will involve the development of the Advisory Group as recommended.

Conclusion

The Forrest Mountain Bike Trails are a key driver for tourism and economic growth in the town of Forrest. The development of the strategic plan aims to build on the current benefits of the mountain bike trails and enhance the visitor experience. The plan looks at key infrastructure upgrades, maintenance and future development of the mountain bike trails.

The strategic plan was developed through a comprehensive and thorough community engagement strategy with key agencies and stakeholders and is now ready to be adopted following public exhibition. Council has received formal endorsement from key agencies Barwon Water and DEWLP for the plan.

Attachments

1. Final Forrest Mountain Bike Trails Strategic Plan

Recommendation(s)

That Council adopts the final Forrest Mountain Bike Trails Strategic Plan.

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**OM152503-12 ASSEMBLY OF COUNCILLORS**

|             |               |           |               |
|-------------|---------------|-----------|---------------|
| AUTHOR:     | Rhonda Deigan | ENDORSED: | Sue Wilkinson |
| DEPARTMENT: | Executive     | FILE REF: | F14/6031      |

**Introduction**

The *Local Government Act 1989* (the Act) requires that records of meetings, which constitute an Assembly of Councillors, be tabled at the next practicable meeting of Council and incorporated in the minutes of the Council meeting.

**Definition**

An “assembly of Councillors” is a defined term under section 3(1) of the *Local Government Act 1989*.

It is a meeting at which matters are considered that are intended or likely to be the subject of a Council decision or the exercise of a delegated authority and which is either of the following:

- A meeting of an advisory committee where at least one Councillor is present or
- A planned or scheduled meeting that includes at least half the Councillors and at least one Council officer.

If a meeting fits either of these types, the procedures applying to an assembly of Councillors must be complied with, irrespective of any name or description given to the committee or meeting.

**Advisory Committees**

Section 3(1) of the Act defines an advisory committee to be any committee established by the Council, other than a special committee, that provides advice to:

- *the Council or*
- *a special committee or*
- *a member of Council staff who has been delegated a power, duty or function of the Council under section 98.*

**Councillor briefings**

One type of meeting that is clearly an assembly of Councillors is the type of regular meeting, commonly referred to as a “Councillor Briefing” or similar name where Councillors and staff meet to discuss issues that are likely to come before Council for decision.

As part of decision making processes at Colac Otway, it is essential that Councillors are briefed on a range of issues which come before Council for consideration. As a means of providing this information, Assembly of Councillor briefings are conducted.

Assemblies are also attended by Council Officers, and sometimes other specific advisors, to provide Councillors with a detailed knowledge and understanding of issues under consideration to a level of detail that would inhibit timely decision-making, that would not be possible in an open council meeting or where decision-making related debate is governed by strict meeting procedures.

While these meetings have no authority to make Council decisions, they are generally assemblies of Councillors and subject to conflict of interest disclosures.

**What records are to be kept**

Section 80A of the Act requires that a record must be kept of an Assembly of Councillors which lists:

- the names of all Councillors and members of Council staff attending;
- the matters considered;
- conflict of interest disclosures made by a Councillor (if any are made); and
- whether a Councillor who has disclosed a conflict of interest leaves the assembly.

It is a requirement that the record of an assembly is to be reported to the next practicable ordinary meeting of Council and be incorporated in the minutes of that Council meeting.

**Assemblies of Councillors**

The following Assemblies of Councillors have been held:

- Central Reserve Advisory Committee 4 February 2015
- Friends of Colac Botanic Gardens 12 February 2015
- Colac Regional Saleyards Advisory Committee 16 February 2015
- Councillor Workshop 18 February 2015
- Councillor Briefing Session 25 February 2015
- Strategic Planning Workshop 4 March 2015
- Councillor Workshop 11 March 2015

**Attachments**

1. Central Reserve Advisory Committee - 4 February 2015
2. Friends of Colac Botanic Gardens Advisory Committee - 12 February 2015
3. Colac Regional Saleyards - 16 February 2015
4. Councillor Workshop 18 February 2015
5. Councillor Briefing Session - 25 February 2015
6. Strategic Planning Workshop - 4 March 2015
7. Councillor Workshop - 11 March 2015

**Recommendation(s)**

***That Council notes the Assembly of Councillors reports for:***

- ***Central Reserve Advisory Committee 4 February 2015***
- ***Friends of Colac Botanic Gardens 12 February 2015***
- ***Colac Regional Saleyards Advisory Committee 16 February 2015***
- ***Councillor Workshop 18 February 2015***
- ***Councillor Briefing Session 25 February 2015***
- ***Strategic Planning Workshop 4 March 2015***
- ***Councillor Workshop 11 March 2015***

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**OM152503-13 ITEM FOR SIGNING & SEALING - STOCK UNDERPASS -
960 IRREWILLIPE ROAD, BARONGAROOK WEST**

| | | | |
|-------------|---------------------------|-----------|--------------|
| AUTHOR: | Paula Gardiner | ENDORSED: | Phil Corluka |
| DEPARTMENT: | Infrastructure & Services | FILE REF: | 11/95455 |

Stock Underpass Agreement between Colac Otway Shire Council, Simon David Scott 960 Irrewillipe Road, Barongarook West and Norman Harold Lowe of 415 Barrys Road, Barongarook West.

A Section 173 Agreement between Colac Otway Shire, Simon David Scott and Norman Harold Lowe is presented for signing and sealing by Council. The Agreement relates to a stock underpass constructed by Simon David Scott under Irrewillipe Road, Barongarook West and the two connecting parcels of land at either end of the underpass.

The parcels of land subject to this Agreement are described as Lot 1, TP 219407L (960 Irrewillipe Road, Barongarook West) and Lot 3, TP 643654A (415 Barrys Road, Barongarook West).

The Term of the Agreement is twenty (20) years.

Attachments

1. Section 173 Agreement for Signature - 960 Irrewillipe Road Barongarook West

Recommendation(s)

That Council delegates the Chief Executive Officer to sign and seal the Stock Underpass Agreement between Colac Otway Shire Council, Simon David Scott and Norman Harold Lowe for a stock underpass under Irrewillipe Road, Barongarook West.

~~~~~



**OM152503-14      ITEM FOR SIGNING & SEALING - STOCK UNDERPASS -  
1425 IRREWILLIPE ROAD, IRREWILLIPE EAST**

AUTHOR:	Paula Gardiner	ENDORSED:	Phil Corluka
DEPARTMENT:	Infrastructure & Services	FILE REF:	F14/9403

Stock Underpass Agreement between Colac Otway Shire Council and J J & F E Parsons of 1780 Irrewillipe Road, Irrewillipe East.

A Section 173 Agreement between Colac Otway Shire Council and J J & F E Parsons of 1780 Irrewillipe Road, Irrewillipe East is presented for signing and sealing by Council. The Agreement relates to a stock underpass constructed by J J & F E Parsons under Irrewillipe Road, Irrewillipe East and the two connecting parcels of land at either end of the underpass.

The parcels of land subject to this Agreement are described as Lot 1, TP 249498 (345 Carlisle Road, Irrewillipe East) and Lot 1, TP 131270 (1425 Irrewillipe Road, Irrewillipe East).

The Term of the Agreement is five (5) years with three further options of five (5) years.

**Attachments**

1. Section 173 Agreement for Signature - 1425 Irrewillipe Road IRREWILLIPE EAST

**Recommendation(s)**

***That Council delegates the Chief Executive Officer to sign and seal the Stock Underpass Agreement between Colac Otway Shire Council and J J & F E Parsons for a stock underpass under Irrewillipe Road, Irrewillipe East.***

~~~~~\ ~~~~~


IN COMMITTEE

Recommendation

That pursuant to the provisions of Section 89(2) of the Local Government Act, the meeting be closed to the public and Council move “In-Committee” in order to deal with:

| <i>SUBJECT</i> | <i>REASON</i> | <i>SECTION OF ACT</i> |
|---|---|------------------------------|
| Minutes of the In-Committee Meeting held on 25 February 2015 | this matter deals with contractual matters | Section 89 (2) (d) |
| Contract 1433 - Supply and Delivery of Grader | this matter deals with contractual matters | Section 89 (2) (d) |
| Contract 1509 – Sand Road Bridge Rehabilitation | this matter deals with contractual matters | Section 89 (2) (d) |
| Confidential Item for Consideration - Colac Otway Performing Arts & Cultural Centre - Future Directions | this matter deals with contractual matters; AND this matter may prejudice the Council or any person | Section 89 (2) (d) (h) |



ORDINARY COUNCIL MEETING

WEDNESDAY, 25 MARCH 2015

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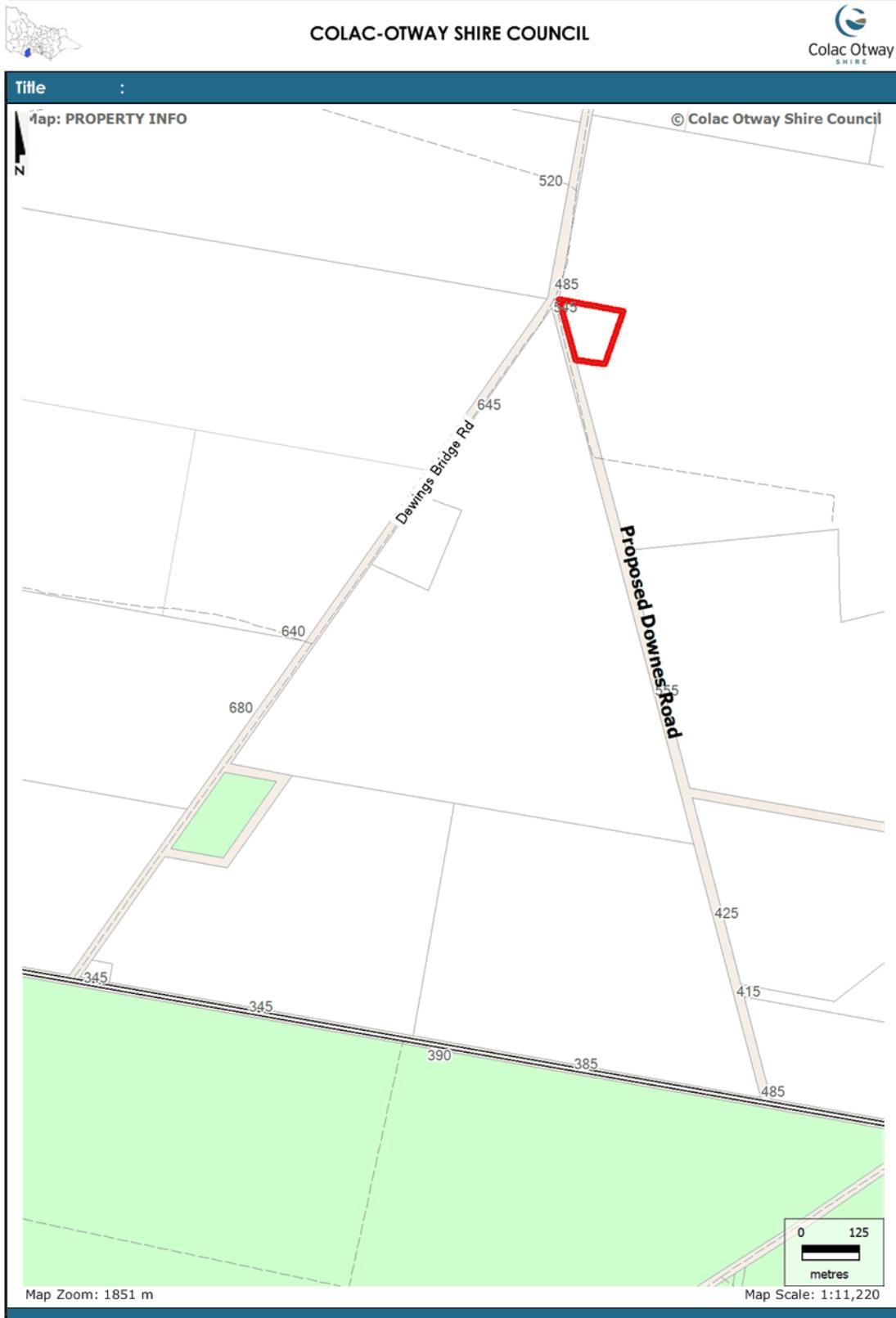
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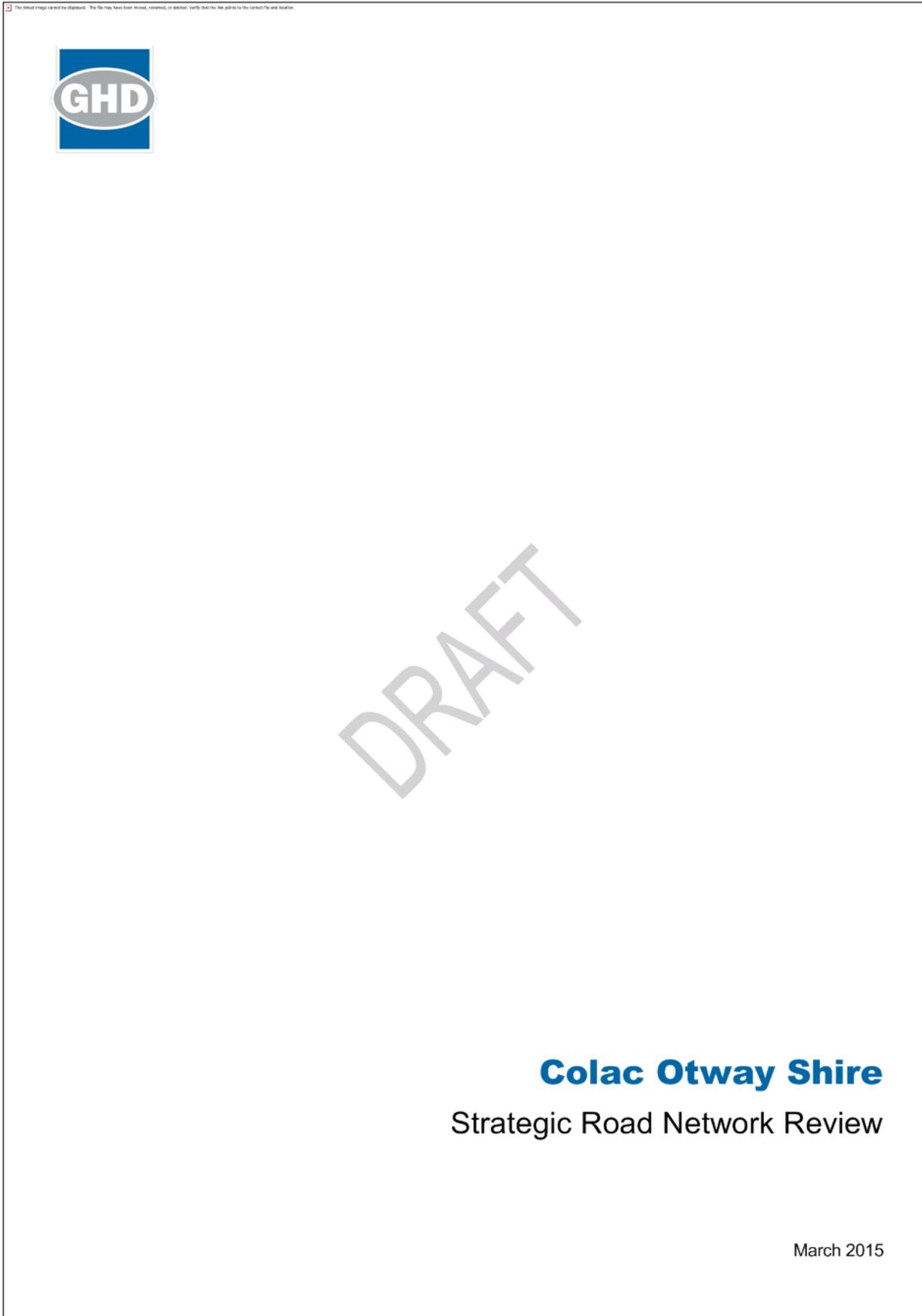
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Created by general on Monday, 16 March 2015





WATER | ENERGY & RESOURCES | ENVIRONMENT | PROPERTY & BUILDINGS | TRANSPORTATION

This report: has been prepared by GHD for Colac Otway Shire and may only be used and relied on by Colac Otway Shire for the purpose agreed between GHD and the Colac Otway Shire as set out in section 1.2 of this report.

GHD otherwise disclaims responsibility to any person other than Colac Otway Shire arising in connection with this report. GHD also excludes implied warranties and conditions, to the extent legally permissible.

The services undertaken by GHD in connection with preparing this report were limited to those specifically detailed in the report and are subject to the scope limitations set out in the report.

The opinions, conclusions and any recommendations in this report are based on conditions encountered and information reviewed at the date of preparation of the report. GHD has no responsibility or obligation to update this report to account for events or changes occurring subsequent to the date that the report was prepared.

The opinions, conclusions and any recommendations in this report are based on assumptions made by GHD described in this report (refer section(s) 1.3 of this report). GHD disclaims liability arising from any of the assumptions being incorrect.

GHD has prepared this report on the basis of information provided by Colac Otway Shire and others who provided information to GHD (including Government authorities), which GHD has not independently verified or checked beyond the agreed scope of work. GHD does not accept liability in connection with such unverified information, including errors and omissions in the report which were caused by errors or omissions in that information.

GHD has prepared the preliminary cost estimate set out in section 4 of this report ("Cost Estimate") using information reasonably available to the GHD employee(s) who prepared this report; and based on assumptions and judgments made by GHD as described in Section 4 of this report.

The Cost Estimate has been prepared for the purpose of a high level investigation to provide Colac Otway Shire with an indication of costs required to upgrade the five key freight routes identified as part of this study and must not be used for any other purpose.

The Cost Estimate is a preliminary estimate only. Actual prices, costs and other variables may be different to those used to prepare the Cost Estimate and may change. Unless as otherwise specified in this report, no detailed quotation has been obtained for actions identified in this report. GHD does not represent, warrant or guarantee that the [works/project] can or will be undertaken at a cost which is the same or less than the Cost Estimate.

Where estimates of potential costs are provided with an indicated level of confidence, notwithstanding the conservatism of the level of confidence selected as the planning level, there remains a chance that the cost will be greater than the planning estimate, and any funding would not be adequate. The confidence level considered to be most appropriate for planning purposes will vary depending on the conservatism of the user and the nature of the project. The user should therefore select appropriate confidence levels to suit their particular risk profile.

The opinions, conclusions and any recommendations in this report are based on information obtained from, and testing undertaken at or in connection with, specific sample points. Site conditions at other parts of the site may be different from the site conditions found at the specific sample points.

Investigations undertaken in respect of this report are constrained by the particular site conditions, such as the location of buildings, services and vegetation. As a result, not all relevant site features and conditions may have been identified in this report.

Site conditions (including the presence of hazardous substances and/or site contamination) may change after the date of this Report. GHD does not accept responsibility arising from, or in connection with, any change to the site conditions. GHD is also not responsible for updating this report if the site conditions change.

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Appendices

- Appendix A – Cape Otway Shire Road Network and Surveyed Freight Routes
- Appendix B – Cape Otway Road Issues Map
- Appendix C – Barongarook Road, Bushby’s Road, Old Yeo Road Issues Map
- Appendix D – Nalangil Road Issues Map
- Appendix E – Irrewillipe Road and Barrys Road Issues Map
- Appendix F – Warrobie Road and Mount Hesse Road Issues Map

1. Introduction

1.1 Context and Background

This Strategic Road Network Review has been developed on behalf of the Colac Otway Shire Council by GHD.

Colac Otway Shire is located approximately 75 kilometres west of Geelong and covers the townships of Colac, Apollo Bay, Beeac, Beech Forrest, Birregurra, Forrest and Lavers Hill. The shire has a population of approximately 20,000 people with almost 12,000 people located in Colac.

The shire covers an area of approximately 3,400 km² and the main industries include dairy farming, agriculture, timber and tourism. There are a number of major highways and roads travelling through the municipality which are managed by VicRoads. However, the majority of the roads within the shire are Council managed roads, which connect to these major roads, and are thus instrumental to the freight movements within Colac Otway Shire.

The key VicRoads managed roads within Colac Otway Shire include:

- Princes Highway;
- Hamilton Highway;
- Great Ocean Road;
- Colac-Ballarat Road;
- Colac-Forrest-Apollo Bay Road; and
- Colac -Lavers Hill Road.

The Princes Highway, Hamilton Highway and Great Ocean Road provide east-west connections across the shire to Geelong, with the Great Ocean Road also a tourist attraction in its own right.

The north-south connections are provided by the Colac-Forrest-Apollo Bay Road and Colac-Lavers Hill Road and local roads. These roads are utilised by primary industry and agriculture (e.g. milk, grain, and livestock), the timber industry as well as local and tourist traffic.

Council wishes to understand the key routes used by freight and to develop a costed long term improvement program for the shire in light of anticipated growth in freight movements and the expected increase in the use of larger trucks for the movement of freight, particularly in light of consolidation of farming industries. This report presents the findings of this review.

1.2 Purpose of this report

The purpose of this review is to:

- Determine whether the existing road hierarchy reflects the current and future road usage, with an emphasis on the roads currently used for freight movements;
- Understand the key routes that freight companies use and develop a priority list of routes to be considered for upgrades and maintenance treatments. This is especially important given that there is likely to be a move towards the use of larger trucks in the coming years; and
- Develop a costed and prioritised long term improvement program for strategic roads under Council control with a particular focus on freight routes.

1.3 Scope and Assumptions

GHD has liaised with Council officers throughout the project to develop a document which will assist Council to identify where maintenance funding should be focused towards. The study has been a high level assessment, primarily based on desktop findings and has also included a site visit of the key routes and attendance at a workshop with freight and heavy vehicle operators.

The long term works program has been developed based on the site inspection of the five routes. A feature survey and detailed length measurements have not been undertaken as part of this assessment. Therefore further detailed assessments of the specific modifications will be required to obtain a greater understanding of the works involved and hence greater certainty for the cost estimate.

2. Freight Routes

2.1 Existing Road Network

The existing road network within Colac Otway Shire is a combination of VicRoads managed roads and Council managed roads. VicRoads generally has a two tier road hierarchy of roads they manage; while Colac Otway Shire has a four-tier rural hierarchy of Council managed road types.

The road types within Colac Otway Shire and the typical geometric widths for these roads are presented in Table 1. The road network within Colac Otway Shire is presented in Figure 1.

Council roads within townships are classified under the four-tier urban hierarchy and are generally of a higher standard than rural roads due to the volume of traffic utilising the roads each day. The urban hierarchy has the same four tiers as the rural hierarchy. However a greater proportion of urban roads have sealed surfaces. Furthermore the geometric conditions of urban roads are usually different to rural roads due to vehicle and pedestrian requirements for these roads.

Additionally, there is usually a hierarchy of roads within each town to prioritise the various modes of transport and their needs within the town centre (i.e. key freight routes, pedestrian routes, cycling routes, local traffic). This level of detail has not been included within the hierarchy as this task is focused on the strategic road network of the shire rather than specific townships.

Table 1 Existing Road Hierarchy Classifications

| Road Type | Manager | Geometric Widths |
|-----------------------|----------|--|
| Highways | VicRoads | Four-lane sealed carriageway with traffic lane widths of approximately 3.5 m. Although some highways only have two lane carriageways. |
| Arterial Roads | VicRoads | Two-lane, two way sealed carriageway. |
| Rural Link Roads | Council | Two-lane, two way roads that are generally sealed however, may also be a gravel surface. The roads provide a link between significant town centres and major traffic generators. |
| Rural Collector Roads | Council | These roads are either sealed or gravel surface roads which provide a route through or between key traffic nodes i.e. residential, industrial, agricultural, tourism and forestry. |
| Rural Access Roads | Council | These roads provide access to properties and generally connect to the collector Council roads. The roads may be either sealed or gravel surfaces. |
| Rural Minor Roads | Council | These roads are either gravel or natural surface roads. They generally provide occasional access to non-residential properties. |

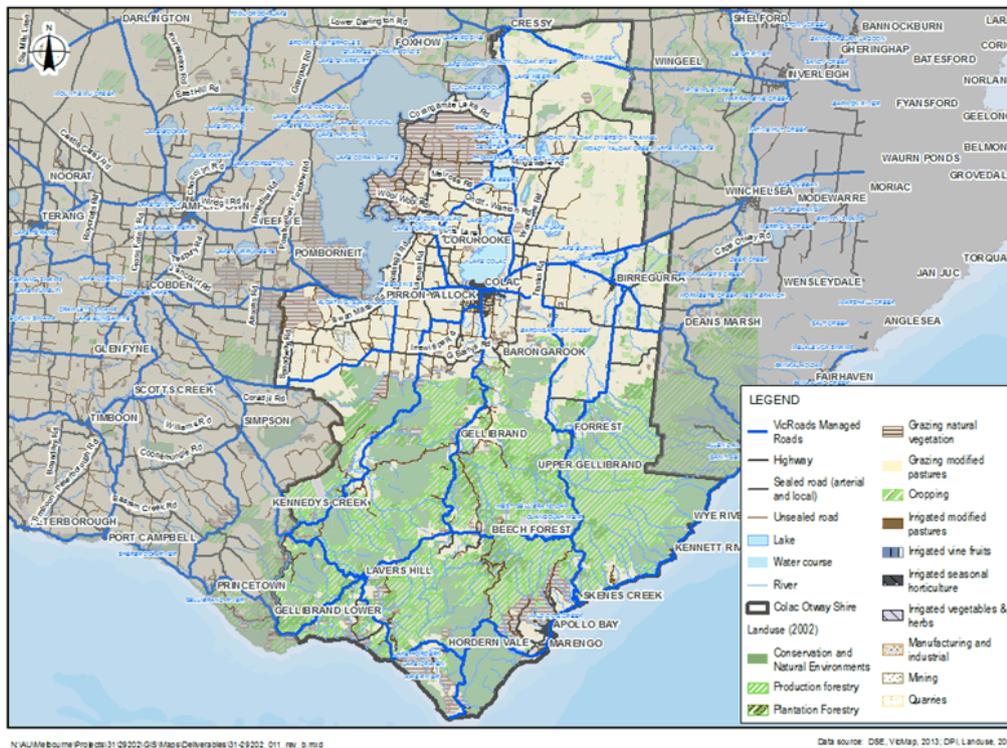


Figure 1 Colac Otway Shire Road Network

As depicted in Figure 1 the VicRoads managed roads create strategic connections between key towns in both the north-south and east-west directions. The Princes Highway and Hamilton Highway are both orientated in a general east-west direction to provide connections to Melbourne and Geelong while other arterial roads managed by VicRoads provide north-south and east-west orientated roads to enable travel between the townships within the shire.

The Council managed roads provide the connections between the arterial roads and link minor townships within the region. Key council roads are focused around the Colac township where there are multiple residential and industrial uses, and consequently higher traffic volumes. Additionally, Council roads are located in remote locations to provide a link from the VicRoads managed roads to a residential property or industrial uses i.e. farming, forestry.

Colac Otway Shire have identified a number of significant council managed roads that are also B-Double declared roads, these include:

- Cape Otway Road.
- Larpent Road.
- Swan Marsh-Irrewillipe Road.
- Speedway Road.
- Carpendeit-Bungador Road.
- Irrewillipe Road.

The majority of roads within Colac Otway Shire are managed by Council, the roads which are managed by VicRoads include:

- | | |
|-----------------------------------|------------------------------|
| • Beech Forest-Lavers Hill Road | • Cororooke Road. |
| • Beech Forest-Mount Sabine Road. | • Forrest-Apollo Bay Road. |
| • Birregurra Road. | • Gellibrand River Road. |
| • Birregurra-Deans Marsh Road. | • Great Ocean Road. |
| • Birregurra Forrest Road. | • Hamilton Highway. |
| • Cape Otway Lighthouse Road | • Lavers Hill-Cobden Road. |
| • Colac-Ballararat Road. | • Princes Highway (West). |
| • Colac-Carlisle Road. | • Skenes Creek Road. |
| • Colac-Forrest Road. | • Timboon-Colac Road. |
| • Colac-Lavers Hill Road. | • Warncoort-Birregurra Road. |

2.2 Industry Consultation

Key freight and heavy transport operators within Colac Otway Shire were consulted to help determine whether the existing road hierarchy reflects the current use of the road network. Additionally, the consultation sought industry views on which routes are envisaged to be utilised in the future. The outcomes of the consultation assisted to identify which routes should be the key freight routes within the Shire and be the focus for further assessment. The involvement of Freight and Heavy Transport Operators was considered to be a key element of this project, as any changes to the road hierarchy would most likely affect these industries the most.

A workshop was held on Friday 22nd March 2013 in Colac Community Library and Learning Centre. The workshop was an interactive format to identify the following:

- Key routes used by transport operators now;
- Key issues with existing routes; and
- Future routes used by transport operators in the short term (<5 years) and the long term (>5 years).

For those transport operators that couldn't attend the workshop a questionnaire was sent out which incorporated the key elements of the workshop.

The freight and heavy transport operators that provided feedback as part of the consultation process include:

- Clearwater Transport;
- Christians Bus Co;
- Colac Otway Shire;
- Fonterra;
- Malec Bros. Transport;
- Midway Plantations;
- Murray Goulburn; and
- Warrnambool Cheese and Butter.

The following sections provide a summary of the inputs gathered from the freight and heavy transport operators.

2.2.1 Key routes used now

According to the inputs provided as part of the consultation process the majority of the main local roads are utilised for heavy vehicle use. The volumes on these roads vary. However, many of the smaller roads are still required to enable access to properties and utilities.

The larger roads that have been identified to be currently utilised by freight companies include:

- Carlisle Road;
- Cobden-Stoneyford Road;
- Coragulac-Beeac Road; and
- Corangamite Lake Road.

It is noted, a length of Corangamite Lake Road and Coragulac-Beeac Road, are not under the control of Council. However these routes were identified by all stakeholders as roads that are frequently utilised by their companies.

Additionally, the freight operators identified a variety of vehicle types travelling along these roads due to the varying access requirements and use of the vehicles. Where it is permitted transport contractors generally seek to utilise high mass limit vehicles to gain efficiencies throughout the logistics processes.

Geelong and Melbourne are both key destinations for goods produced within the Colac Otway Region. Consequently the majority of freight vehicles do travel in an easterly direction.

2.2.2 Key issues with existing routes

The general feedback on the condition of the local roads within Colac Otway Shire is that they needed further maintenance to be able to accommodate the volumes of heavy vehicles currently utilising the roads. The key issues with the roads were generally consistent among the responses and included:

- Poor and varying road surfaces including rough and gravel roads with pot holes;
- Soft road edges;
- Winding roads;
- Steep roads;
- Trees overhanging onto the roads;
- Narrow bridges;
- Limited access for more efficient, larger configuration higher mass trucks; and
- Narrow shoulders.

2.2.3 Future routes

It was noted by respondents that future freight routes are difficult to identify as they generally did not know if origins/destinations were likely to alter in the coming years. Moving industries (i.e. forestry) are likely to alter routes to accommodate the new origin locations. However it is anticipated the general routes will remain relatively constant provided conditions of the routes are improved and permit larger vehicles.

The majority of responses indicated that vehicle fleets are likely to be upgraded to larger vehicles to allow for greater efficiency of the load and minimise the growth in number of vehicles on the road network. However, the number and type of vehicles that will be used is also uncertain as this will depend on the timing of the upgrades and changes to road conditions/permits which allow for larger vehicles.

2.3 Proposed Road Hierarchy

As discussed previously Colac Otway Shire manages a large number of roads for a variety of uses and varying vehicles. To utilise maintenance funding more efficiently Colac Otway Shire wishes to modify the existing Council road hierarchy to establish preferred freight routes that can be upgraded to meet the minimum geometric road requirements to accommodate heavy vehicles, while other roads are maintained to a lower standard.

As outlined in Section 2.1 the existing Council road hierarchy is generally four tiers for both urban and rural locations. Furthermore the majority of Council roads surround the Colac township due to industrial uses within the area.

The consultation process with key freight and heavy transport operators indicated that currently the majority of Council roads are utilised by industry as all roads are of a similar standard and consequently truck drivers will utilise the most direct route.

The proposed road hierarchy should include freight preferred routes designated as Rural Link Roads, with a focus around the Colac township.

It is acknowledged that there are a number of roads in regional areas that require heavy vehicle use. However the number of vehicles travelling along these roads is expected to be lower than those surrounding Colac. Consequently the preferred freight routes selected are those that link key industry and farming areas to the VicRoads arterial road network surrounding Colac. The

route chosen has been to generally provide a freight ring road around Colac and to supplement the existing VicRoads approved and managed freight routes.

The preferred freight routes identified are set out below, which were selected for further analysis (as discussed in Section 3):

- **Cape Otway Road:**
Linking the Birregurra township to Winchelsea-Deans Marsh Road (VicRoads managed road) which provides a connection to Winchelsea.
- **Barongarook Road, Bushby's Road, Old Yeo Road, Collins Road:**
Part of a southern and eastern bypass of the Colac township passing through areas of industry/farming.
- **Irrewillipe Road and Barrys Road:**
Part of a southern bypass of the Colac township passing through southern areas of industry/farming.
- **Warrowie Road and Mount Hesse Road:**
Link connecting to industry and farming areas to the north east of Colac. The link is between the Princes Highway and Eurack-Winchelsea Road.
- **Nalangil Road:**
North western link providing a connection to industry and farming areas to the north of Colac near Cororooke.

Please refer to Appendix A for the location of these freight routes.

2.4 Minimum standard requirements for freight routes

According to the AustRoads road design guidelines, the following geometric dimensions are required for vehicles with significant volumes of heavy vehicles and should be considered for the proposed freight routes within Colac Otway Shire:

- Lane width: 3.5 m – 3.75 m;
- Shoulder: 1 m – 1.5 m;
- Pavement thickness (traffic lane): 450 mm thick;
- Pavement thickness (shoulder): 300 mm thick;
- Clear zone: 9 m, assuming between 1,500 and 6,000 vehicles per day; and
- Line marking: Centre and edge lines.

3. Proposed Freight Routes

Heavy vehicles are progressively increasing in size on all parts of the Victorian road network. This means an increasing emphasis on greater road widths and clearances to accommodate these larger vehicles and their turning movement requirements.

To identify the improvements required for the five key freight routes within Colac Otway Shire, (identified as part of the industry consultation), a site inspection was undertaken on Friday 6 September 2013. The site inspection involved driving the length of each route identified and stopping at key locations to inspect specific details. The inspection was focused on road safety issues, pavement width and clear zone issues.

The safety assessment comprised a drive-through of the identified routes and focused on forward visibility, clear zone provision and any isolated features that present significant road safety problems (such as weak or narrow bridges). At 100km/h, the required stopping sight distance (SSD) for trucks on a flat road is 191m. Generally, it is considered that visibility is adequate along all surveyed routes. There are locations where tree canopies overhang the road or obstruct sight distance around a bend, but these issues are generally not of concern as they can be addressed with regular maintenance.

However, in many locations the clear zone requirements are not met. Dependent on traffic volumes the required clear zone is between 6.0m and 10.5m for a 100km/h road. In general, the roads are bordered by trees, fences and embankments, many of which would have to be removed or shielded to satisfy clear zone requirements. Generally, the objects at the edge of the roads also prevent any widening that would be required to bring the roads to a standard suitable for regular freight. These locations are identified in the sections below.

The key infrastructure issues include:

- Widening the pavement and shoulder widths to meet the 6.5m width requirement for B-double vehicles.
- Widening of culverts and/or bridges to accommodate the road width requirements.
- Relocating power poles to be outside of the clear zone requirements (due to widening of the road or poles being located within the existing clear zone).
- Removing vegetation from the road side to improve sight lines and provide sufficient clearances for the road widening works.

The following sections present a summary of the identified physical constraints for each key route. These constraints for each route have been mapped and included in Appendix B to Appendix F.

3.1 Cape Otway Road

The proposed freight route covering Cape Otway Road is approximately 16km long. The road runs approximately parallel to the Princes Highway, in an east – west alignment. The proposed route is sealed with a width of approximately 7.5m wide and generally with no formed shoulder. The road is typically at grade, and is bounded by pastoral land, with some managed vegetation on both sides of the road. The road reserve is approximately 15m wide, with table drainage on both sides. Power poles are typically on the north side of the road.

Figure 2 shows a typical cross-section on Cape Otway Road.



Figure 2 Cape Otway Road

There are approximately five culverts or bridges and three pipe culverts which may need to be widened as part of any works. Trees that run along the road reserve, forming a hedge, may need to be removed to allow for road widening. The power poles that run along the northern side of the road alignment may need to be relocated if any widening is to take place on this side of the road. A 20m length of table drain and adjacent field was noted to be flooded.

Figure 3 presents a summary of the locations where improvements are required along the assessed length of Cape Otway Road. Further details of the location of these improvements are included in Appendix B.

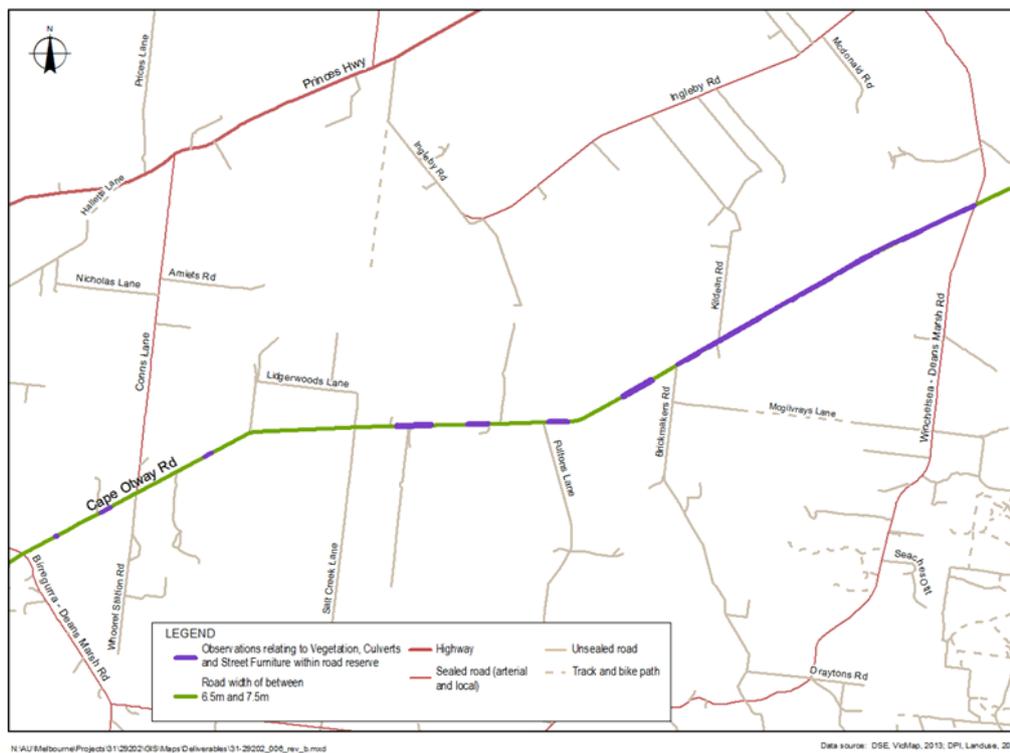


Figure 3 Cape Otway Road Improvement Projects

3.2 Barongarook Road, Bushby’s Road, Old Yeo Road, Collins Road

The proposed freight route covering Barongarook Road, Bushby’s Road, Old Yeo Road and Collins Road is approximately 14.8km long and generally follows a north-south alignment around the south east of Colac. The land is heavily vegetated to the south, interspersed with private residences. To the north of Oakley’s Road, the land becomes less undulating and more pastoral.

The proposed route is currently sealed and varies in width between 5.5m and 6.5m with no shoulder. Large amounts of vegetation run parallel to the road alignment within the road reserve which may require removal for any widening works.

Figure 4 shows a typical cross-section along the Barongarook Road, Bushby’s Road, Old Yeo Road and Collins Road route.

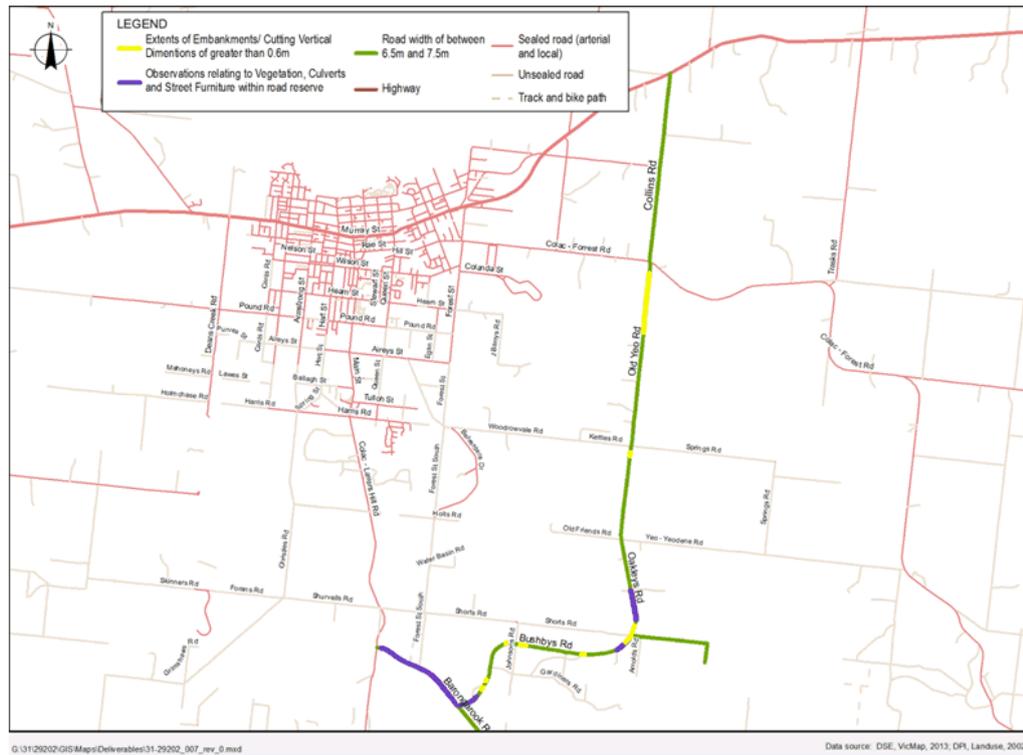


Figure 4 Barongarook Road

The road reserve includes a total cumulative length of 2.3km of embankments and 1.8km of cuttings. It should be noted that all earthworks are less than 3m in height or depth. The embankments and cuttings to the south of Oakley’s Road abut private residences which may require some acquisition to provide land for the future earthwork footprint. The embankments to the north approach culverts or minor bridges, each have safety barriers surrounding them.

Figure 5 presents a summary of the locations where improvements are required along the assessed length of Barongarook Road, Bushby’s Road and Old Yeo Road. Further details of the location of these improvements are included in Appendix C.

Figure 5 Barongarook Road, Bushby's Road, Old Yeo Road Improvement Projects



3.3 Nalangil Road

The proposed freight route covering Nalangil Road is approximately 9km in length and follows a north-south alignment, east of the southeast edge of Lake Corangamite. The land is typically pastoral, with very slight gradients at the midsection of the route. The road passes one farm.

The proposed route is currently sealed and varies in width between 5.5m and 6.5m. It should be noted that where the road is 5.5m or less in width that there tends to be 1m shoulders on either side.

The road is sealed, occasionally with shoulders. The road reserve is approximately 15m wide, but at earthworks the reserve fence was noted to be typically at the toe of embankments or crest of cuttings. Verge drainage was not noted. Cuttings were noted to be excavated within basaltic material. A 3m high, 80m long embankment was observed. Widening works on both sides of the embankment would be required. Based on the assumption that the embankment is constructed of locally sourced material, care should be taken to reduce the likelihood of differential settlement due to an increase in traffic loading and seasonal movement occurring between the widened material and current embankment construction.

Figure 6 shows a typical cross-section on Nalangil Road.



Figure 6 Nalangil Road

Other issues that may require addressing include a structure approximately 20m long which could be a box culvert or bridge which may require extension and the relocation of four power poles.

Figure 7 presents a summary of the locations where improvements are required along the assessed length of Nalangil Road. Further details of the location of these improvements are included in Appendix D.

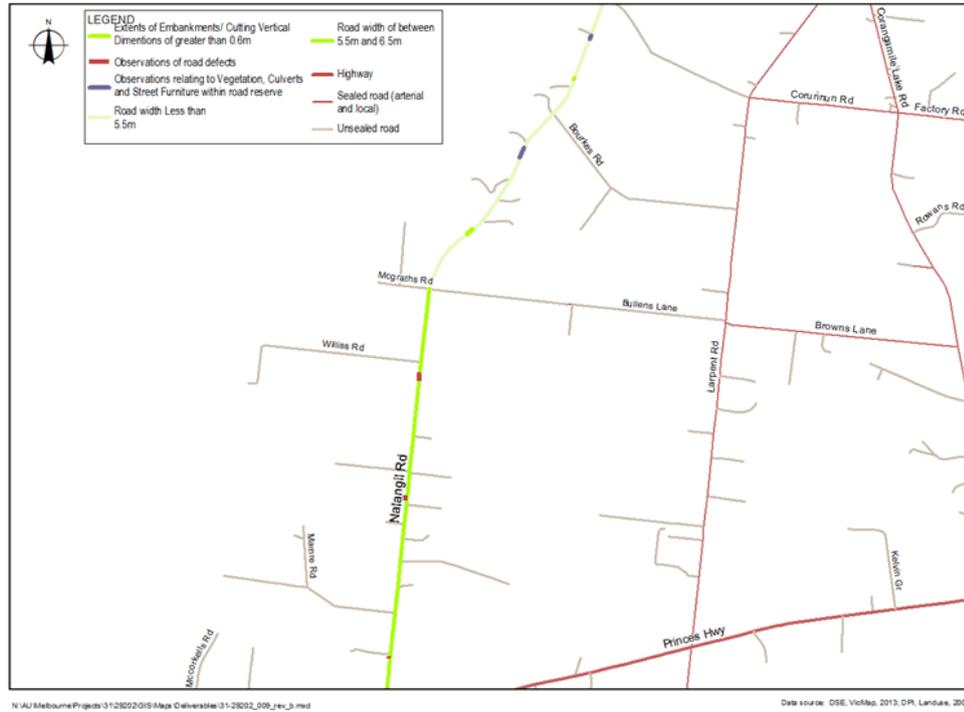


Figure 7 Nalangil Road Improvement Projects

3.4 Irrewillipe Road and Barrys Road

The proposed freight route covering Irrewillipe Road and Barrys Road is approximately 13.3km long and follows an east west alignment. The road passes through some arable fields bounded by mature vegetation, and residences, again typically bounded by mature vegetation. The land undulates, with the elevation varying from 160m to 290m above sea level.

The proposed route is currently sealed and is typically 6.0m in width with no shoulder. Large amounts of vegetation run parallel to the road alignment within the road reserve which may require removal for any widening works. The road reserve is typically 3m from the edge of the road and may have to be extended to accommodate any surface drainage measures required. Approximately 16 power poles would have to be relocated as part of the preparation works to the road widening works.

Five embankments (cumulative length of 700m) would have to be widened. The embankments are typically 2m high, although there is one 4m high embankment 60m long, with a culvert at its deepest location. A separate embankment is only 1.5m high, but approaches a culvert which is embedded within a stream bed. The culvert is on a slight bend in the road, and safety fencing is present over a length of approximately 30m. The width of the road at this location is approximately 6.5m. One other pipe culvert crossing was noted.

Figure 8 shows a typical cross-section along the Irrewillipe Road and Barrys Road route.



Figure 8 Irrewillipe Road and Barrys Road

Intersection widening with the use of a full depth pavement may be required at the intersection of Tomahawk Creek and Irrewillipe Road. However, it is noted that VicRoads manages and maintains this intersection and so any improvements here would require VicRoads funding.

Figure 9 presents a summary of the locations where improvements are required along the assessed length of Irrewillipe Road and Barrys Road. Further details of the location of these improvements are included in Appendix E.

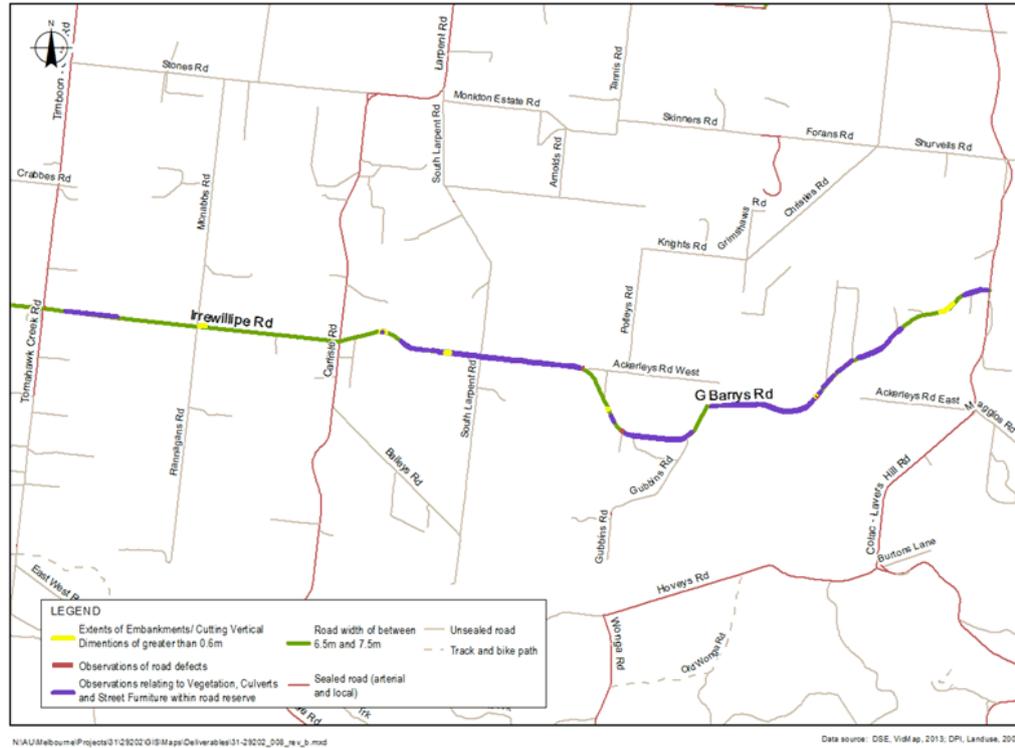


Figure 9 Irrewillipe Road and Barrys Road Improvement Projects

3.5 Warrowie Road and Mount Hesse Road

The proposed freight route covering Warrowie Road and Mount Hesse Road is approximately 23km in length in a north-south alignment east of Lake Colac and Lake Beeac. The land is typically flat to the south, with some slight rises and valleys to the north. The road passes by approximately eight residences and with a similar amount of farm and other rural industrial entrances.

The proposed route is currently sealed, along Mount Hesse Road the pavement is approximately 5.0m wide, and along Warrowie Road the pavement is approximately 6.5m. There are several areas of pavement, totalling approximately 7000m², that exhibit cracking and pavement deformation that indicate the pavement construction is too weak for the current traffic loading.

Figure 10 shows a typical cross-section on the Mount Hesse section of the Warrowie Road-Mount Hesse Road route.



Figure 10 Mount Hesse Road

The upgrade works would comprise widening works and pavement remediation works to enable the roads to adequately accommodate current and predicted freight use, and for maintenance works costs to be kept at a reasonable level throughout the design life.

The road is sealed but has no shoulders. The road reserve width is typically 15m in width and includes table drainage or 0.6m deep ditches within both verges. The road is typically at grade, and follows the gradual undulations of the land, along Mount Hesse Road. Embankments and cuttings are more frequent along Mount Hesse Road than Warrowie Road and are all less than 3m in height or depth. The length of embankment slope (considering both sides of the road) is 900m, while the length of cutting slopes is estimated at 1300m. Widening of these earthworks is expected to require localised widening of the road reserve.

The pavement defects typically comprised longitudinal cracking within the nearside wheel track of both lanes. In places, the pavement has deteriorated and rutting and pot holes and alligator cracking accompany the longitudinal cracks. The cause of this is thought to be weak base, sub grade layer or poor drainage. Should widening works be carried out along this freight route, the works should include the full depth reconstruction of existing pavement where cracking is observed.

It was noted that the safety fencing and signs associated with the bridge crossing some 20m east of the junction of Eurack-Wichelsea Road, and Mount Hesse Road, significantly reduced the visibility of vehicles turning out of Mount Hesse Road.

A 20m length of table drain and adjacent field was noted to be flooded.

Other issues that may require addressing include a pipe culvert that may require extending, the relocation of 13 power poles and the removal of vegetation within the road reserve.

Figure 11 presents a summary of the locations where improvements are required along the assessed length of Warrowie Road and Mount Hesse Road. Further details of the location of these improvements are included in Appendix F.

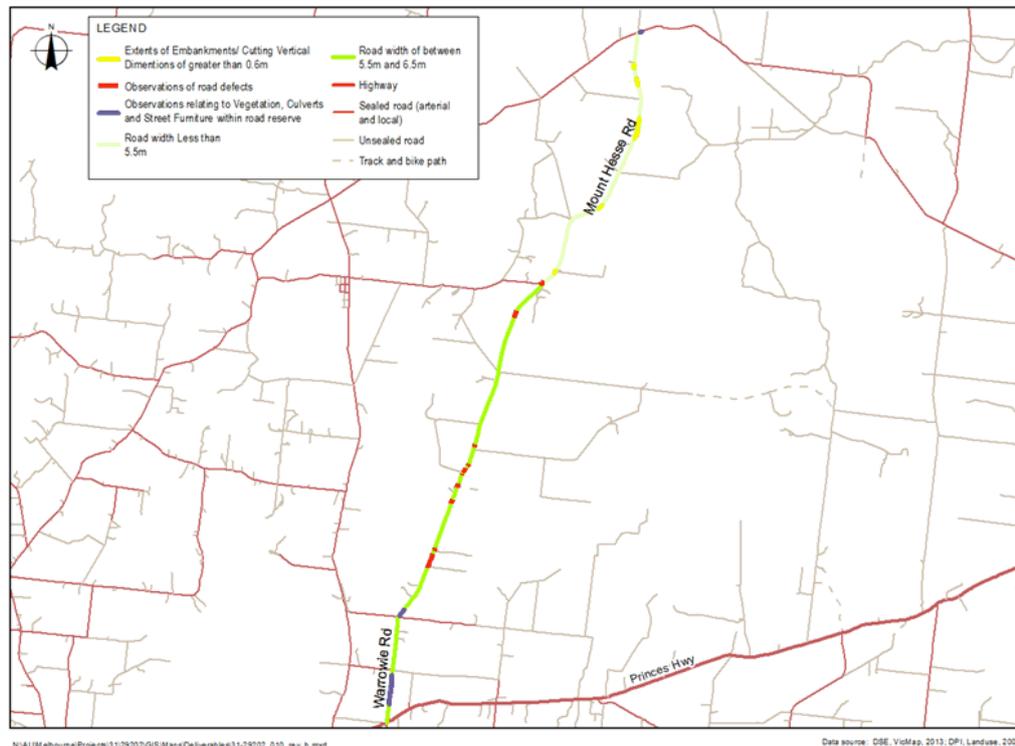


Figure 11 Warrowie Road and Mount Hesse Road Improvement Projects

3.6 Summary of Findings

The site inspections of the key freight routes identified the key elements of the road lengths assessed. Table 2 presents a summary of the key findings of each route inspected.

Table 2 Summary of Site Inspection Findings

| Road | Current Road Widths | Sealed Shoulders | Line Marking | Hazards |
|--|---------------------|------------------|--------------|---------------------------------------|
| Cape Otway Road | 6.5-7.5 m | None | Centre line | Power poles
Culverts
Vegetation |
| Barongarook Road, Bushby's Road, Old Yeo Road and Collins Road | 5.5-6.5 m | None | Centre line | Vegetation
Earthworks |
| Nalangil Road | 5.5-6.5 m | 1 m shoulder | Centre line | Power poles
Culverts
Vegetation |
| Irrewillipe Road and Barrys Road | 6.0 m | None | Centre line | Power poles
Culverts
Vegetation |
| Warrowie Road and Mount Hesse Road | 5.0 – 6.5 m | None | Centre line | Power poles
Culverts
Vegetation |

Based on the site inspections of the five freight routes and an understanding of the current and future local road hierarchy a prioritised list of these freight routes has been developed. The prioritised order for the five routes is as follows:

- a. Barongarook Road, Bushby's Road, Old Yeo Road and Collins Road.
- b. Irrewillipe Road and Barrys Road.
- c. Nalangil Road.
- d. Warrowie Road and Mount Hesse Road.
- e. Cape Otway Road.

The prioritisation has focused on developing a local road network ring around the southern side of the Colac township to accommodate the local industry within the area. By upgrading these roads to create this freight network ring (routes "a" and "b") will assist to encourage heavy vehicles to access properties from this road and also has the potential to encourage growth within the region.

The other routes provide the local road provision between the VicRoads managed arterial roads, they have been prioritised based on the understood level of heavy vehicle usage.

While, the prioritised order is recommended to provide the greatest benefit to the region and industry changes to demand may alter and therefore may result in priorities for upgrades needing to be amended to meet the demand.

4. High Level Cost Estimate

This section outlines high level cost estimates of the recommendations for each of the five proposed freight routes outlined in Section 3. Depending on the recommendation, the estimated cost could be fully borne by Colac Otway Shire Council or shared with VicRoads or Department of Transport, Planning and Local Infrastructure. For simplicity, the full cost of improvement works has been provided.

The cost estimates have been prepared for the purpose of gaining an understanding of the relative scale of the recommendations only and must not be used for any other purpose. Maintenance costs have not been considered as part of this assessment.

The cost estimate is high level only. They have been developed based on unit rates and have not been based on any conceptual designs. Actual prices, costs and other variables may be different to those used to prepare the cost estimates and may change. No detailed quotation has been obtained for any of the recommendations. GHD does not represent, warrant or guarantee that the works can or will be undertaken at a cost which is the same or less than the cost estimate. The cost estimates do not include any allowance for land acquisition or service relocation, nor do they include indexing as it is not possible to determine when these works will be undertaken.

It is recommended that Colac Otway Shire Council further investigate recommendations (e.g. develop concept designs for more accurate costing, undertake feature survey of the routes) it wishes to implement.

The cost estimates for the recommendations are presented in Table 3.

Table 3 Estimated cost for each route

| Route | Item | Estimated Cost |
|--|------------|----------------|
| Barongarook Road, Bushby’s Road, Old Yeo Road, Collins Road. | Road works | \$2.95M |
| Irrewillipe Road and Barrys Road. | Road works | \$4.9M |
| Nalangil Road. | Road works | \$1.9M |
| Warrowie Road and Mount Hesse Road. | Road works | \$2.7M |
| Cape Otway Road. | Road works | \$4.5M |

5. Funding Opportunities

Generally funding for Council managed roads is through Council budgets, which are an allocation from overall federal funding. However funding from State and Federal governments is possible for specific links or upgrades if there is a regional or State benefit and demonstrated need for the funding.

The key rationale for funding is generally through economic growth of the region, which in regards to the Colac Otway Shire would be in relation to freight movements and/or tourism movements.

It is important to recognise that opportunities to access funding programs/sources are part of an integrated consideration. There are three general phases of project development that influence funding requirements and strategy. These are as follows:

- Return on assets - what financial/economic return is required to offset the cost?
- Procurement options – mechanism for investment funding; and
- Funding sources.

Additionally, this area is constantly evolving and new opportunities to fund projects can be explored. More recent trends are discussed below.

5.1 Funding Framework

Funding applications for local roads within the Colac Otway Shire to State or Federal governments are usually in the form of a business case. A business case document should identify the needs and drivers for the project and to outline the expected benefits and costs of the project. The benefits and needs of each project should be justified with statistics.

The key industries within the Colac Otway Shire that could assist to provide justification for future project funding applications include:

- Timber – for local road projects in the south of the Shire;
- Dairy – for local road projects in the south western areas of the Shire;
- Tourism – for key road links connecting townships, particularly north south connections to encourage travel beyond the Great Ocean Road;
- Farming/Grazing/Meat – for local road projects in the north west of the Shire; and
- Heavy vehicle access for local industrial areas around Colac.

The key elements of each funding application should include:

- **Key drivers for the Colac Otway Shire.** This will generally be focused on tourism, freight (industries mentioned above) and road safety. Local road upgrades assist to sustain and improve competitiveness within the supply chain for freight and encourages growth of tourism within the region.
- **Key reasons/drivers for this project to be undertaken.** This should also include the outcomes if the upgrade/project is not undertaken.
- **Expected benefits of the project.** Benefits will vary depending on the type of project but are essential to justify funding approval. These benefits should be verified with evidence as much as possible. The benefits could include:
 - Tourism: If projects upgrade road links from current key tourist destinations along the Great Ocean Road to townships inland within the Colac Otway Shire, growth in

tourism could occur. Visitors are more likely to utilise a good quality road and explore the region if the drive is comfortable than travel along an uneven surface to an unknown destination.

- Freight: Widening the road width to be able to accommodate two heavy vehicles passing will prevent vehicles travelling on the pavement edge and causing breakup of the pavement. This will reduce overall maintenance of the road as the pavement is able to better accommodate heavy vehicles compared to the pavement edge.
- **Anticipated costs for the project.** Costs of the project will be an important component of the funding application to enable funds to be secured and to establish the benefit cost ratio for the project. This will assist to identify if the project is viable to proceed and will enable the State and Federal Governments to assess the funding application compared to other applications.

A summary of the funding framework is provided in Table 4.

Table 4 Funding Framework Summary

| Project Need | Project Benefits | Data Requirements |
|--|--|---|
| <ul style="list-style-type: none"> • Increasing heavy vehicle usage of roads. • Road Safety. | <ul style="list-style-type: none"> • Freight efficiency. • Reduced long term maintenance costs. • Tourism – increasing GRP. | <ul style="list-style-type: none"> • Freight volumes currently utilising the local roads. • Forecasted freight volumes on the local roads. • Expected industry growth. • Forecasted tourism growth. |

5.2 Return on asset investment

With a number of competing priorities and limited project funding available, a transparent and robust project prioritisation process is the key to effective project selection and investment.

Before the project is considered for funding, financial or economic benefits (monetary, travel time, travel safety, environmental, employment) need to be considered. Effective economic evaluation of projects is crucial in enabling the prioritisation and achieving return on investment.

Most state and territory jurisdictions are using the ATC (2006) National Guidelines for Transport System Management in Australia in one form or another. The level of use and adherence to these guidelines is dependent on the level of requirements of the funder e.g. Commonwealth and/or State Governments.

The State Government and other jurisdictions require a qualitative assessment of the options in terms of how they align against broader objectives, policies and strategic settings; and a consistent approach to economic evaluation via a full detailed Benefit Cost Analysis for those projects considered to have strategic merit. The sequential stages are structured to ensure that decisions are taken in an objective and systematic way, thus leading to the adoption of the most effective and efficient policy solutions. Given growing budgetary constraints, there is a trend amongst funding agencies to insist on initially investigating non-infrastructure solutions to resolve issues, rather than immediately use an infrastructure solution to solve problems.

Some of the lack of transparency is due to the fact that the overall budget tends to be broken into programmes with a discrete allocation of funding, with projects remaining captured within their programmes without the opportunity to be compared across the network (there is a sense of frustration by network managers that insufficient funds will mean that they will never be able to achieve best optimisation of the network).

5.3 Procurement assessment

A detailed procurement assessment is essential in helping to identify an effective funding mechanism. The delivery method of the strategy will ensure minimal disruption to the surrounding environment, users and businesses and aims to ensure that inputs from various stakeholders are managed effectively. Risks will need to be allocated appropriately and there will need to be a 'value-for-money' outcome for the State.

The key objectives of the procurement strategy include:

- To ensure all outputs are delivered to the quality required, within the specified timeframe and within project budget;
- The delivery method will minimise any disruption to the surrounding environment, users and business;
- To ensure that the various stakeholder inputs are managed effectively; and
- To allocate risks appropriately.

To ensure value for money outcomes for the State, issues that are considered as part of procurement strategy may include:

- Understanding of project requirements and specifications;
- Time certainty
- Cost;
- Innovation;
- Project complexity;
- Risk understanding and transfer; and
- Supplier base.

The project delivery methods vary in risk allocation between the principal and service provider in terms of project delivery risks, costs and performance of the completed project. The options typically include traditional, performance, management, finance or relationship based delivery methods and some examples are outlined in Table 5.

Table 5 Procurement methods assessment

| Procurement Model | Comments |
|---|--|
| Traditional procurement / head contractor | <p>This is a commonly used form of contract under which the Principal would have full responsibility for design and documentation developed by a design team engaged under a separate contract.</p> <p>The principal issues tenders for construction in accordance with the design. Contractor is engaged under a single lump sum agreement.</p> |
| Design & construct | Under this option the Principal with support from its technical advisers prepares functional and performance requirements documents and seeks tenders for completion of all phases of the project including design documentation phase, construction phase and commissioning phase. |
| Design, construct and maintain | Under this option the Principal with support from its technical advisers prepares functional and performance requirements documents and seeks tenders for completion of all phases of the project including design documentation phase, construction phase, commissioning phase and maintenance phase. Additionally the contractor has ongoing maintenance obligation (and/or operation obligations). |
| Construction management | <p>The Principal would, with support from its technical advisers and the project manager, prepare a functional key user performance requirements document, and seek tenders for the respective engineering, procurement, construction and management services.</p> <p>The engineering, procurement, construction and management contractor adopts an advisory role, acting as an agent responsible for managing and coordinating each of the other subcontractors (i.e. construction). However, the sub-contractors are engaged by and report directly to the Principal.</p> |
| Managing contractor | Under this form of contract the Principal would appoint a Managing Contractor who takes responsibility for all works and delivery risks for a lump sum management fee. The Managing Contractor may engage sub-contractors and/or deliver parts of the assignment directly |
| Alliance | The Principal would collaborate with a contractor and/or designers to share risks and responsibilities. All risks would be shared by the alliance participants. |
| Partnership Victoria-Public Private Partnership | A consortium contracts to Design, Construct, Maintain and Finance the facilities and assumes responsibility for facilities maintenance and asset replacement over a defined period (typically around 30 years). |

5.4 Funding options

In general the sources of funding opportunities for transport infrastructure remain constant, however the funding program titles, objectives and requirements are frequently updated. Therefore any information on funding sources is general in nature and needs to be reviewed when an investment is ready for consideration. Table 6 provides an outline of typical funding options.

Table 6 Funding options

| Source | Current status | Target/Comments |
|---------------------------------|---|--|
| Commonwealth Government | Regional Infrastructure Fund | Stream 1 – commitments made in 2010 (\$916 mill)
Stream 2 – Economic infrastructure projects and Regional Infrastructure Planning projects (\$2.5 billion)
Stream 3 – Regional Development Australia Fund (1 billion including 573 mill from RIF) |
| | Financial Assistance Grants to Local Government | Provided under the Local Government (Financial Assistance) Act 1995. |
| | Funding programs to local Governments | Includes: <ul style="list-style-type: none"> • Roads to recovery program • Regional and Local Community Infrastructure Program • Black Spot Program |
| State Government (VIC) | State budget | Annual BERC budget process – process must comply with Investment Lifecycle and High Value High Risk guidelines.
Application of Investment Management process |
| | Regional Growth Fund | Administered by Regional Development Victoria under the Regional Growth Fund Act 2011 |
| | Access support programs | Programs that focus on connectivity and facilitation of access to transport modes.
They include for example:
Transport Connections
TravelSmart |
| | VicRoads blackspot and road safety program | VicRoads have a blackspot and road safety program to upgrade road length or intersections with road safety issues.

This funding is typically for VicRoads managed roads however there may be potential opportunities to utilise some of this funding if there are particular road safety issues on Council managed roads. |
| Private and/or industry funding | Limited application | Involving the freight sector and/or owners of properties where Council roads are utilised to access these properties. This could be in the form of direct charging for the use of infrastructure, such as road pricing, allocates it to higher value uses. Infrastructure that is provided to facilitate economic activities may be funded by the beneficiaries. |

6. Conclusion

This strategic network review has analysed the current use of the local road network to transport freight throughout Colac Otway Shire. Through consultation with the trucking industry, it is clear that there is a freight demand on local roads. However this is hampered by poor quality and narrow roads.

There is potential benefit in focusing Council's resources on maintaining some key freight roads within the Shire to improve the local road network to accommodate the current and future freight demand.

This review analysed five key freight routes that were identified through the consultation process. These routes provide connectivity around Colac in addition to the VicRoads declared road network. The routes that are recommended to be focused on include the following:

- Barongarook Road, Bushby's Road, Old Yeo Road, and Collins Road.
- Irrewillipe Road and Barrys Road.
- Nalangil Road.
- Warrowie Road and Mount Hesse Road.
- Cape Otway Road.

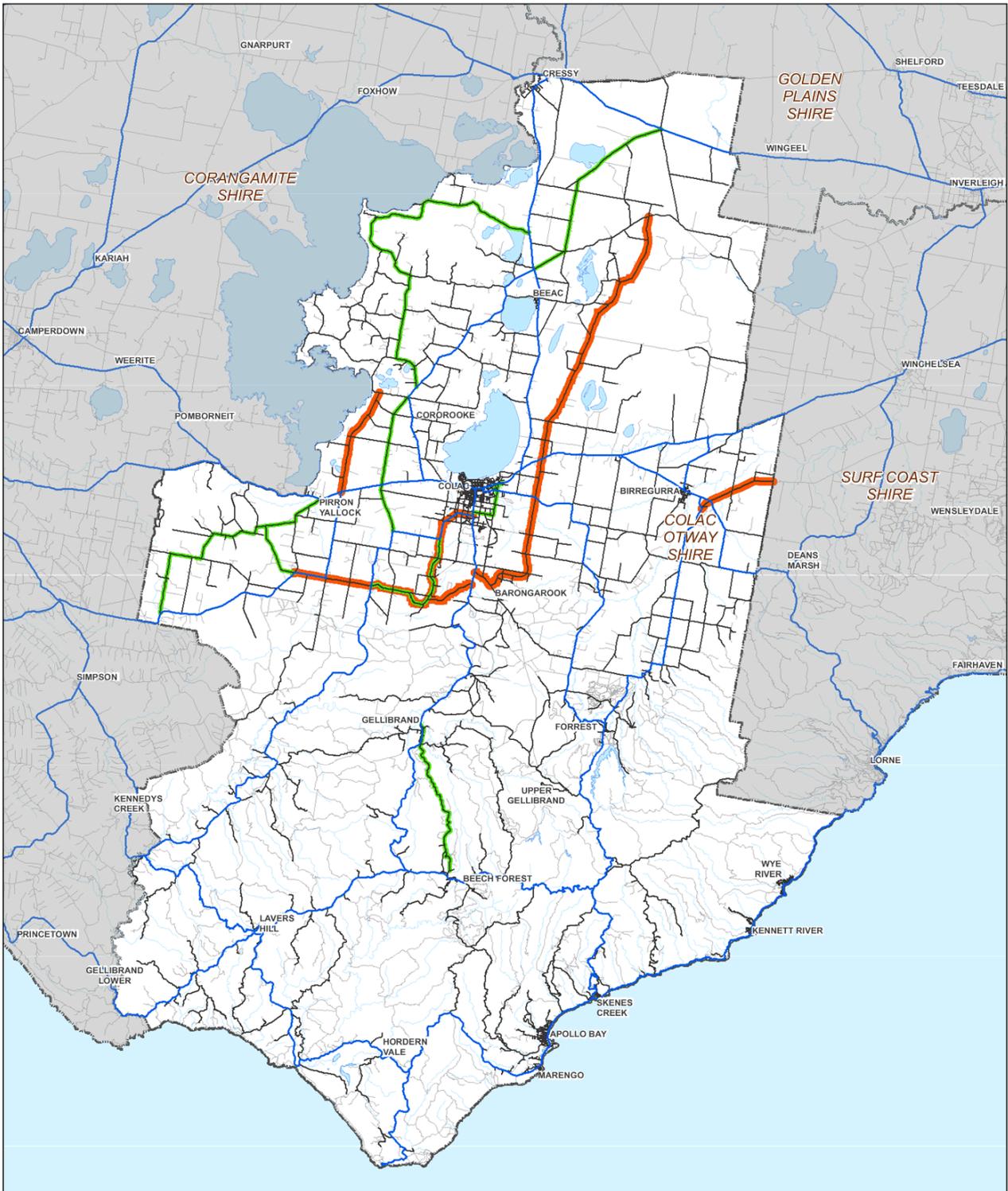
While it is acknowledged that there are other freight routes within Colac Otway Shire, the routes selected above are likely to carry higher traffic volumes into areas that are not well serviced by the declared road network and therefore provide greater benefit to the local industry when compared to other potential freight routes.

High level cost estimates have been developed for the upgrading of these routes to meet AustRoads standards. These costs are indicative only and should be reassessed once concept designs have been developed for any proposed works.

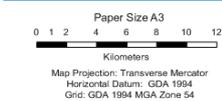


Appendices

Appendix A – Cape Otway Shire Road Network and Surveyed Freight Routes



- LEGEND**
- Colac Otway Shire managed roads
 - VicRoads managed roads
 - Council strategic freight routes
 - GHD surveyed roads



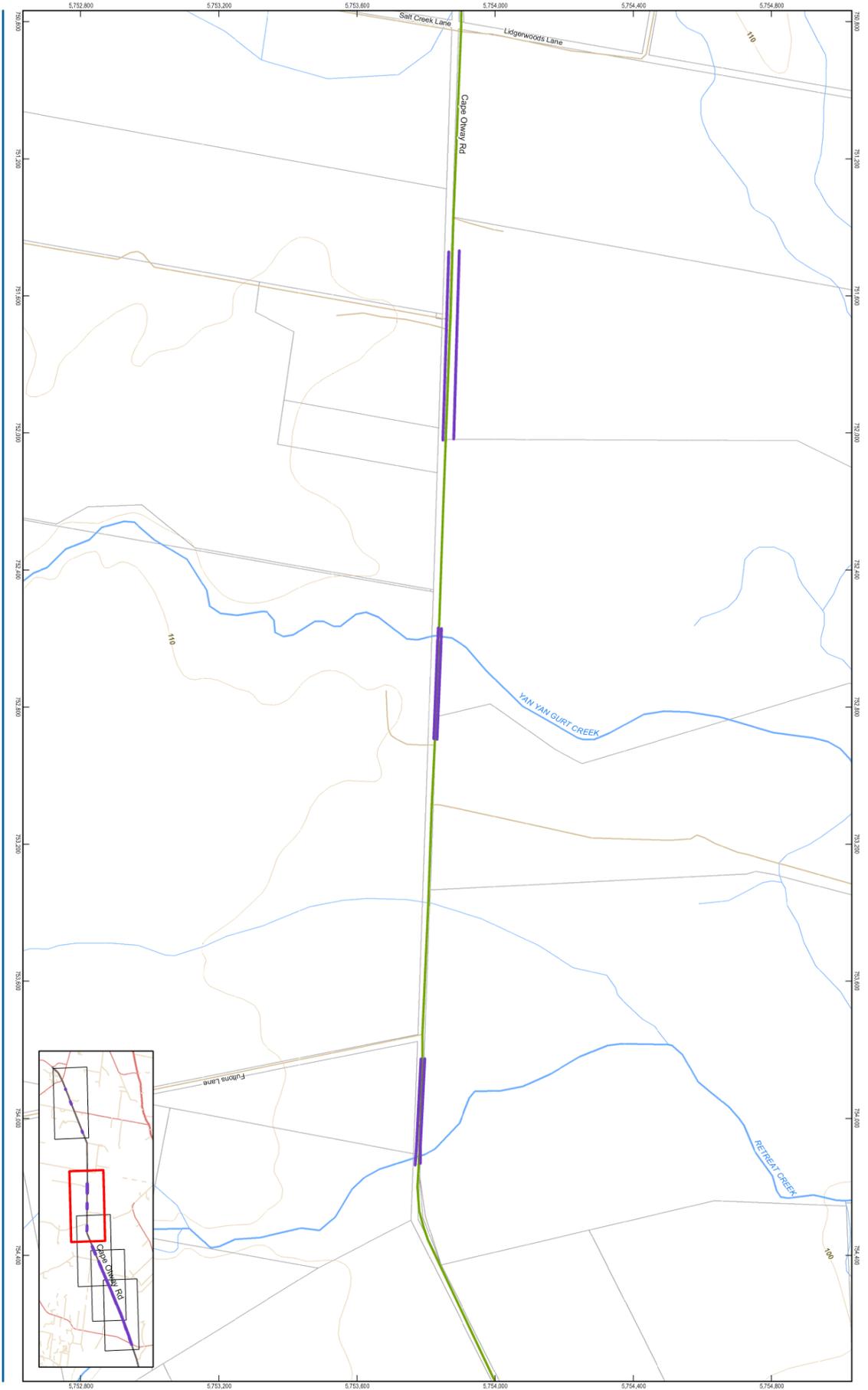
Colac Otway Shire Council
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**Cape Otway Shire
 Road network**

Job Number 31-29202
 Revision E
 Date 28 Apr 2014

Appendix A

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Appendix B – Cape Otway Road Issues Map



Paper Size A3
 0 50 100 200 300 400
 Metres



- LEGEND**
- Observations relating to Vegetation, Culverts and Street Furniture within road reserve
 - Unsealed road
 - River
 - Stream
 - Contour 10 m
 - Practical
 - Road width of between 5.5m and 7.5m
 - Sealed road (asphalt and local)

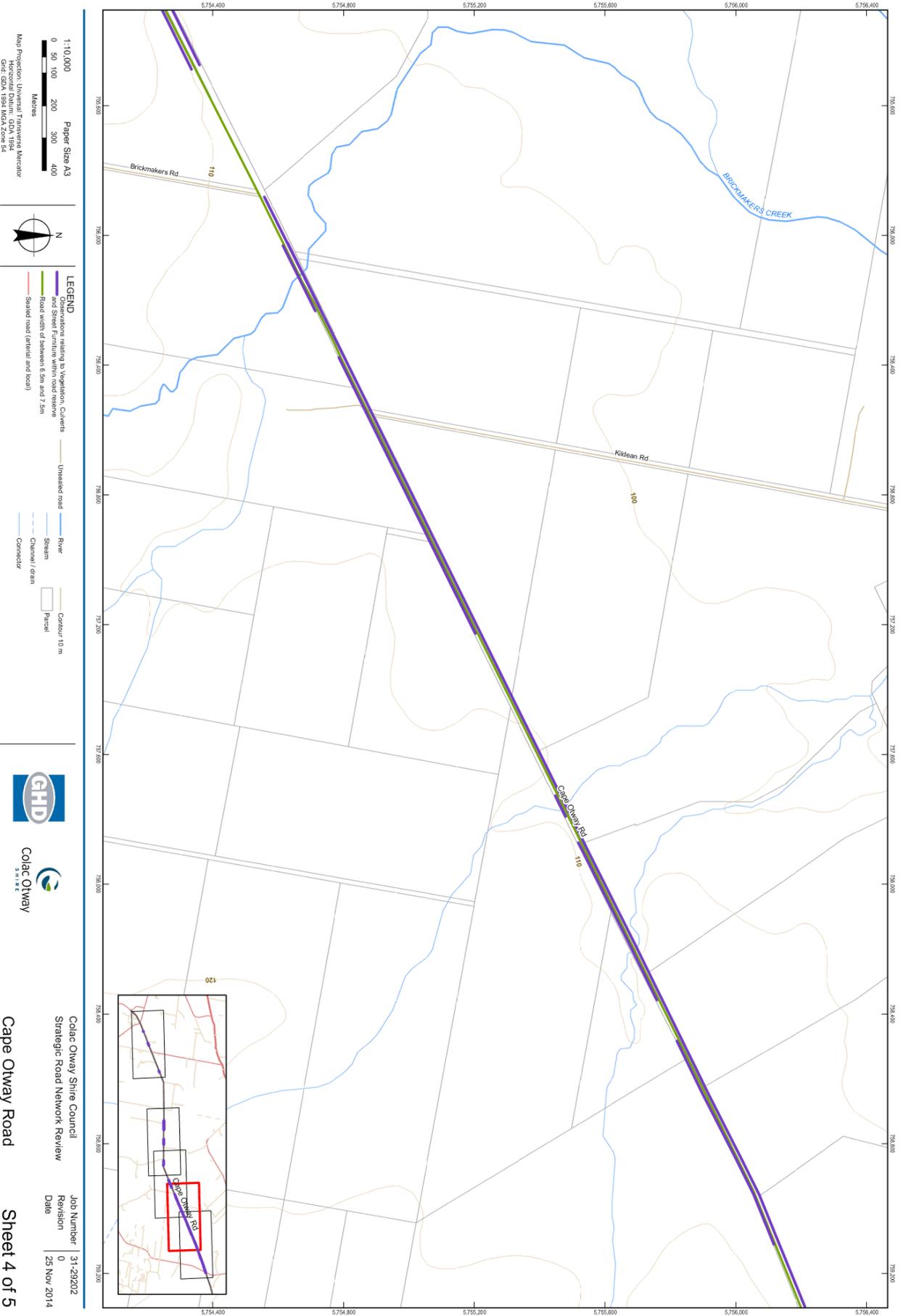


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Cape Otway Road
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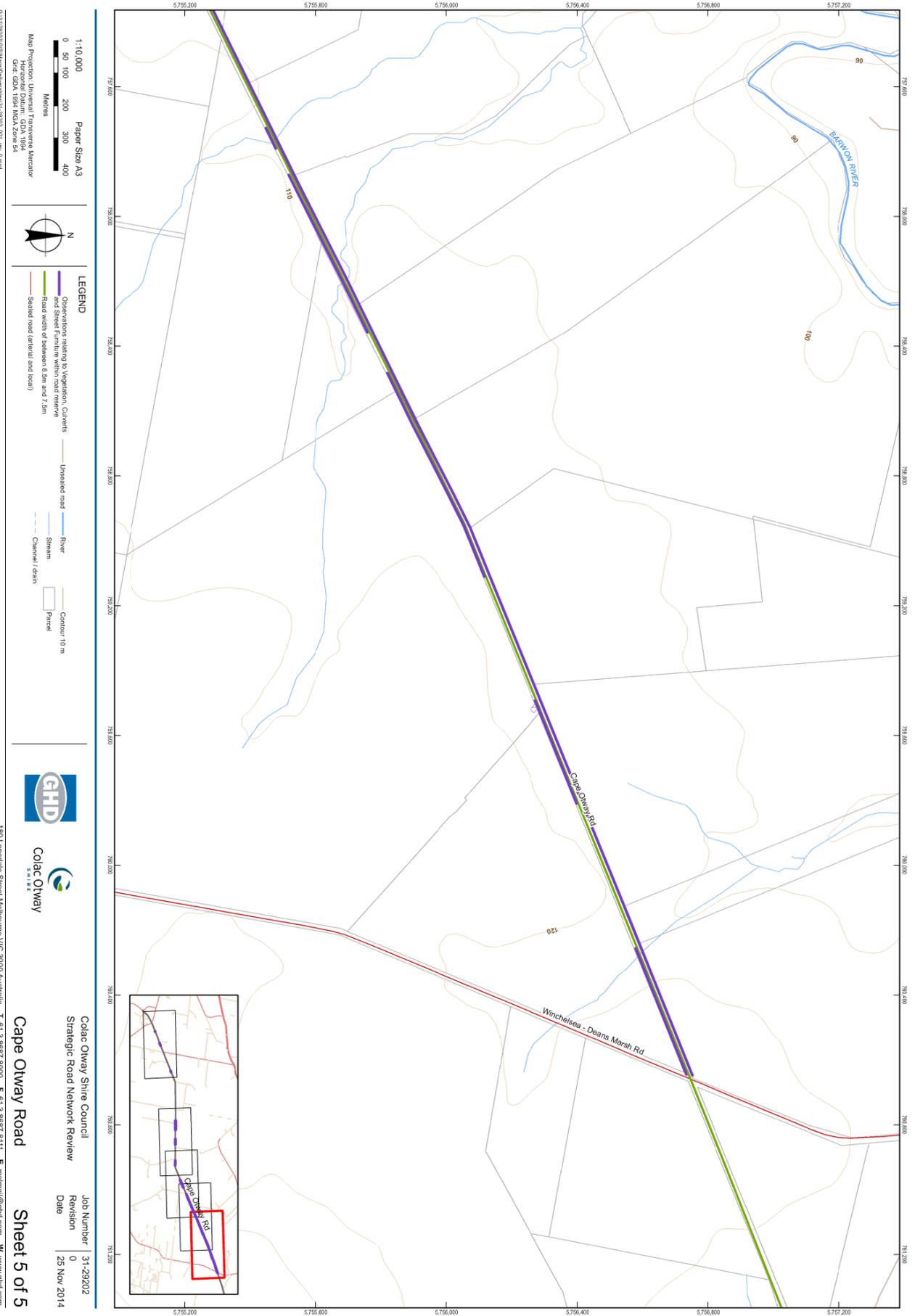


Cape Otway Road

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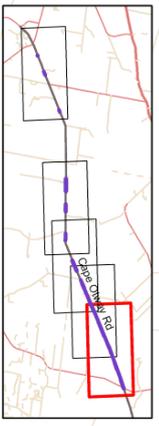


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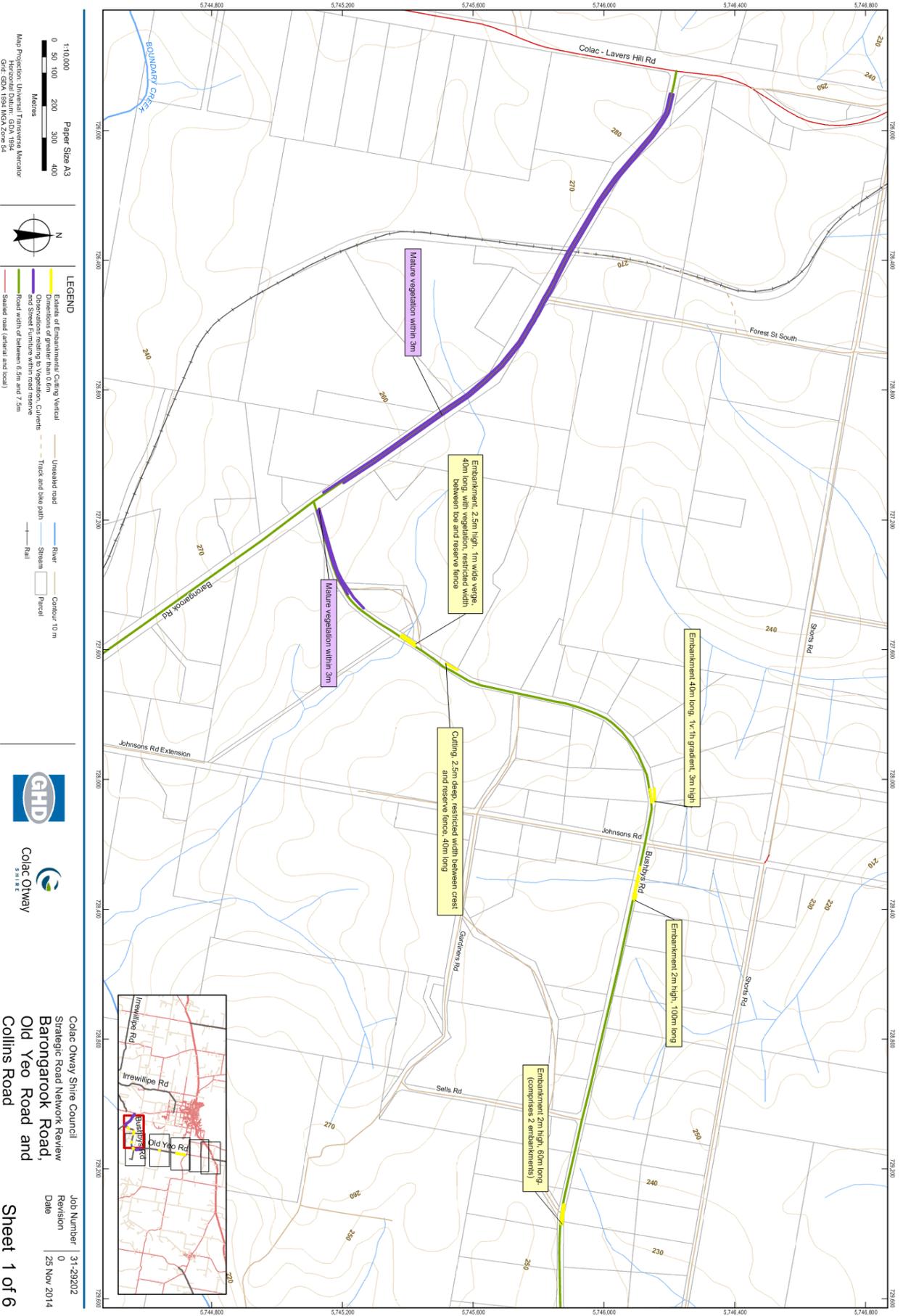


Cape Otway Road
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Appendix C – Barongarook Road, Bushby’s Road, Old Yeo Road, Collins Road Issues Map



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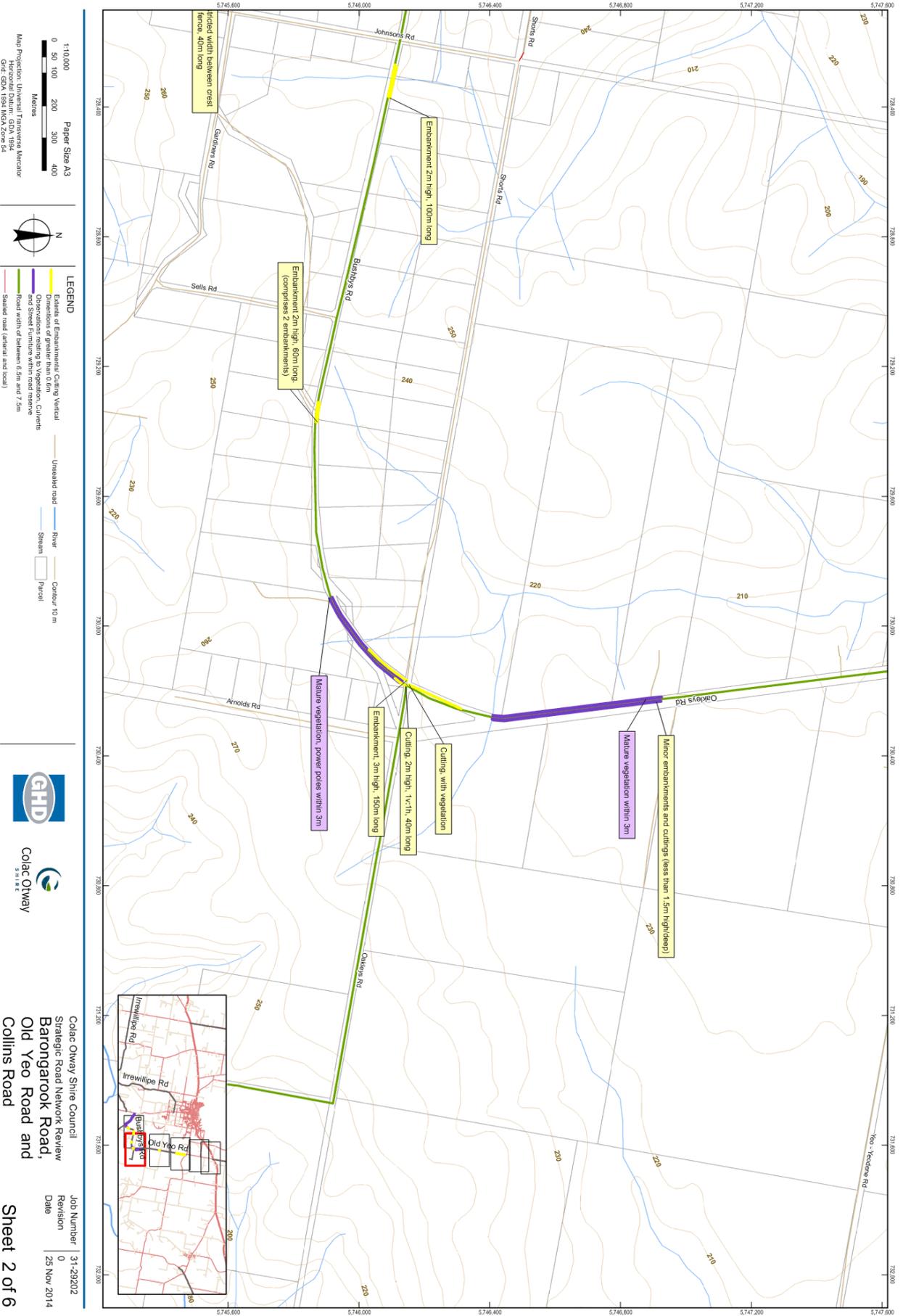


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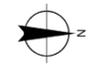
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 Strategic Road Network Review
 Barongarook Road,
 Old Yeo Road and
 Collins Road

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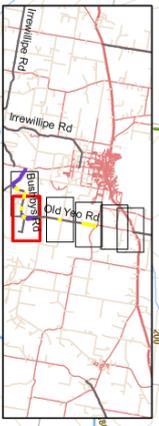
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 Horizontal Datum: GDA 1984
 Grid: GDA 1984 MGA Zone 54



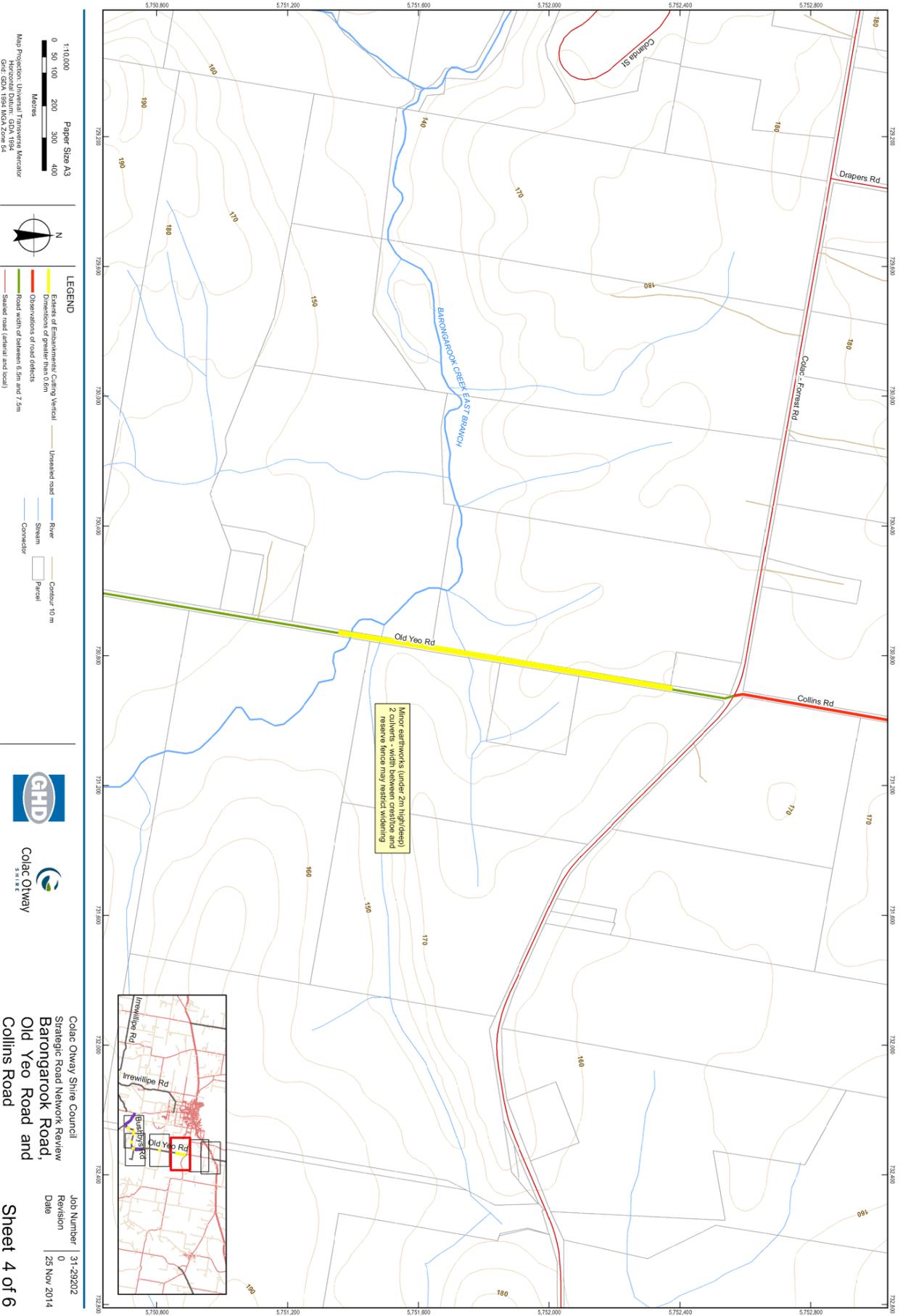
LEGEND
 Embankment Cutting Vertical
 Dimensions of greater than 6m
 Operations relating to Vegetation, Cuttings
 Road width of between 6.5m and 7.5m
 Standard road (dashed and solid)
 Unsealed road
 River
 Stream
 Contour 1m
 Parcel



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 Collins Road
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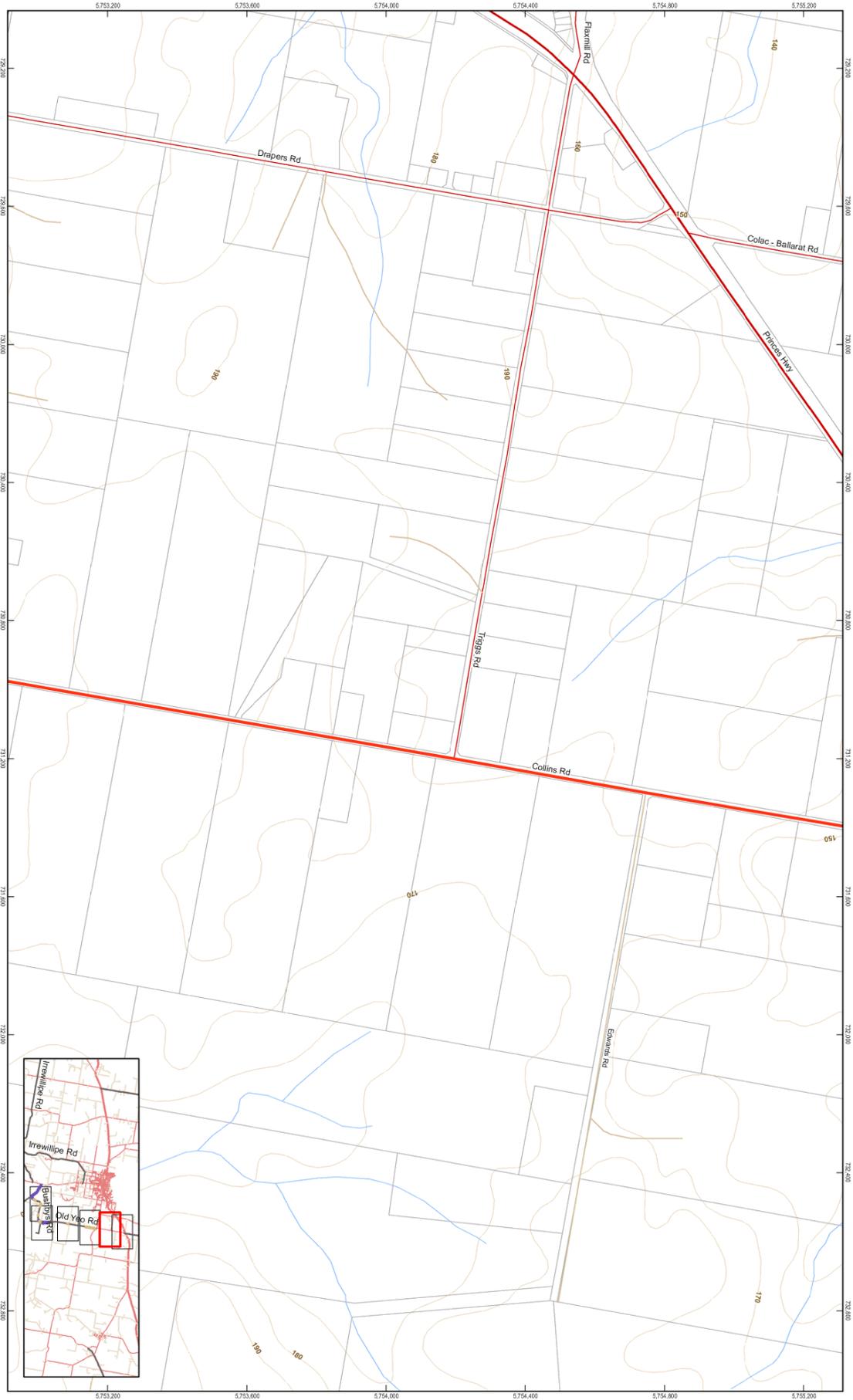


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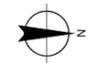
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- LEGEND**
- Observations of road defects
 - Sealed road (external and local)
 - Unsealed road
 - Stream
 - Contour: 10 m
 - Parcel



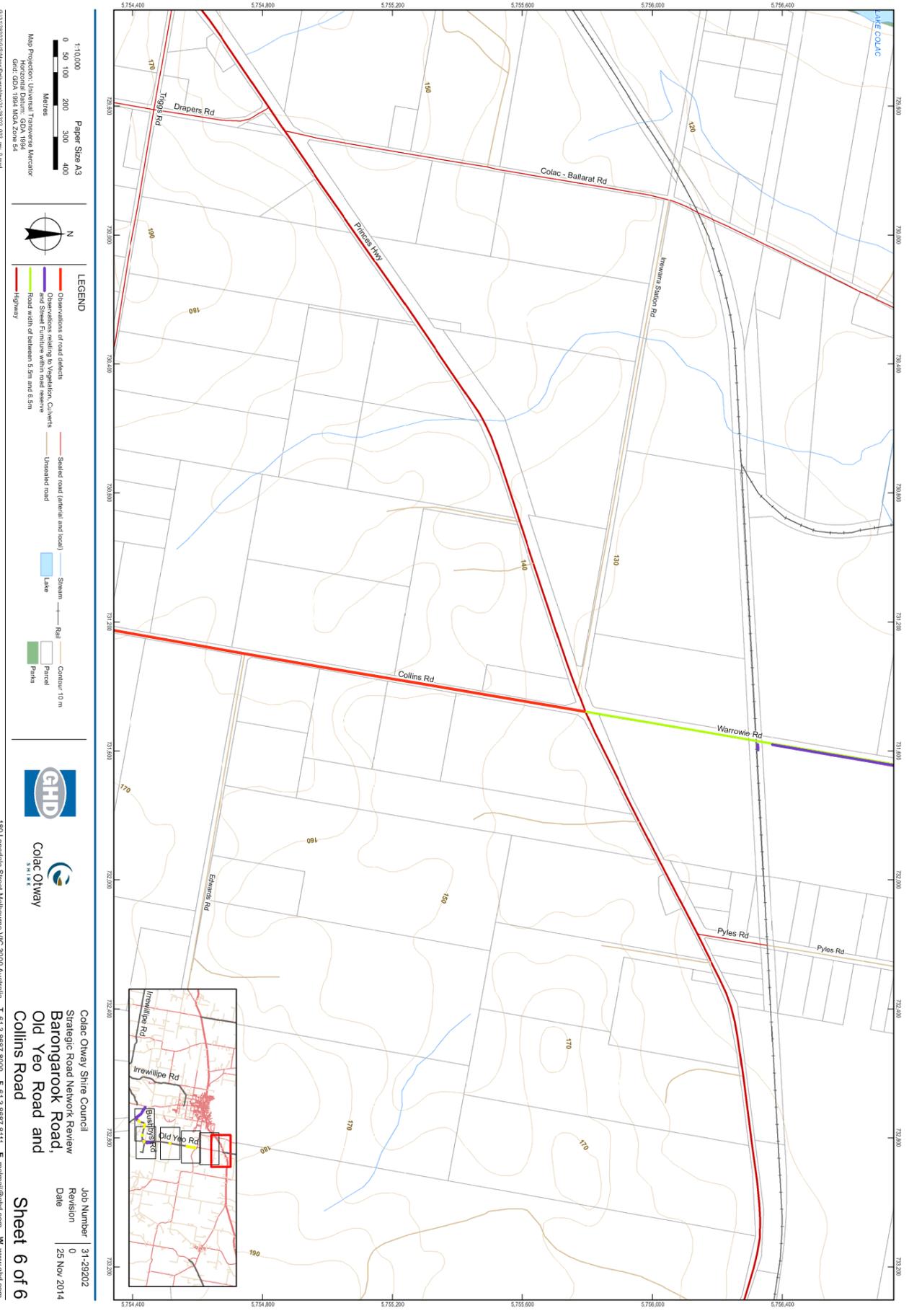
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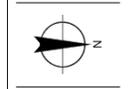
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- LEGEND**
- Observations of road defects
 - Observations relating to Vegetation, Culverts
 - Sealed road (asphalt and coal)
 - Unsealed road
 - Road width of between 5.5m and 6.5m
 - Highway
 - Stream
 - Lake
 - Rail
 - Contour 10 m
 - Parcel
 - Pits



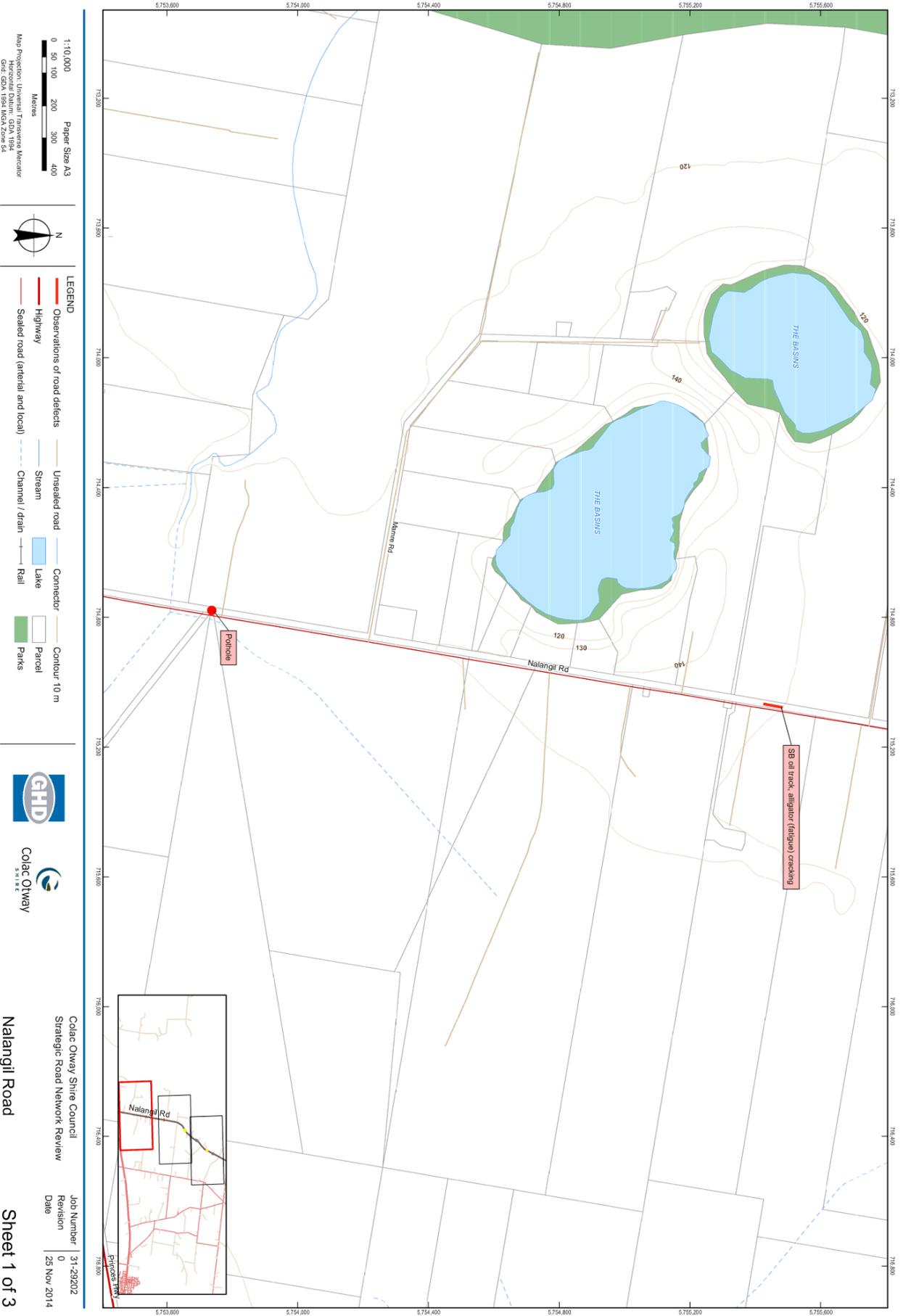
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Appendix D – Nalangil Road Issues Map



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Map Projection: Universal Transverse Mercator
Horizontal Datum: GDA 1984
Grid: GDA 1984 MGA Zone 54

LEGEND

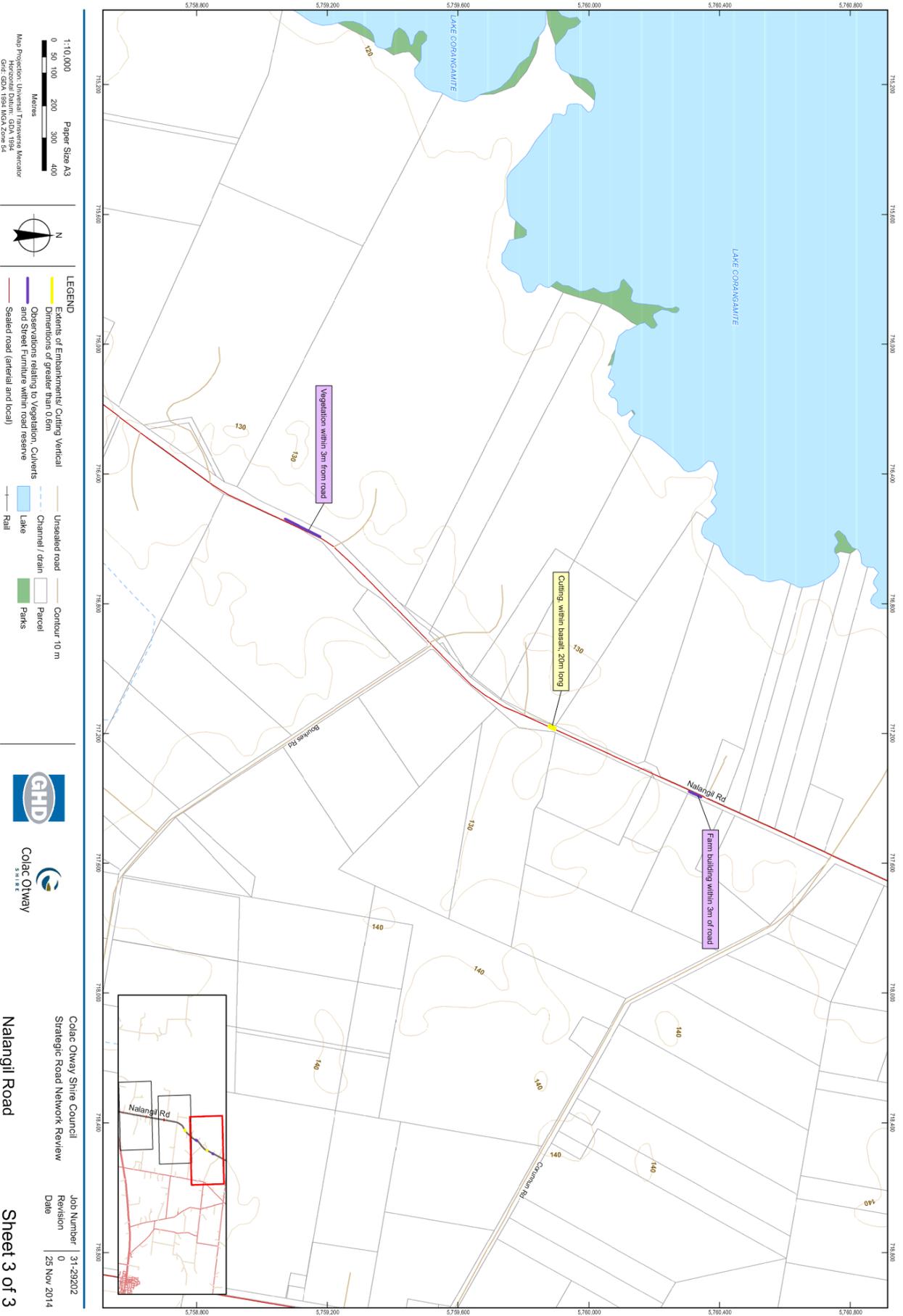
- Observations of road defects
- Highway
- Sealed road (arterial and local)
- Unsealed road
- Stream
- Channel / drain
- Connector
- Lake
- Parcel
- Contour 10 m
- Rail
- Parks

Colac Otway Shire Council
Strategic Road Network Review

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Nalangil Road
Sheet 1 of 3

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Metres

Map Projection: Universal Transverse Mercator
Horizontal Datum: GDA 1984
Grid: GDA 1984 MGA Zone 54

LEGEND

- Extents of Embankments/Cutting Vertical
- Dimensions of greater than 0.6m
- Observations relating to Vegetation, Culverts and Street Furniture within road reserve
- Sealed road (arterial and local)
- Unsealed road
- Channel / drain
- Lake
- Rail
- Contour 10 m
- Parcel
- Parks



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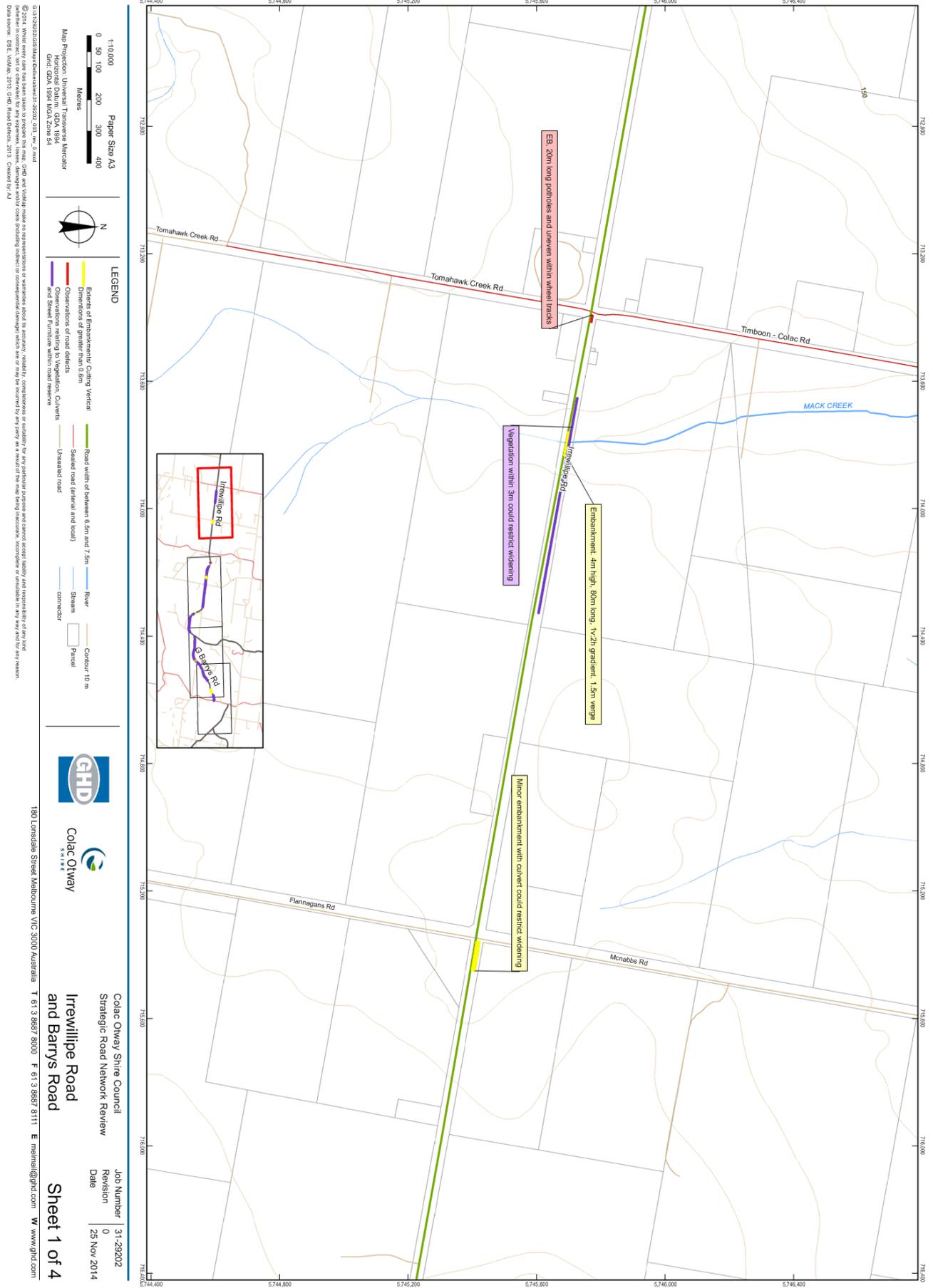
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Strategic Road Network Review

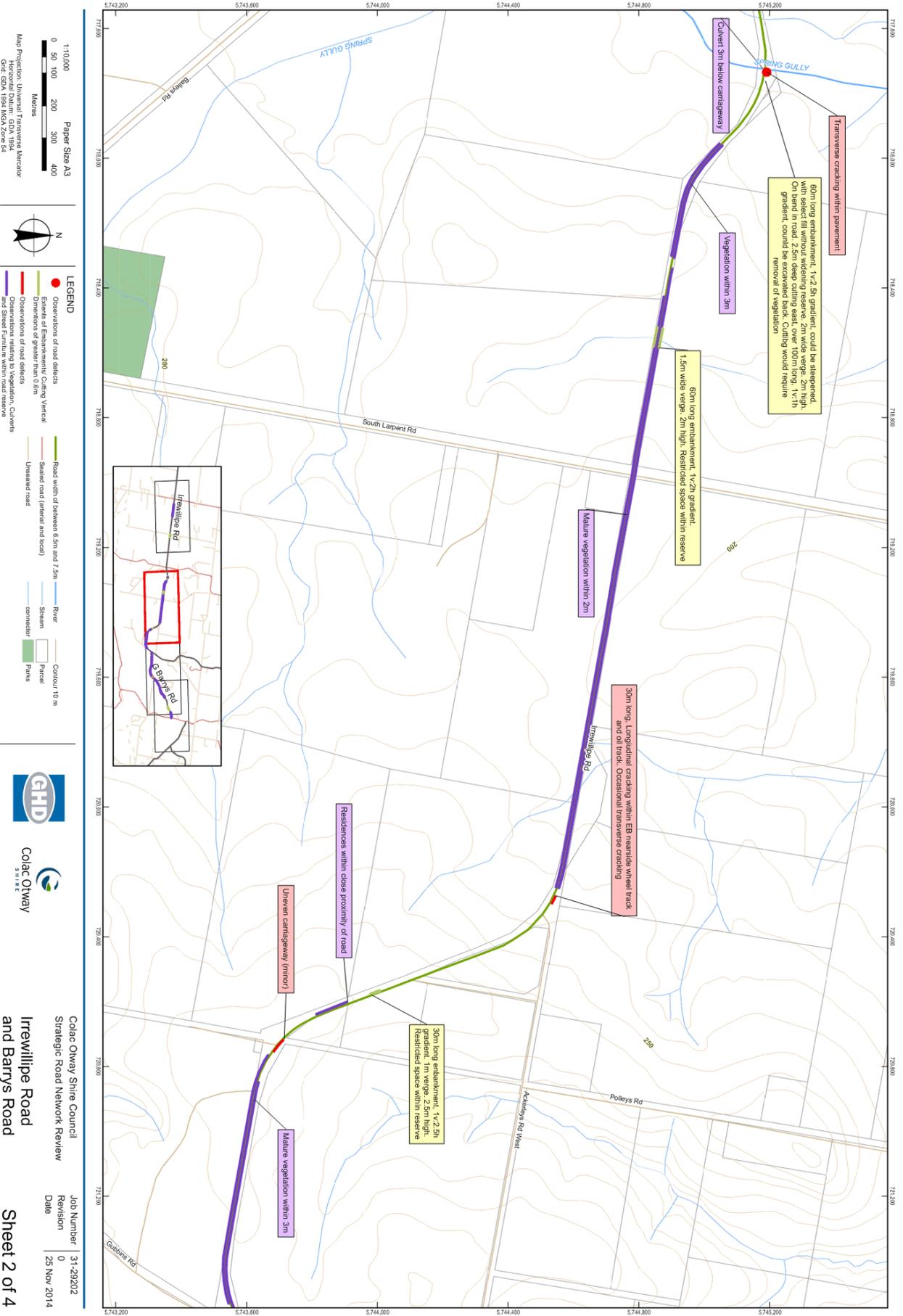
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Revision 0
Date 25 Nov 2014

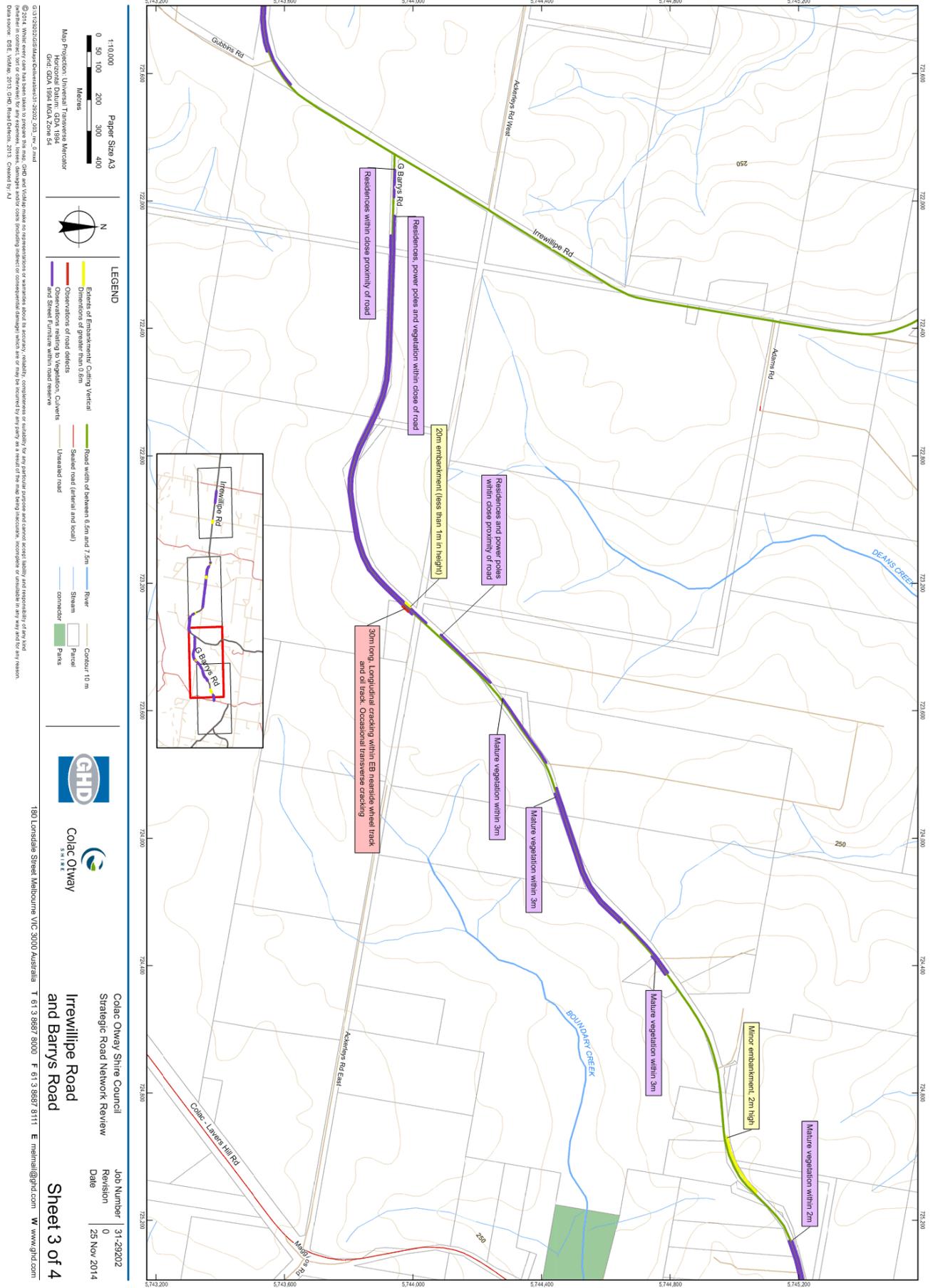
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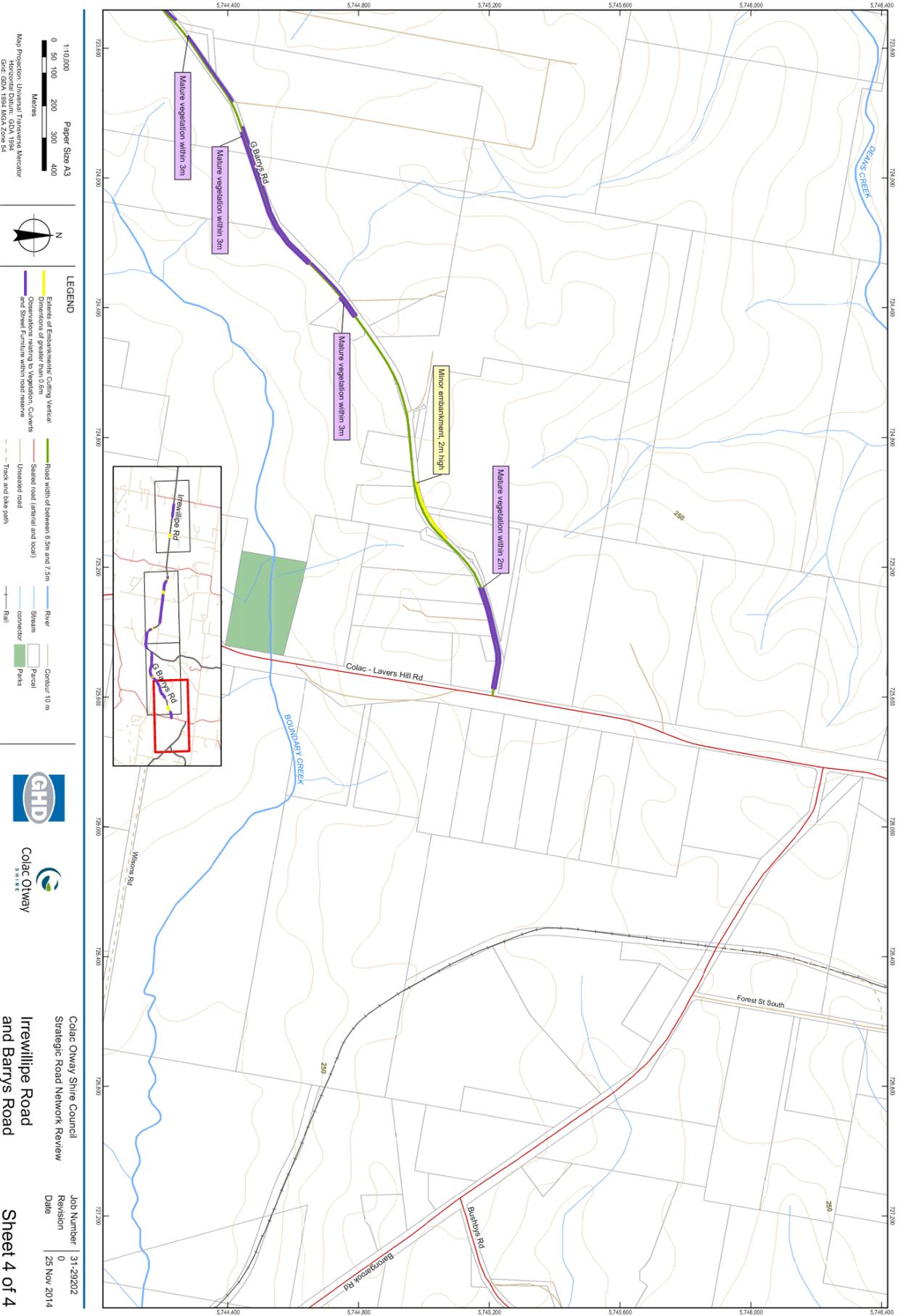
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Appendix E – Irrewillipe Road and Barrys Road Issues Map









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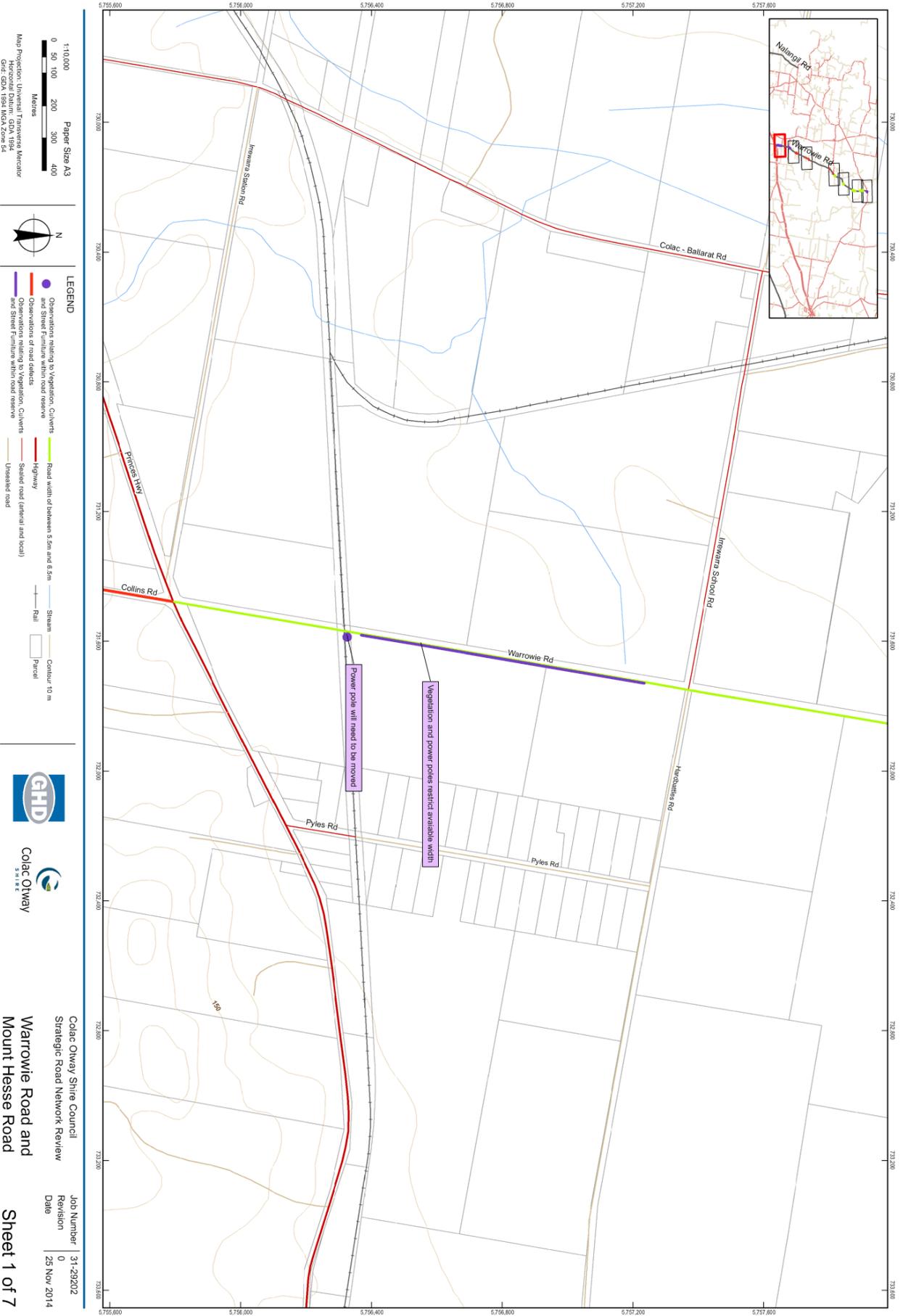


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Irrewillipe Road
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Appendix F – Warrowie Road and Mount Hesse Road Issues Map



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 Map Projection: Universal Transverse Mercator
 Horizontal Datum: GDA 1984
 Grid: GDA 1984 MGA Zone 54

LEGEND

- Observations relating to Vegetation, Culverts and Street Furniture within road reserve
- Observations of road defects
- Observations relating to Vegetation, Culverts and Street Furniture within road reserve
- Road width of between 5.5m and 6.5m
- Highway
- Sealed road (external and local)
- Unsealed road
- Stream
- Contour 10 m
- Rail
- Parcel

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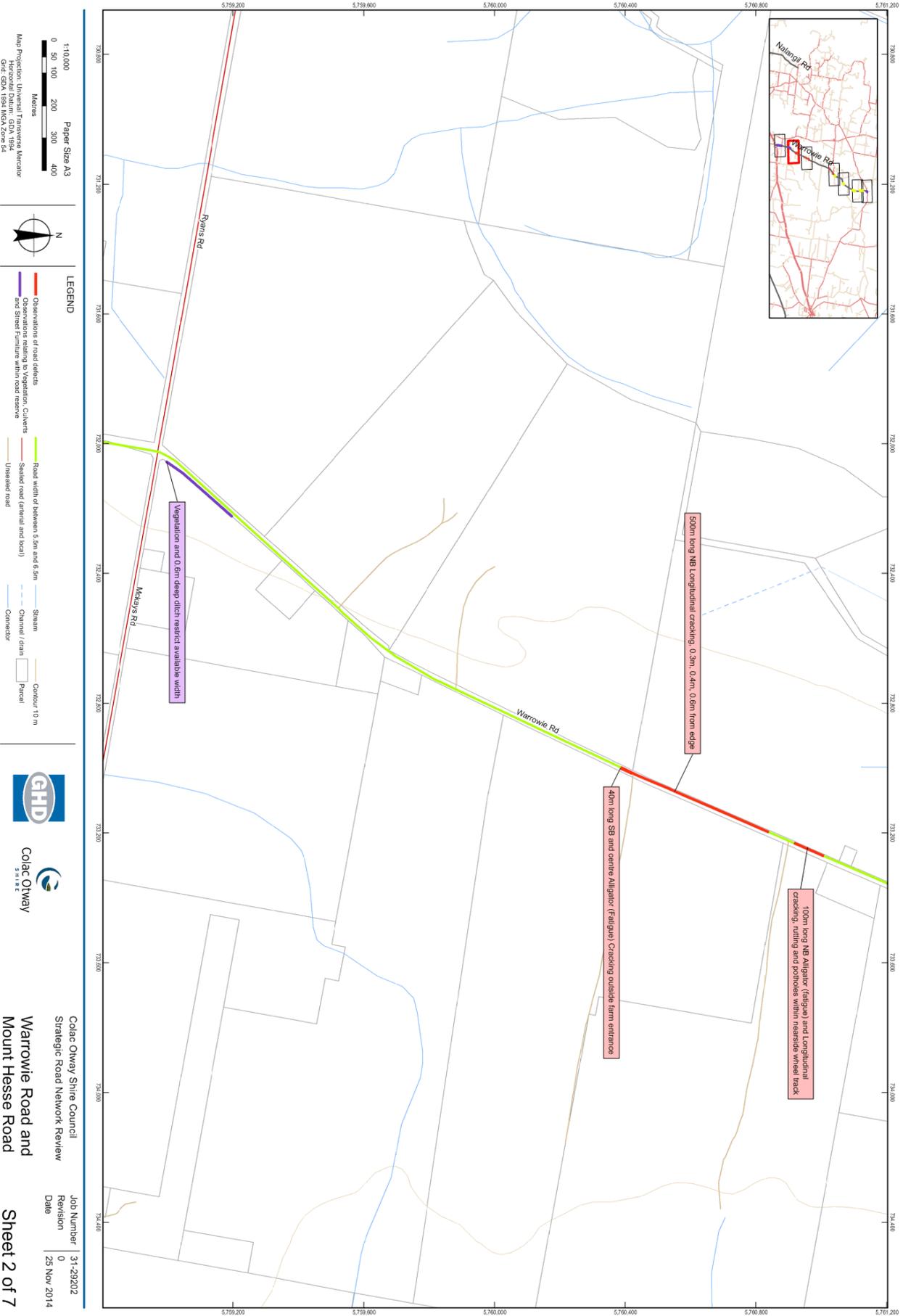


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 Warrowie Road and
 Mount Hesse Road

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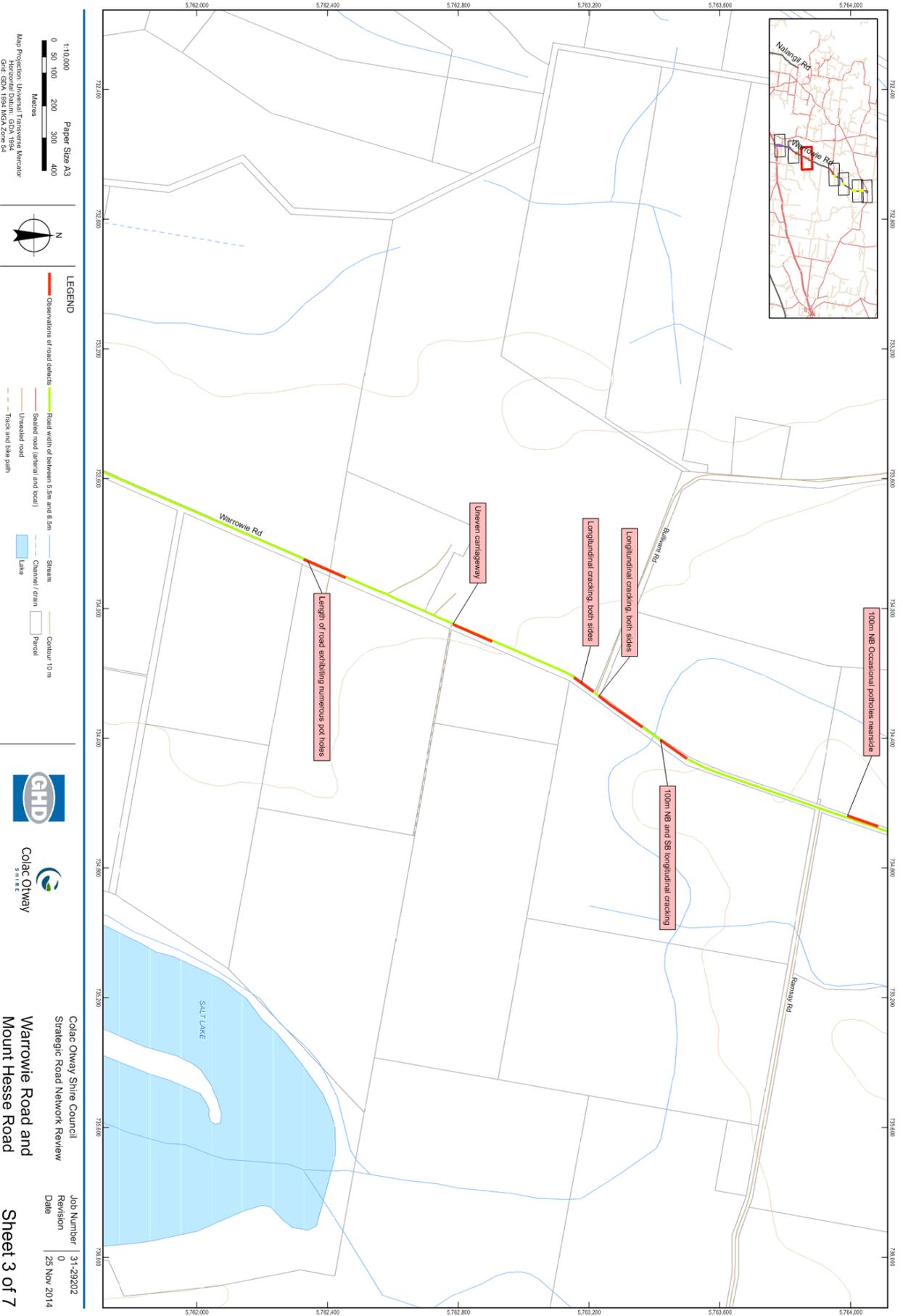
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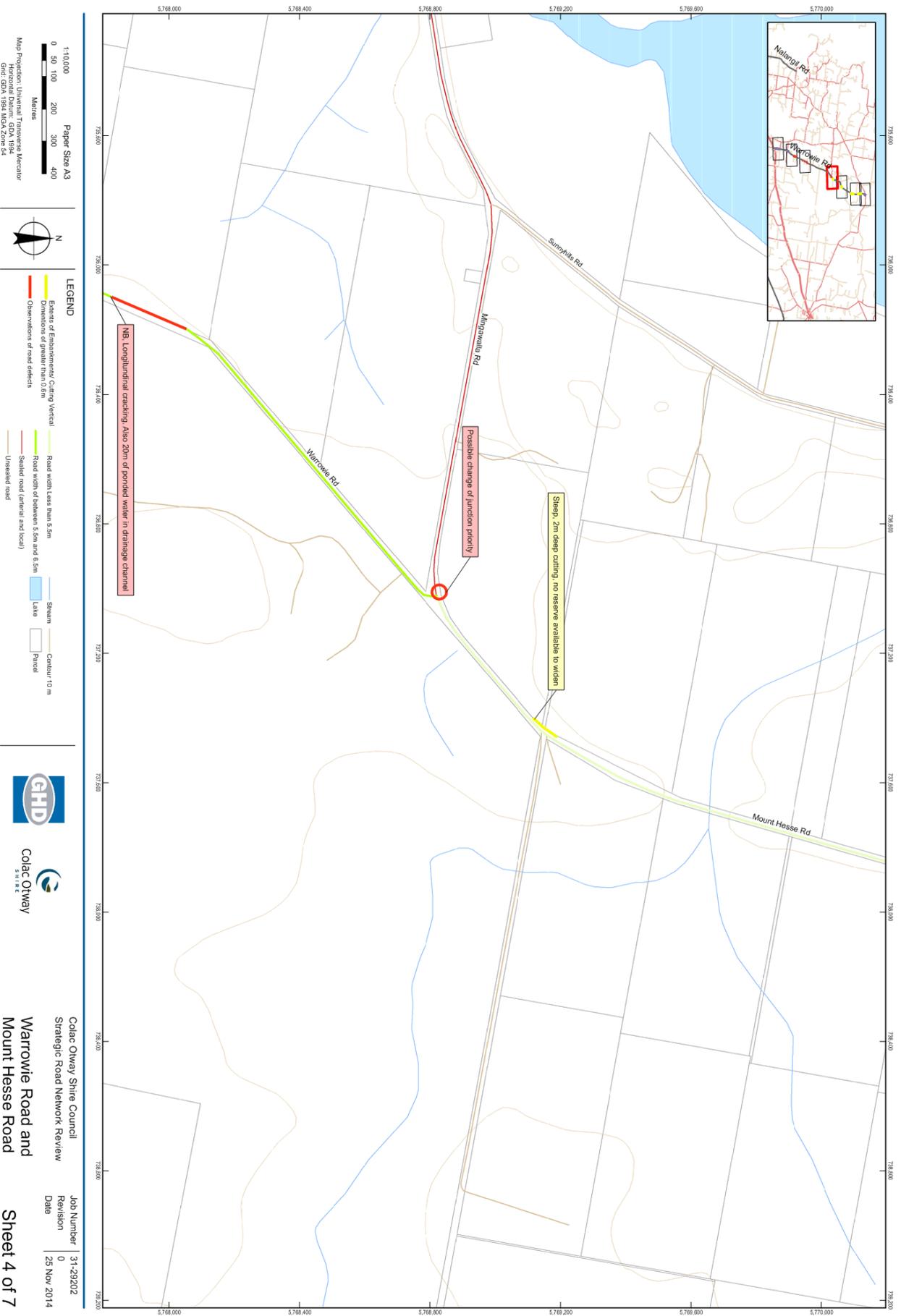


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 Warrowie Road and
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 Map Projection: Universal Transverse Mercator
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 GDA: GDA 1984 MGA Zone 54

LEGEND

- Yellow line: Road width of greater than 6.5m
- Green line: Road width between 5.5m and 6.5m
- Red line: Observations of road defects
- Blue line: Stream
- Blue area: Lake
- Orange line: Centreline
- Grey line: Parcel
- Red line: Sealed road (external and local)
- Black line: Unsealed road

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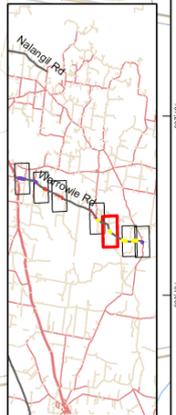
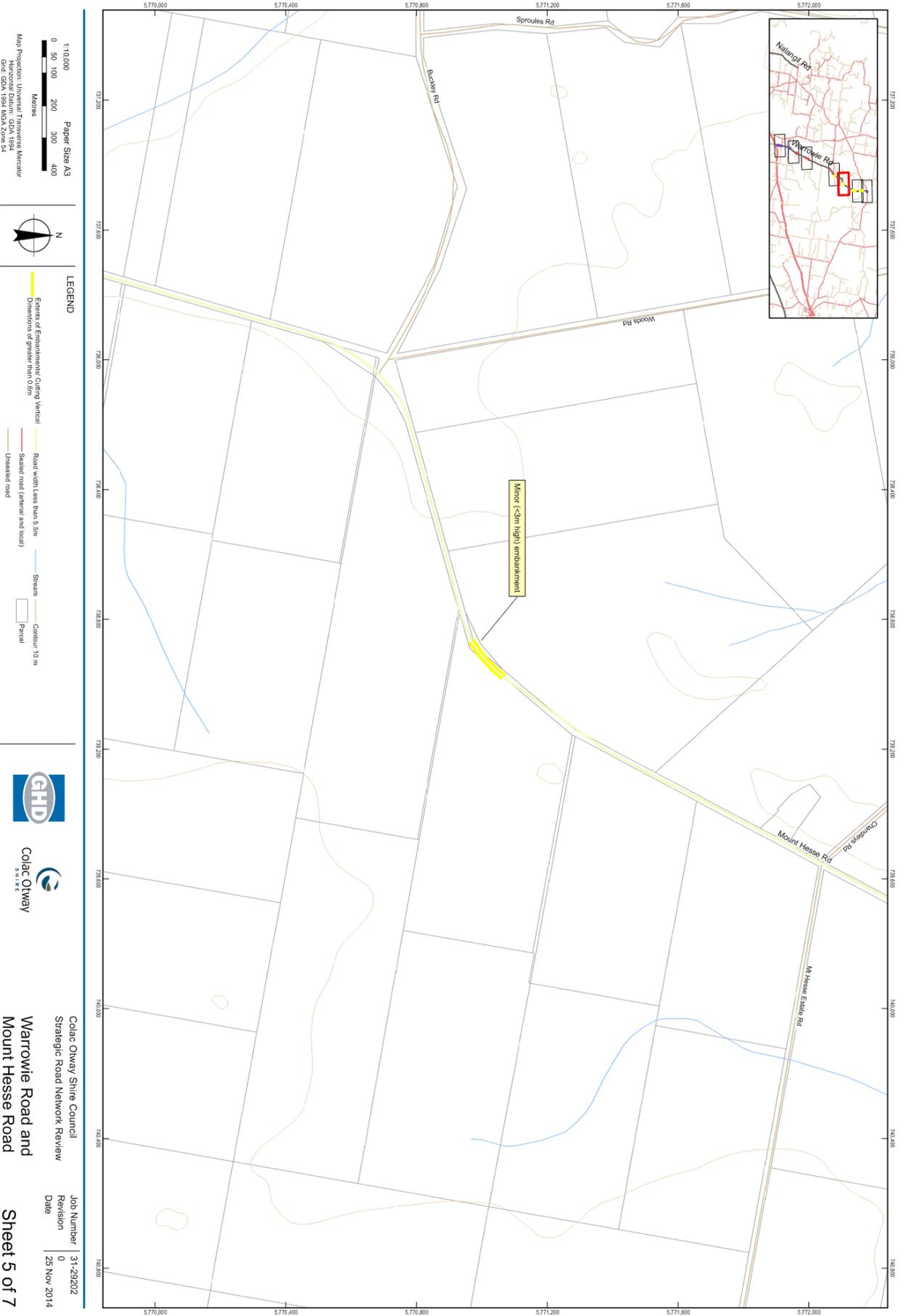


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Map Projection: Universal Transverse Mercator
Horizontal Datum: GDA 1984
Grid: GDA 1984 MGA Zone 54

LEGEND

- Extent of Embankment/Cutting Vertical
- Dimensions of greater than 5.5m
- Road width Less than 5.5m
- Sealed road (external and local)
- Unsealed road
- Stream
- Contour 10 m
- Parcel



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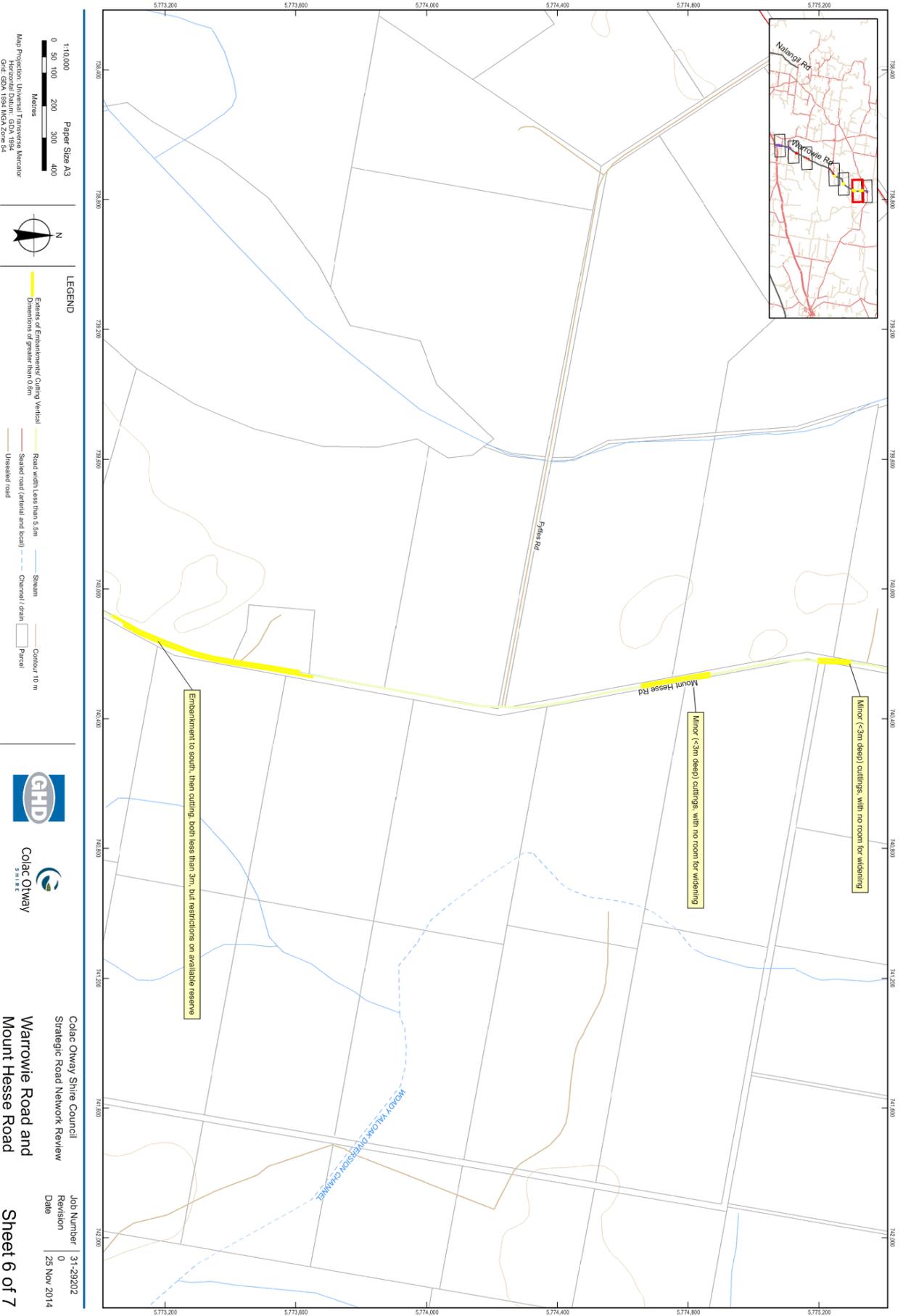
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 Paper Size A3
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 GDA: GDA 1984 MGA Zone 54

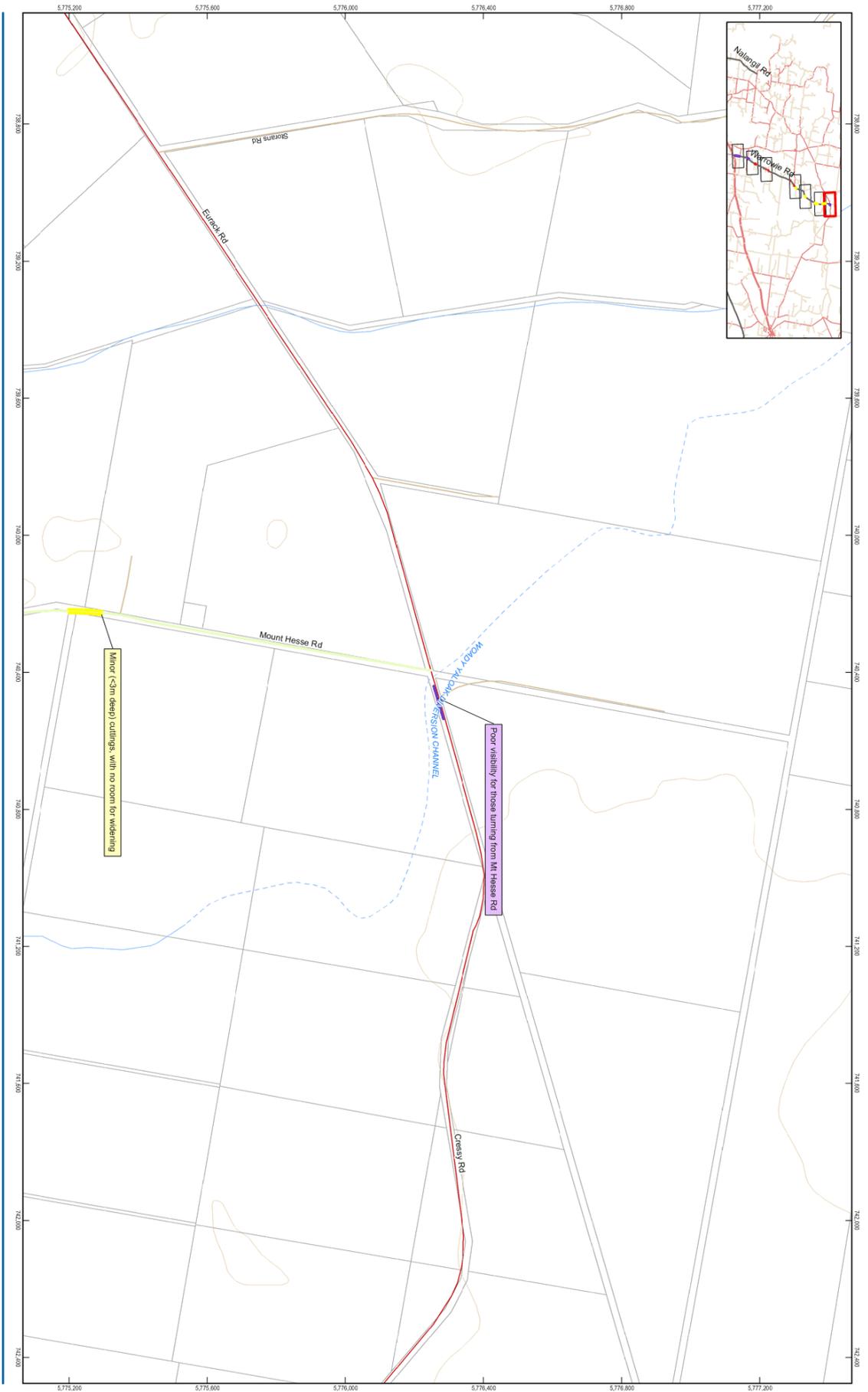
LEGEND
 Embankment/Cutting Vertical
 Dimensions of greater than 5m
 Road width Less than 5m
 Sealed road (detailed and local)
 Unsealed road
 Stream
 Channel / drain
 Contour 10 m
 Parcel

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Metres

Map Projection: Universal Transverse Mercator
Horizontal Datum: GDA 1984
Grid: GDA 1984 MGA Zone 54

N

LEGEND

- Extent of Embankment/Cutting Vertical
- Observations relating to Vegetation, Culverts and Street Furniture within road reserve
- Road width less than 5.5m
- Sealed road (external and local)
- Unsealed road
- Stream
- Channel / drain
- Contour 10 m
- Parcel

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Document Status

| Rev No. | Author | Reviewer | | Approved for Issue | | |
|---------|------------|---------------|---|--------------------|---|------------|
| | | Name | Signature | Name | Signature | Date |
| 0 | Nicole Guy | Tony Frodsham |  | Tony Frodsham | | 08/10/2013 |
| 1 | Nicole Guy | Tony Frodsham |  | Tony Frodsham |  | 16/5/2014 |
| 2 | Nicole Guy | Toby Cooper | | Tony Frodsham | | 20/11/2014 |

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Small Town Improvement Program - GUIDELINES

Objective

The Small Town Improvement Program (STIP) aims to assist in improving infrastructure on public land in the small towns of the Colac Otway Shire. Small towns are defined as any town other than Colac.

Council currently allocates \$100,000 per year towards this Program.

Proposals for projects are sought annually from Small Town Community Groups/ Associations/ Organisations/ community representatives to assist in implementing this program. This program does not allocate funding directly to community groups. The successful projects will be managed by Council.

Groups eligible to apply for consideration of allocation of this fund

Groups eligible to apply for consideration of allocation of this fund are Action Groups, Advancement Associations, Community Groups/ Progress Associations or community groups/ representatives, where no formal Progress Association/Action Group/Advancement Association exists (who can demonstrate a high level of community support) within the small towns of the Colac Otway Shire. These groups are community based, not for profit organisations who wish to assist Council in implementing capital works infrastructure projects. These projects must be identified in a strategic document, eg: Township Master Plan / Infrastructure Plan, a structure plan or neighbourhood character study or equivalent demonstrated high level of community support and be considered a priority to the community.

Criteria of what will be considered for allocation

Capital works projects **must** result from the recommendations of a **Small Town Master Plan / Infrastructure Plan** or similar strategic document, such as a **Structure Plan** or **Neighbourhood Character Study** or **have equivalent community support**. Council recognises that the development of these plans has involved the broader community and has prioritised capital works projects according to community preference. However, in determining allocations of the Small Town Improvement Program the following factors will also be taken into account:

- That the proposed project has broad community support
- The community benefits (short and long term) provided by the project
- Whether the proposed project provides new or increased opportunities for residents of the Colac Otway community
- The amount of funding the town has received in the past from Local, State & Federal Governments
- Whether the proposed project has the ability to attract additional external funding
- The track record of the group in the past in assisting and supporting community infrastructure projects and planning
- The provision of sufficient documentation on the proposal including letters of support and the proposal is included in the relevant strategic document(s)
- Whether the proposed project is able to be completed for the amount requested to be allocated
- The priority rating for funding compared to other suitable applications



Criteria for what will not be considered for allocation -

- Requests for project proposals which do not fit within the recommendations of the relevant Strategic Document/s
- Requests for project proposals for general ongoing administration and project management costs
- Project proposals which are on privately owned land
- Project proposals which have already received funding under the Council's Community Funding Program
- Incomplete applications will not be considered.

Amounts

Once approved, funding allocations will not be paid to the applicant as the project will be allocated in Council's budget. The project will be managed by Council.

Project Proposals up to \$25,000 (Council contribution) will be considered and preference will be given to projects where a contribution is available from the community.

If more than \$25,000 is required from Council to complete the project, a letter should be written to the Chief Executive Officer requesting special consideration.

Should a project not be able to proceed, for example, where an application for external grant funds has been unsuccessful, the funds shall be returned to the budget and the community can apply for STIP allocations in the following year.

How to Apply

Community Groups are welcome to submit proposals for more than one (1) project for consideration, however only one (1) proposal from each township may be funded. Unfunded proposals are eligible to be resubmitted the following year by the Community group. Further information may be requested if the project is funded.

Applicants should note that the submission of an application does not guarantee funding of the proposal.

Project proposals may be submitted on the Application form and emailed to inq@colacotway.vic.gov.au or
Hard copy applications should be sent to:

**STIP Applications
Economic Development Unit
Colac Otway Shire
PO Box 283
COLAC VIC 3250**

Application forms are available on Colac Otway Shire's website www.colacotway.vic.gov.au – from the masthead select 'Forms', Small Towns Improvement Program Application and Guidelines.

Applications close on the last Friday in March annually.

For further information regarding the Small Town Improvement Program or assistance completing this form, please contact Katrina Kehoe, Economic Development Officer on 5232 9444.

1 Apollo Bay Chamber of Commerce & Tourism , Barwon Downs Community Group, Beeac District Progress Association, Beech Forest and District Progress Association, Birregurra Community Group, Carlisle River Community Group, Cressy and District Action Group, Forrest and District Community Group, Kennett River Association Inc., Lavers Hill Progress Association, Skenes Creek Advancement Association Inc., Swan Marsh Community & Neighbourhood Association, Wye River & Separation Creek Progress Association, Red Rock Progress Association

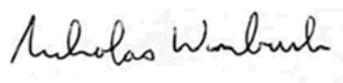
Planning and Environment Act 1987

Panel Report

**Colac Otway Planning Scheme Amendment C73
Apollo Bay Harbour**

20 February 2015

Planning and Environment Act 1987
Panel Report pursuant to Section 25 of the Act
Colac Otway Planning Scheme Amendment C73
Apollo Bay Harbour
20 February 2015



Nick Wimbush, Chair



John Collins, Member

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List of Abbreviations

| | |
|-------|--|
| CAP | Coastal Action Plan |
| CHVA | Coastal Hazard Vulnerability Assessment |
| CMA | <i>Coastal Management Act 1995</i> |
| DELWP | Department of Environment, Land, Water and Planning |
| DEPI | Department of Environment and Primary Industries (now DELWP) |
| DPCD | Department of Planning and Community Development (DPCD) |
| DSE | Department of Sustainability and Environment (former) |
| DTPLI | Department of Transport, Planning and Local Infrastructure (now DELWP) |
| EbD | Enquiry by Design |
| LPPF | Local Planning Policy Framework |
| MSS | Municipal Strategic Statement |
| OCC | Otway Coast Committee |
| PCRZ | Public Conservation and Resource Zone |
| PPRZ | Public Park and Recreation Zone |
| RDV | Regional Development Victoria (former) |
| SPPF | State Planning Policy Framework |
| SUZ | Special Use Zone |
| VCS | Victorian Coastal Strategy |
| VPP | Victoria Planning Provisions |
| WCB | Western Coastal Board |

Executive Summary

Colac Otway Planning Scheme Amendment C73 proposes to apply the Special Use Zone Schedule 2 (SUZ2) to the Apollo Bay Harbour ('the harbour') and surrounds to provide a coherent planning framework for the detailed master planning exercise to come.

Redevelopment of the harbour has been a long term goal to provide improved boating and recreational facilities for residents and visitors alike, whilst protecting the fundamental heritage, landscape and public realm assets that make the harbour so attractive.

In many coastal locations, there are a diversity of views as to what form and to what extent development and revitalisation should occur, and Apollo Bay is no different. Council has clearly struggled over a long period of time to get agreement from stakeholders, including the community, on a clear way forward for harbour development.

In particular, there have been a number of uses proposed which have proved to be controversial; particularly an earlier plan to allow hotel development on the harbour foreshore; and in more recent time consideration of whether a new community swimming pool and associated facilities could form part of a revamped harbour.

A Master Plan was developed which was to be incorporated into the planning scheme as a high level 'blueprint' for both the spatial distribution of key activities, and to provide guidance on key issues such as design and operation of the harbour. Amendment C73 was initially exhibited in late 2013 with the Master Plan to be an Incorporated Document and the harbour rezoned to SUZ2. Following exhibition of the Amendment, and partly in response to submissions, Council revised Amendment C73 significantly.

The revised Amendment C73, rather than including the Master Plan within the planning scheme, includes in text some of the key elements and design principles from the Master Plan, but without any graphical representation.

The revised Amendment proposes that the next level of planning be undertaken through the preparation of a Development Plan, an extensive list of requirements for which is included in the SUZ2. This Development Plan would require public consultation under the proposed SUZ2, but there is no compulsion on Council as the responsible authority to modify the Development Plan in response to submissions, and no third party rights for submitters.

Forty submissions were received on the Amendment, of which 35 were in complete or general support and five were objections. The objections primarily related to keeping non-coastal dependent uses out of the harbour. Such uses were said to include, for example, the swimming pool. There were also submissions on other issues such as car parking.

Many of the submissions to the Amendment raised issues which can only be addressed to any great extent in the next phase, that being the preparation of the Development Plan. The Panel reflects on many of these issues in this report, but many of them are not determinative of the Amendment.

Having reviewed the Amendment and the submissions, the Panel generally supports the Amendment and considers it offers a reasonable planning framework for the future. The Panel does observe, however, that many of the key elements of the harbour's future still appear to be in considerable dispute. By not including the Master Plan as an Incorporated

Document, Council may have made a rod for its own back and simply delayed the difficult consideration of some of the controversial elements and issues.

The Panel considers that if the Amendment is approved, it will be essential to involve design and other professionals to ensure that any plans for the harbour are well thought out and will 'work' in terms of pedestrian and vehicle movement, cohesive coastal design, harbour operations and financial viability.

Based on the reasons set out in this Report, the Panel recommends:

- 1. Adopt Colac Otway Planning Scheme Amendment C73 as exhibited subject to applying the revised Special Use Zone Schedule 2 as shown in Appendix C of this report.**

1 Overview of Amendment and Panel process

1.1 The Amendment

Colac Otway Planning Scheme Amendment C73 (the Amendment) was prepared by the Colac Otway Shire Council as Planning Authority. The Amendment proposes to put in place a planning framework for the future development of the Apollo Bay Harbour and immediate surrounds.

The Amendment proposes to make the following changes to the Colac Otway Planning Scheme:

- Amend Clause 21.03-3 (Apollo Bay and Marengo) to refer to the preparation of the Development Plan for the Apollo Bay Harbour
- Amend Clause 21.05-4 (Tourism) to refer to the preparation of a Development Plan for the Apollo Bay Harbour and its potential to increase tourism stays along the coast
- Amend Clause 21.06 (General implementation) by referring to the use of the SUZ2 for the Apollo Bay Harbour
- Rezone the Apollo Bay Harbour and its surrounds from part Public Park and Recreation Zone (PPRZ) and Public Conservation and Resource Zone (PCRZ) to Special Use Zone – Schedule 2 (SUZ2).

The changes to clause 21 were not contentious and the Panel does not deal with them further in this report.

1.2 Exhibition and submissions

1.2.1 First exhibition

The Amendment was originally exhibited between 19 September and 30 October 2013 and 27 submissions were received. In response to submissions, at its meeting on 26 February 2014, Council resolved to make changes to the *Apollo Bay Harbour Master Plan 2013* (the Master Plan) to provide for the potential consideration of a public indoor swimming pool/sea baths and other health and wellbeing uses at the harbour. At this meeting, Council also resolved to refer the submissions to a Panel.

As a result, a Panel to consider the Amendment was appointed under delegation from the Minister for Planning under the *Planning and Environment Act 1987* and comprised Ms Kathy Mitchell (Chair) and Mr John Collins. However, due to member availability the Panel was re-constituted on 8 April 2014 with Mr Nick Wimbush (Chair) and Mr Collins.

At its meeting on 23 April 2014, Council resolved to make changes to the Amendment arising from the February 2014 resolution that would require the re-exhibition of the Amendment. These changes included removing reference to the Master Plan as an Incorporated Document and requiring a Development Plan to be completed prior to any buildings and works.

The Directions Hearing was held on 26 May 2014 in Apollo Bay conducted by Mr Wimbush. At the Directions Hearing Council advised that significant changes to the Amendment are proposed as agreed by Council at its meeting of 23 April 2014. The essence of these changes

was to remove the Master Plan as an Incorporated Document, but to include some of the development principles in the Master Plan in a revised and expanded SUZ Schedule to feed into a Development Plan.

1.2.2 Re-exhibition

Re-exhibition took place between 30 July and 10 September 2014. Submissions were taken and considered by Council as per a normal Amendment. In response to re-exhibition of the Amendment, Council received 40 submissions, including five objecting submissions. On 22 October 2014 Council resolved to refer submissions to a Panel. Submissions arising from the re-exhibition focused on the following issues:

- *Only Harbour-dependent uses should be located within the Harbour precinct. Non-dependent uses (such as the pool) will use car parking for uses that are Harbour-dependent*
- *The health and wellbeing centre would not be viable without boutique accommodation*
- *The indoor pool should be located on the current pool site (i.e. at the school)*
- *The redevelopment of the boat ramp car park may result in less parking spaces available.*

The 35 supporting submissions were largely in support of the Amendment because it:

- *Maintains the prohibition on the hotel/accommodation*
- *Allows Council to consider the possibility of an indoor pool and health and wellbeing facility at some time in the future (it would no longer be prohibited).*

Council notes that the vast majority of submitters had also made a submission to the original exhibition of Amendment C73 in 2013, prior to Council's decision to change the Amendment into its current form.

A second Directions Hearing was held on the 17 November 2014 in Apollo Bay conducted by Mr Wimbush. The arrangements for the main Hearing were finalised and Directions made accordingly.

1.3 Hearing and inspections

The Panel sat at the Apollo Bay Hotel on 8 and 9 December 2014 to hear submissions in respect of the Amendment. An accompanied site inspection was undertaken on the morning of the 8 December 2014 (discussed in detail in Section 4.1.1). In addition, the Panel members undertook a number of inspections of different elements of the harbour and surrounds together and separately.

Those heard are listed in Table 1. The full list of submitters is shown in Appendix A.

In reaching its conclusions and recommendations, the Panel has read and considered the submissions and a range of other material referred to it including verbal presentations. The following chapters of this report discuss the issues raised in submission relating to the Amendment in further detail.

Table 1 Parties to the Panel Hearing

| Submitter | Represented by |
|--|--|
| Colac Otway Shire Council | Mr David Merrett of ISIS Planning, assisted by Mr Don Lewis of Council |
| Apollo Bay Sailing Club Inc. | Mr Tim Cobb who called evidence in yachting and boating from Mr Steve Walker |
| Mr Tony Webber | |
| Mr Peter Fillmore | Mr Tony Webber |
| Department of Environment and Primary Industries | Mr Geoff Brooks |
| Ms Ngaira Smith | |
| Mr Andrew Buchanan | Assisted by Mr Chris Dance |
| Apollo Bay and District Health Foundation | Ms Kerry McMahon and Mr Stuart Holbery |
| Apollo Bay Aquatic Centre Committee | Ms Jane Gross and Mr Damian Gorman |
| Apollo Bay Chamber of Commerce and Tourism Inc. | Mr John Riches and Mr Wayne Kayler-Thomson |
| Mr Philip Lawson | |

1.4 Procedural issue

Council provided notice of the revised Amendment to all submitters from the first round as well as normal notice. Submitters from the first round were still required to provide a new or revised submission.

This caused a degree of confusion, with some submitters from the first round assuming they were a party to the Hearing even if they had not provided a new or revised submission. The Panel wrote to all submitters from the first and second rounds advising of the Hearing.¹

To avoid further confusion, the Panel has also proceeded on the basis of considering submissions and issues from all submissions. This approach was supported in the Hearing by Council.

The Panel is satisfied that the hearing requirements of the *Planning and Environment Act 1987*, to provide a reasonable opportunity for submitters to be heard, has been complied with.²

1.5 Issues dealt with in this report

This report deals with the issues under the following headings:

- Planning context
- Specific issues
 - Visual analysis and impact
 - Built form, height and design excellence

¹ Many submitters submitted to both rounds of exhibition and this is shown in Appendix A.

² Section 24.

- Car parking
- Linkages to the town centre
- Cultural heritage
- Aquatic/wellness centre in the harbour precinct
- Schedule 2 to the proposed Special Use Zone.

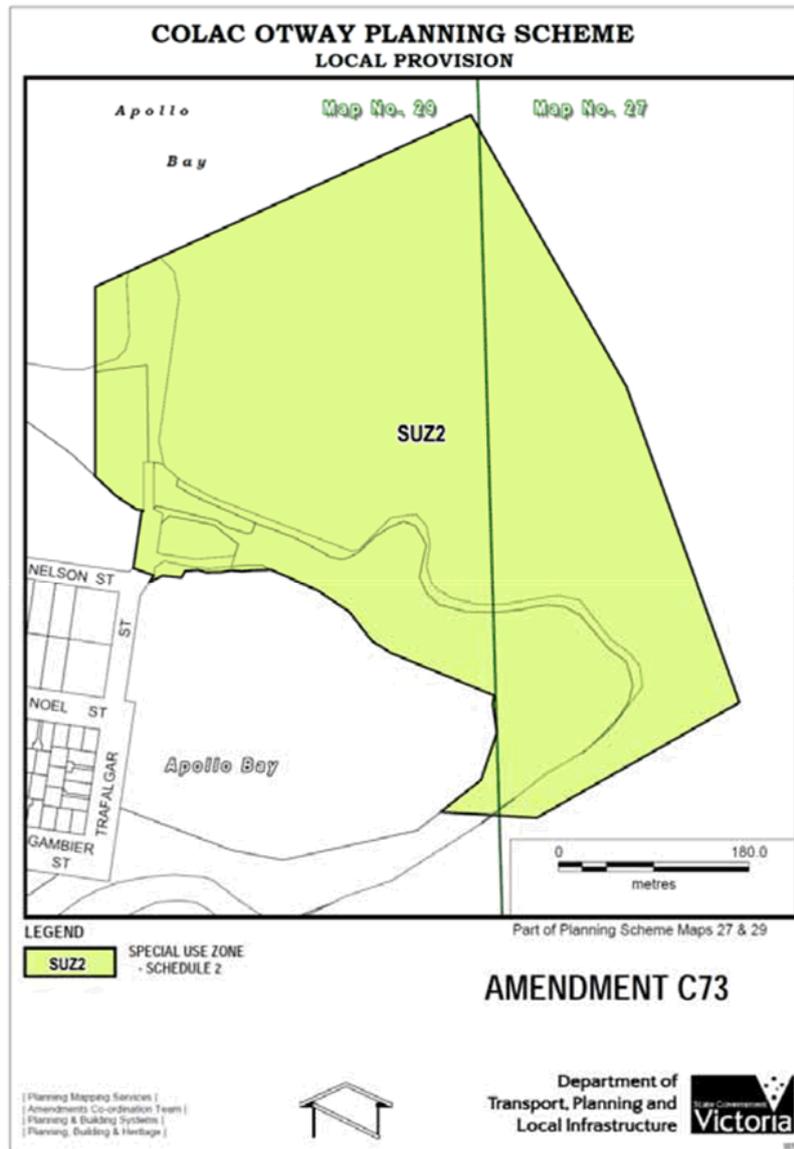
A number of submissions raised the issue of climate change and coastal hazard vulnerability. In the Panel's view this is an issue that will be dealt with at the time of development, through for example, finished floor levels and construction standards. The Panel notes and supports the requirement for a Coastal Hazard Vulnerability Assessment (CHVA) to be prepared as part of the Development Plan in Clause 4.0 of the SUZ Schedule. The Panel does not address this further as a separate issue.

2 Background

2.1 The subject site and surrounds

The Amendment applies to the Apollo Bay Harbour (the harbour) and its surrounds as shown in Figure 1. Council manages the harbour, including the Port of Apollo Bay, through an agreement with the State Government.

Figure 1 Proposed Special Use 2 Zone



Apollo Bay is a coastal town located approximately 150 kilometres south west of Melbourne, on the Great Ocean Road. The harbour at Apollo Bay, which is located south of the main commercial centre, was established in 1949 and is situated at Point Bunbury. The harbour has approximately 56 commercial fishing fleet vessels that are reliant on it (including boats that are not moored in the harbour but use the boat ramps).³ An aerial photograph is provided in Figure 2.

Figure 2 The Apollo Bay Harbour



2.2 Apollo Bay Harbour Master Plan 2013

The *Apollo Bay Harbour Master Plan 2013* process commenced in 2007 and was initiated by a Harbour Capacity Study in 2006. This study identified significant opportunities to expand the harbour, provide better facilities and improve the tourism focus of the harbour. The Master Plan has been through a number of iterations, including an Enquiry by Design process in 2008.

Key features of the Master Plan included:

- *The existing Fisherman's Co-Op building to be retained with modified access to separate vehicles from pedestrians and a new sales and interpretive centre including seafood restaurant*

³ Master Plan Report, p5.

- *Toilets, interpretive displays on maritime history of Apollo Bay to be located adjacent to the current building allowing the Mother's Beach car park to be formalised to enable more parking spaces and to be made safer*
- *A new small recreational marina to be developed on the eastern breakwater as demand warrants*
- *The current boat ramp to be retained and widened to increase capacity*
- *Port operations base to be rebuilt adjacent to the current site including enhanced facilities for Harbour Management, boat repairs, an upgraded slip and sliding berth for emergency access for Marine rescue*
- *New sailing club facilities for club activities, race management, storage, repairs, toilets and showers*
- *New space for water based commercial activity within the Harbour, possibly a floating restaurant*
- *A wide boardwalk providing pedestrian connections through the precinct*
- *A new small extension to the western pier to be constructed to modify the wave surge impact on boat moorings and a proposed extension to the eastern breakwater to be investigated*
- *A new Great Ocean Road Aboriginal Interpretive Centre*
- *Public toilets and change rooms to be provided at Mothers Beach car park, the Harbour Beach and within the harbour central precinct.*⁴

The Master Plan states that the golf course and access roads will not be significantly affected.

The Master Plan originally included consideration of a hotel/wellness centre, but in February 2012, in response to community concern regarding this component, Council resolved to delete it.

Amendment C73 was to initially include the Master Plan as an Incorporated Document. The revised Amendment C73 before the Panel includes some of the key directions and principles from the Master Plan into the SUZ Schedule 2, but does not propose to include the Plan itself in the planning scheme.

2.3 Amendment background

Chapter 3 of Council's submission to the Hearing outlines the planning for the harbour and history of the Amendment. Some of the key milestones are outlined below.

Council and the former Department of Sustainability and Environment (DSE) commenced preparation of a Harbour Capacity Study in 2005 which planned for the harbour redevelopment. The Harbour Capacity Study recognised the following:

*...the port facilities were old and dilapidated, that the fishing industry had declined, and that the future of the precinct would be in redevelopment to attract tourists.*⁵

⁴ Apollo Bay Master Plan Report pp 3-4.

⁵ Council submission p5.

Council has listed the redevelopment of the harbour as a Council priority in the 2005-2009, 2009-2013 and 2013-2017 Council Plans.

In 2006, the harbour master planning process commenced and a draft was developed by Meinhardt in 2007. The draft Master Plan was not supported by the local community as they expressed issues with the scale of works proposed.⁶

Consequently, RDV funded an Enquiry by Design (EbD) process to work with the Apollo Bay community to develop a Master Plan that was more in line with community expectations. The five day consultation and design process resulted in the Apollo Bay Harbour Precinct Master Plan, which was initially adopted by Council in 2008.⁷

Council commissioned Meinhardt in 2012 to finalise the Master Plan.

Under the current zone (Public Park and Recreation Zone (PPRZ)) it would be possible to implement some of the Master Plan projects as the public land manager without the need for a planning permit. However, Council considers that, in using the Amendment approach, the process will be more transparent and include formal community input.

Subject to approval of the Amendment and receipt of external funding, Council will prepare a Development Plan which will provide the detailed framework for harbour development.

⁶ Council submission p7.

⁷ Council submission p8.

3 Planning Context

Council provided a response to the Strategic Assessment Guidelines as part of the Explanatory Report.

The Panel has reviewed the policy context of the Amendment and made a brief appraisal of the relevant zone and overlay controls and other relevant planning strategies.

3.1 Planning policy framework

3.1.1 State Planning Policy Framework

Council submitted that the Amendment is supported by the following clauses in the State Planning Policy Framework (SPPF):

Clause 11.03-2 Open space management – this clause seeks to *provide for the long term management of public open space by ensuring public access is not prevented by developments along stream banks and foreshores and public land immediately adjoining waterways and coastlines remains in public ownership*. Council submitted that improved facilities at the harbour will increase public access, and linkages between the town centre and the harbour will be improved.

Clause 11.05-5 Coastal settlement seeks to plan for sustainable coastal settlement on land within a settlement boundary. Council stated that *...the Harbour is within the settlement boundary of Apollo Bay*.⁸

Clause 12.02 Coastal Crown land – this clause seeks to *achieve development that provides an environmental, social and economic balance*. Council stated that the Amendment proposes new coastal dependent development in an area that has a modified landscape and improves public access to the harbour. They consider that it also protects Aboriginal cultural heritage.

Clause 13.01-1 Coastal inundation and erosion – this clause seeks to *plan for and manage the potential coastal impacts of climate change*. Council noted that the Amendment includes a Coastal Hazard Vulnerability Assessment (CHVA) that states that development will need to address sea level rise. However, it is considered that there is *...a low risk of coastal inundation to 2100 and a low-medium risk for coastal erosion and recession to 2100*.⁹

Clause 15.03 Heritage – this clause seeks to ensure the conservation of places of heritage significance, both Aboriginal and non-Aboriginal. Council considers that *...the Amendment protects and enhances the heritage and Aboriginal heritage significance of the harbour*.¹⁰

Clause 17.03 Tourism – the Amendment seeks to *encourage tourism development to maximise the employment and long-term economic, social and cultural benefits of developing the State as a competitive domestic and international tourist destination*.¹¹ Council stated that the Amendment seeks to increase the tourism appeal of the harbour through the following: small scale commercial developments, the retention of the port

⁸ Council submission p18.

⁹ Council submission p19.

¹⁰ Council submission p12.

¹¹ Council submission p19.

operations, improvements to the public realm with the aim of extending tourist stays in the area. In addition, the G21 Regional Growth Plan identifies the redevelopment of Apollo Bay Harbour as one of its major projects.¹²

3.1.2 Local Planning Policy Framework

The Municipal Strategic Statement (MSS) identifies Apollo Bay as the second largest urban centre in the municipality. The MSS states that the Apollo Bay Harbour needs to be developed *...with a tourism, fishing, boating, commercial and recreational focus strengthening links to the town centre of Apollo Bay and providing net community benefits.*¹³

The Apollo Bay, Marengo and Skenes Creek Framework Plan also identifies the potential for the redevelopment of the Apollo Bay Harbour.

Council notes that the framework plan is to be amended under the exhibited Amendment C74, however the reference to the harbour remains the same.

Council submitted that the Amendment is supported by the following specific clauses.

Clause 21.03 Settlement – This clause states that Apollo Bay is the major coastal urban centre in the Shire and is experiencing high rates of development. It provides a range of entertainment and recreational related facilities, including swimming, fishing, golf, cafes and restaurants. Strategies include to *promote a pattern of settlements in the coastal strip that balances between opportunity for growth and retention of environmental and cultural qualities.*¹⁴

Clause 21.03-3 Apollo Bay and Marengo has the following objectives:

- *To develop Apollo Bay as an attractive residential community which provides a high quality environment as a significant tourist centre*
- *To retain Apollo Bay, Marengo and Skenes Creek as distinct coastal settlements with their own local character*
- *To ensure that the natural beauty of the area is reflected in new development*
- *To preserve the seaside character village of Apollo Bay*
- *To ensure that change demonstrates a commitment to sustainability and is responsive to the natural environment.*¹⁵

Strategies include to *Ensure that the development of Apollo Bay and Marengo occurs generally in accordance with the strategic directions outlined in the Apollo Bay Framework Plan attached to this clause.*

Under *Activities: Business, Tourism, Community and Recreation*, it states:

- *Develop the Apollo Bay Harbour Precinct with a tourism, fishing, boating, commercial and recreational focus strengthening links to the town centre of Apollo Bay and providing net community benefits*

¹² Council submission p19.

¹³ Council submission p19.

¹⁴ Explanatory Report.

¹⁵ Explanatory Report.

- *Encourage future recreation facilities to be located together with other community facilities in a central and accessible location*
- *Ensure that community, health, education and recreation facilities are provided to meet the needs of current and projected future residents and visitors to the area*
- *Support the growth of tourism as a major employer for the region.*

Under *Landscape setting and environment* it states:

- *Reinforce and enhance the identity and the sense of arrival and departure at the entrances to Apollo Bay and Marengo*
- *Improve the appearance and amenity of foreshore reserve in Apollo Bay and reduce the impact of the existing and future structures on the naturalness of the setting*
- *Achieve improved visual and physical links between the Apollo Bay town centre and the beach*
- *Protect and enhance the significant views and vistas available from the settlements, the beach and the harbour, as well as views available from key vantage points in the hills.*

Under *Access* it states:

Create a highly walkable town centre in Apollo Bay with safe and convenient access to shops, community facilities and recreational activities.

Clause 21.05-4 Tourism states:

The increase in tourism has the potential to create conflicts through environmental damage, pressure on attractive and sensitive areas for development to meet accommodation demands and dangers to residents and visitors in areas sensitive to landscape and bushfires.

Protecting those resources on which tourism depends is vitally important. They include the natural environmental and cultural features, heritage, infrastructure, settlements and townscape design and presentation.

With tourism as a major growth sector it will be necessary to recognise more closely the ways in which other industries and businesses can add to the tourist appreciation of the region both through improvements to the quality of the visual landscape and in the introduction of new experiences.

Objectives include:

- *To encourage growth in tourism in a way that assists diversification in the economy and ensures the protection of key environmental features*
- *To encourage investment in tourism that has close linkages with local industries and the environment*
- *To protect visual and environmental features which are of major significance and contribute to the tourism assets of the Shire.*

3.1.3 Planning scheme provisions

(i) Zones

The Amendment proposes to apply the Special Use Zone Schedule 2 (SUZ2) to the harbour and surrounds. In its submission, Council stated that the SUZ2 will allow Council to prepare specific use and development controls for the harbour. The SUZ2 proposes to:

- *Require a Development Plan to address the complex nature of the precinct, particularly the area around the Port operations, public access, commercial development and parking*
- *Not require a planning permit for any use of the land that is consistent with an approved Development Plan, allow Council to consider some uses with a permit (e.g. indoor swimming pool) and prohibit other uses (e.g. accommodation)*
- *Allow for some development to occur prior to the approval of a Development Plan and require a permit for buildings and works for all but:*
 - *Car/boat parking redevelopments*
 - *Minor public works such as pathways, bbqs, shelters, picnic tables and the like (and toilets)*
 - *Dredging works to maintain navigable depths*
 - *Repair or removal of navigation aids.¹⁶*

The purposes of the SUZ2 as exhibited are extensive and are:

- *To provide for the co-ordinated redevelopment of the Apollo Bay Harbour that is consistent with an adopted Development Plan*
- *To upgrade the boating facilities to a regional level facility in accordance with the Boating Coastal Action Plan 2007*
- *To accommodate Port Operations in new and improved facilities*
- *To contribute to the development and growth of regional tourism*
- *To ensure that the combination of uses, their overall density and the scale, character and level of development are compatible with:*
 - *The amenity of the surrounding area*
 - *The skyline as seen from the northern and eastern breakwater*
 - *The capacity of the existing road system and any proposed modifications to accommodate an increase in traffic*
 - *The need to provide for ongoing commercial port operations and commercial tourism uses*
- *To encourage small scale commercial development that does not adversely impact retailing in the Apollo Bay shopping centre*
- *To provide safe and efficient vehicle, pedestrian and cycling movements and linkages between the Harbour and the shopping centre*
- *To encourage a high standard of coastal urban design*

¹⁶ Council submission p4.

- To ensure development is protected from climate change impacts such as sea level rise and storm surge
- To ensure any redevelopment is consistent with the heritage values of the precinct
- To provide for the ongoing operation of the Apollo Bay golf course.

(ii) Overlays

The Heritage Overlay (HO300) already applies to the harbour and surrounds. This is not proposed to be modified by the Amendment. The HO is further discussed in Section 4.5.

3.1.4 Ministerial Directions and Practice Notes

(i) Ministerial directions

The Panel is satisfied that the Amendment is consistent with the *Ministerial Direction on the Form and Content of Planning Schemes* under section 7(5) of the *Planning and Environment Act 1987* subject to some relatively minor drafting issues discussed in Chapter 5.

The Panel is satisfied that the Amendment is consistent with *Ministerial Direction 11 – Strategic Assessment of Amendments* under section 12(2) of the *Planning and Environment Act 1987*.

The Panel is satisfied that the Amendment is consistent with *Ministerial Direction No 13 – Managing Coastal Hazards and the Coastal Impacts of Climate Change*.

(ii) Planning practice notes

The following practice notes are relevant to the Amendment:

- Planning Practice Note 2: *Public Land Zones*, October 2013 (PPN2)
- VPP Practice Note: *Applying the Special Use Zone*, February 1999

The harbour and surrounds are Crown land, and thus publicly owned. PPN2 makes it clear that the public land zones, the Public Conservation and Resource Zone (PCRZ) and the Public Park and Recreation Zone (PPRZ), are not used to identify land use or legal status of the land. PPN2 notes that they are intended to set out statutory requirements for use and development in the context of public land legislation. PPN2 notes that the '*...principles are similar to those that apply to the Special Use Zone*'.

The PPRZ, which currently applies to the harbour, is used in many similar contexts including, for example, the St Kilda Harbour, Blairgowrie Yacht Squadron, the Lorne Foreshore and many other coastal locations. The PPRZ does not exclude public or commercial development. It also provides for an Incorporated Document and a schedule to the zone to guide development.

To use this zone and approach would have required a detailed plan for the harbour to have been developed and exhibited with the Amendment, perhaps a similar plan to the Master Plan exhibited with the earlier version of Amendment C73.

The practice note for the SUZ, admittedly now dated, suggests the following:

A Special Use Zone can be considered when either:

- *Appropriate combination of the other available zones, overlays and local policies cannot give effect to the desired objectives or requirements.*
- *The site adjoins more than one zone and the strategic intent of the site, if it was to be redeveloped, is not known and it is therefore not possible to determine which zone is appropriate.*

The Panel notes the SUZ has been used in other coastal Crown land locations, such as for the St Kilda Sea Baths (SUZ1 to the Port Phillip Planning Scheme). It has also been used on other coastal locations, the BAE site in Williamstown being a prime example where SUZ5 to the Hobsons Bay Planning Scheme contains requirements for a 'Physical Framework Plan' and 'Development Plan'.

3.2 Coastal planning and management

Relevant legislation, strategies and policies were brought to the Panel's attention.

3.2.1 Coastal Management Act 1995

The *Coastal Management Act 1995* (CMA) provides the planning and management framework for Victorian coastal areas including the Apollo Bay Harbour.

(i) Victorian Coastal Strategy 2014

The *Victorian Coastal Strategy* (VCS) 2014 was prepared under the CMA and adopted by the Victorian Government on 1 September 2014 as the principal policy statement for coastal planning and management in Victoria.

The VCS 2014 articulates the following principles which are relevant to the Amendment:

- *Principle 2.2 – Coastal settlements and communities – addresses the concepts of activity and recreation nodes as foci within settlements for tourism and recreation activity.*
- *Principle 2.3 – Port Precincts – includes the Desired Outcome that Victoria's local and commercial ports are planned, developed and managed taking into account the character, amenity and sustainability of the coast and their regions.*
- *Principle 3.1 – Coastal buildings, infrastructure and management – contains a Desired Outcome that: Coastal Crown land is not used for structures unless they provide significant community benefit, and their functionality depends on them being near the water.*

The VCS is also called up in clause 12 of the SPPF as a reference document.

In his submission Mr Brooks for the Department of Environment and Primary Industries (DEPI) submitted:

On balance DEPI considers the amendment provides an active and positive response to, and is consistent with, the VCS 2014.

(ii) Western Coastal Action Plan Review 2012

The Western Coastal Board (WCB), which is appointed under the CMA to advise on coastal planning matters, in 2012 undertook a reviews of Coastal Action Plans (CAPs) in the region.

This Review found that CAPs have been very successful in delivering Integrated Coastal Zone Management on the west coast of Victoria. The Apollo Bay Harbour plays a critical role on the Apollo Bay foreshore and in the region.

The Panel notes a Draft *Western Region Coastal Plan* was placed on exhibition for comment after the Panel Hearing for Amendment C73.

(iii) Boating Coastal Action Plan Review 2013

A Boating CAP was prepared by the WCB and released in 2010. In 2013 a review of the CAP was undertaken.

The Boating CAP provides a strategic framework for the provision of recreation boating facilities in Western Victoria. The CAP identifies the harbour as a Regional Boating Facility with a high strategic priority.

The CAP notes:¹⁷

Actions related to the Apollo Bay Harbour have been classified as In Progress, but with 40% completeness. A recent funding application for the upgrade of the boat ramp car park was unsuccessful. It is now proposed to proceed with upgrade of the boat ramp car park in two stages, the first will incorporate construction and sealing of the car park and the second stage will incorporate landscaping, lighting, car park furniture and signage. The subsequent stages of development will focus on the construction of a toilet facility, a community clubroom, upgrade of the boat ramp and slipway facility and extension of the marina.

The Colac Otway Shire Council advises that funding applications may need to include more information on design, the staging of the project and more precise costings and that further funds may be required to enable the development of concept plans and the preparation of detailed cost estimates.

(iv) CMA consent

There is a separate head of power for approving development on coastal Crown land¹⁸ contained in the CMA in Division 4, sections 37-40. This consent is exercised by the Minister for Environment or delegate; is separate to planning consent; and contains no third party rights of review.

Development issued with a planning permit under the *Planning and Environment Act 1987*, or development not requiring a permit under that Act will generally still require consent under the CMA.¹⁹

¹⁷ On page 23.

¹⁸ Defined in the CMA as amongst other things, Crown land within 200m of the high water mark.

¹⁹ Mr Brooks noted this in his submission for DEPI at paragraph 7.

3.2.2 Otway Coast Committee Coastal Management Plan 2012

The Otway Coast Committee (OCC) is appointed by the Victorian State Government to manage 28.5 km of coastal Crown land between Marengo and Separation Creek (Wye River). The Otway Coast Committee *Coastal Management Plan* was prepared in November 2012.

Under the OCC Coastal Management Plan, it is a priority to ... *Develop and progressively implement foreshore master plans for all townships in the following prioritised order: Apollo Bay, Wye River/Separation Creek, Kennett River, Skenes Creek and Marengo, working closely with local communities.*

3.3 G21 Regional Growth Plan 2013

The G21 Regional Growth Plan was prepared in 2013. The G21 Plan identifies and attempts to manage growth and land use pressures to 2050. It pulls together the strategic land use and growth planning already done across the region and builds on this to identify where future residential and employment growth will occur and the critical infrastructure required to support it. It identifies the redevelopment of the Apollo Bay Harbour as one of its pillar projects.

3.4 Panel discussion and conclusions

The Panel has reviewed the planning context for the Amendment and considers there is overwhelming recognition of the harbour as an important coastal node for recreation, boating and development with significant heritage and landscape values.

There is nothing the Panel can identify that suggests that providing the cultural and natural values of the harbour precinct are protected, that the harbour's role should not be further developed and enhanced. Indeed the policy framework in many instances uses more facilitative language (enhance, improve, develop) than is found in other more 'neutral' policies in the planning scheme.

The challenge is to work through the future planning to determine the type and extent of development in the harbour precinct that achieves the planning objectives, whilst still protecting the important values that exist.

Council has put forward the SUZ in the Amendment, coupled with the need to prepare a Development Plan to guide future development. The Panel is not convinced that the PPRZ and an Incorporated Document could not achieve a similar outcome, but this would require agreement with the community and stakeholders through the Amendment on a detailed development plan (the Incorporated Document), something Council has found difficult to achieve to date. It could also 'lock in' an Incorporated Document that may require a planning scheme amendment to change, not necessarily a desirable outcome.

Instead Council is proposing the 'catch-all' zone, the SUZ. Why the SUZ with a Development Plan Overlay for that element was not used rather than putting the requirement for a Development Plan in the SUZ is still not clear to the Panel. The inclusion of the requirements for a development plan and the development principles in the schedule make the schedule lengthy and complex. It contains a mix of specific design requirements and generic principles that may be difficult to implement in the Development Plan and development stages post-Amendment.

The Panel considers this drafting issue in detail in Chapter 5. On balance however, the Panel considers that the overall directions for developing and enhancing the harbour are clearly supported in planning policy and has accordingly supported the Amendment in its recommendation in chief.

4 Specific issues

Submitters to the Amendment raised a number of issues in relation to the Amendment and harbour development. Many of these issues can only be comprehensively addressed at the Development Plan stage.

Where the issue might lead to a change in Amendment C73, the Panel identifies this and provides commentary accordingly.

4.1 Visual analysis and impact

4.1.1 Site inspection

Prior to the commencement of the hearing the Panel inspected the harbour environs accompanied by Council staff and submitters. The inspection focussed on the southern shore of the harbour including the boat ramp and adjacent car park, the Apollo Bay Sailing Club, the Fishermans Co-op building and its surrounds, the port operations area, the western (Mothers Beach) car park and the inner (western) breakwater.

The Panel also made unaccompanied inspections of areas adjacent to the harbour precinct including its approach from the town centre, the beach and the northern and southern Great Ocean Road entrances to Apollo Bay.

These inspections were particularly useful in giving the Panel an understanding of the landscape, key views of and from within the harbour precinct, the nature of the informal links between existing developments within the harbour precinct, as well as the somewhat indirect current links between the harbour precinct and the town centre.

The extent of the precinct and its relationship to the Apollo Bay golf course and town centre are shown in the aerial photograph in Figure 2.

Existing features within the harbour

The key existing features of the harbour precinct include the low ridge along the southern shore of the harbour sloping gradually from west to east. The central section of this ridge has been partially reclaimed and terraced. The lower terrace adjacent to the harbour is occupied by the port operations office and storage buildings/yard and slipway.

Figure 3 Photo of Port buildings from Fishermans Co-op terrace²⁰



The Fishermans Co-op building is located on the upper terrace surrounded by informal parking and outdoor eating/viewing areas. This area currently provides some of the best views of and from the harbour precinct (see Figure 3). There is a current proposal to construct a formal viewing deck north of the Fishermans Co-op over the steep slope between the two terraces.

From areas to the north of the harbour the view of the southern skyline is dominated by the established trees within the golf course (see Figure 4). These serve to reduce the visual impact of the existing buildings (and particularly the Fishermans Co-op building). Preservation of the existing skyline is widely regarded as a key consideration in the detailed design of any development within the harbour precinct.

Figure 4 View of harbour looking south from end of eastern breakwater



East and west of the reclaimed central section of the southern ridgeline the land is lower. To the east the land slopes more gradually from the Apollo Bay Golf Course down to the boat

²⁰ All photographs from Figure 3 onwards taken by Member Collins.

ramp and waters edge (see Figure 5). Similarly at the western end the Mothers Beach car park and adjacent wharf areas are set at a lower level than the Fishermans Co-op building.

Figure 5 View from golf course across eastern end of harbour



The two breakwaters, eastern (see Figure 6) and inner, enclose the harbour. The breakwaters are currently accessible to pedestrians but do not incorporate any shelter or other amenities for visitors. The inner breakwater includes a limited number of fixed moorings.

Figure 6 Eastern breakwater



A low rock shelf exists west of the boat ramp along the southern edge of the harbour. As shown below this rock shelf is exposed at low tide.

Figure 7 Rock shelf in harbour



An area of shifting sand dunes and the harbour beach (see Figure 8) exists in the south-eastern corner of the harbour precinct. This area also includes a small beach area facing Bass Strait that acts as a sand trap and from which the Council periodically removes excess sand to reduce sand build-up at the harbour entrance.

Figure 8 Harbour beach



Between the sand dune/beach area and the golf course the major feature of the existing landscape is the large informal parking area for the boat ramp.

Figure 9 Informal car park at boat ramp



Views to the harbour precinct

From the township of Apollo Bay and the Apollo Bay beach the view of the harbour precinct is substantially screened by existing vegetation in the foreshore reserve, sand dunes and golf course and by the height of the inner breakwater (see Figure 10).

Figure 10 View to the harbour from the beach to the west



Views from the harbour precinct

From within, the harbour precinct provides a series of close and far views that make the precinct especially valued by Apollo Bay residents and appreciated by those visitors who come upon it. The view to the north and west is dominated by the hills of the Otway Range with the Apollo Bay beach and township in the foreground.

Figures 11 and 12 show views across the harbour to the surrounding hills.

Figure 11 View to north across slipway and harbour



Figure 12 View to west across harbour



At present these distant views can best be appreciated both from the terraced area north of the Fishermans Co-op and from the harbour beach and breakwaters. In future, subject to relocation of the port operations buildings and opening up of the southern foreshore of the harbour, the views will potentially also be able to be seen from both levels of the terraces on the southern shore.

From the port operations wharf and inner breakwater there are also excellent views to Mothers Beach, the coastal dunes and parts of the Apollo Bay town centre.

To the south the view is of, and enclosed by, the golf course.

From the eastern edge of the precinct there are extensive views of Bass Strait and the Great Ocean Road coastline.

A key feature in the regional landscape

In summary the Apollo Bay Harbour is a key feature in the landscape of the southern section of the Otway Coast. It is especially prominent from the surrounding hills and from some of the higher portions of the township.

From the centre of Apollo Bay the harbour is largely screened from direct view although the breakwater and glimpses of the harbour can be seen from the town beach.

At a greater distance the harbour can be seen when approaching Apollo Bay from the north along the Great Ocean Road. From the southern approaches to Apollo Bay the harbour precinct is hidden by the landform and golf course landscaping until you are about to enter the precinct itself.

In the context of the wider Great Ocean Road region the harbour precinct provides Apollo Bay with a significant point of difference and has the potential to become a key landmark on the tourist journey along the Great Ocean Road.

It is therefore not surprising that the potential visual impact of development within the harbour precinct was a central issue for this Amendment.

4.1.2 Submissions presenting visual analysis of the harbour precinct

A substantial part of the submissions presented to the Panel, in effect, dealt with the risk of inappropriate development within the harbour precinct detracting from the visual amenity and character of Apollo Bay. A number of submitters expressed concern that inappropriate development could result in the harbour precinct becoming too much like the Lorne foreshore and contributing to the loss of Apollo Bay's 'coastal village' feel. As outlined later in this report the solution proposed by such submitters was usually to suggest very tight limits on the nature, scale and height of development within the harbour precinct.

Similar concerns had played an important role in Council's decision to scale back the range and intensity of development within the harbour precinct (compared to proposals in earlier planning studies) and to imbed some prescriptive controls on building height and scale in the current Amendment.

Despite the centrality of concerns regarding the potential visual impact of overdevelopment on the amenity of the harbour precinct and the centre of Apollo Bay most submissions presented to the Panel did not include a detailed analysis of the visual context of the

harbour precinct and its relationship to the Apollo Bay township. The principal exception was the submission of Mr Andrew Buchanan and Mr Chris Dance.

This section of the report seeks to draw out from the submission presented, and the inspection undertaken by the Panel, the key issues of potential visual impact of development within the harbour precinct.

Mr Merrett for Council presented the history of Council's planning for the redevelopment of the Apollo Bay harbour including recounting the progressive scaling back of the complexity (in terms of uses) and scale (in terms of building heights and bulk) envisaged for the harbour precinct. The Council made clear that its actions in this regard took account of community concerns regarding the possible visual, and other, impacts of such development.

Council outlined its general goals for harbour development along with the various considerations and constraints that had influenced the final form of the Amendment. It also presented the Council's response to key points made in submissions on the Amendment.

Council did not present an explicit visual analysis of the precinct and its relationship to the Apollo Bay town centre.

The most comprehensive submission directly presenting a landscape analysis of the Apollo Bay harbour precinct was that of Messrs Buchanan and Dance. Their submission included a slide presentation providing a visual and urban design analysis of the harbour precinct along with views of the precinct from a number of vantage points around and within Apollo Bay and of views within and from the harbour area. Their presentation highlighted the variety of views of the inner harbour area, the beach and the magnificent Apollo Bay coast and hills landscape setting that can be obtained from viewpoints within the harbour precinct.

Mr Buchanan and Mr Dance emphasised, among other things, that:

- *despite its close proximity to the Apollo Bay activity centre (approximately 800 metres west) the Harbour is physically and visually remote from town and peripheral to the tourist experience (of Apollo Bay). Due to its indirect relationship to the road network, and because of the encirclement of the intervening golf course landscape, this precinct is obscured from the public view and, as a consequence, difficult to find*
- *the Foreshore Reserve, which is located between the Great Ocean Road and the coast at Apollo Bay terminates at the fenced golf course thereby preventing access by park users to the Harbour*
- *Likewise from the south on Mounts Bay, the foreshore reserve is terminated by (the golf course and)pedestrian access is further impeded by the Barham River estuary*
- *the Discontinuity of the Coastal reserve, especially from the north, and consequential 'crowding' of the Harbour by the surrounding golf course, is a major impediment to optimal harbour development and connectivity to the Town.*

Messrs Buchanan and Dance pointed out that while the harbour can be seen from the Apollo Bay beach the view is essentially of the inner breakwater with little of the activity within the harbour being visible. They contrasted this with the attractive glimpses into the inner

harbour that could potentially be obtained from a viewpoint on the dunes above Mothers Beach. At present this view is inaccessible to both locals and tourists. They concluded that, at present, *In effect this renders the Harbour “an island” in our coastal landscape.*

They concluded that *there appears to be an absence of an overall guiding Urban Design Framework which addresses the long-term integration of the foreshore reserve, central activity area with the Harbour and beyond to the Barham River estuary and Mounts Bay.* They suggested that such a Framework needs to *‘transcend the detail and establish an overall vision not unlike the Town’s founding fathers’ commitment that led (in 1910) to the grand avenue of Cyprus trees and other works stretching from Wild Dog Creek to Nelson Street.*

They proposed that a *refreshed Urban Design Framework Plan should be prepared to illustrate these changes in the context of the Foreshore Reserve from Cawood Street to the estuary and beyond* and concluded that *In the final analysis, it is the Harbour that should be the climax of Apollo Bay’s foreshore and seamlessly integrated with it.*

In addressing the views and the visual experience of visitors once they area within the harbour precinct Messrs Buchannan and Dance stressed that *‘it is important to accentuate the charming working port character of the Harbour precinct. The Harbour currently manifests an honest and functional vibe; there is a strong sense of purpose evidenced by fishing activity, boat maintenance, etc, with little affection ... The balance of a wide range of uses and activities helps create that character and should be embraced in any future development of the site’.* They contrasted this character of Apollo Bay Harbour with *‘a number of similar little fishing villages in south-east Australia (that) have been “made over” and in the process, to their detriment, the balance of land uses has been tipped usually in favour of marinas installed for part-time recreational boating.’*

Messrs Buchannan and Dance’s visual analysis led them to suggest *‘creating in the Harbour a little nautical hamlet between the western car park and the launching ramp. Such a maritime/urban node should be expressed dramatically in design terms with clear visual links between it and the surrounding landscape... If properly designed, such a little village would constitute a “sight for sore eyes” for sailors entering after a stormy “pasting” in Bass Strait, a beautiful and comfortable place to eat, drink, promenade and hang out.’*

Messrs Buchanan and Dance’s submissions in relation to the landscape and urban design concluded by emphasising that *‘Access is vital and needs to be direct and would be helped by the destination (Harbour) being clearly visible and attractive from afar.’*

While a number of other submitters or witnesses made points that implicitly relate to the potential impact on the landscape and key views of development within the harbour precinct few directly presented an analysis of the landscape in and around the harbour precinct.

For example Ms Ngaira Smith emphasised the quality and importance of the views from the informal viewing area, and the proposed 24 metre by 4 metre deck, north of the Fishermans Co-op. Ms Smith expressed concern regarding the impact on these views of foreshore development that extended to the 9 metre height limit proposed in the Amendment which

she stated would change the vista from the northern (inner) breakwater to *'shop, shop, shop and glass, glass, glass and not that of a working harbour'*.

On the other hand the submission presented by the Apollo Bay Chamber of Commerce and Tourism emphasised the importance of a landmark development that would draw attention to the harbour precinct from the town centre and the approaches to Apollo Bay and encourage tourists to the region to spend more time and money in Apollo Bay.

The Chamber's submission was supported by Mr Wayne Kayler-Thomson, Chairman of the Great Ocean Road Regional Tourism Board. Mr Kayler-Thomson stated that the harbour precinct was 'an outstanding opportunity that has been wasted for a number of years' and that the Regional Tourism Board considers the harbour precinct *'pivotal to future tourism development in the Apollo Bay region'*. He expressed the view that what was required was *'an iconic destination that would make Apollo Bay a must visit location'* for visitors to the region.

4.1.3 Discussion

Submitters agreed on the importance of careful management of future development to provide for a mix of uses while maintaining the 'working harbour' character of the precinct, to improve amenity, and to protect the heritage values of and views of and from the precinct.

More generally there was broad agreement that any development in the harbour precinct should contribute to protecting and enhancing the 'seaside village' character of Apollo Bay township.

The challenge is how best to achieve these aims while also making the most of the opportunities the redevelopment of the harbour precinct presents for tourism and employment growth and for providing enhanced facilities for the local community.

The planning that has led to the current Amendment provides a useful starting point. In the Panel's view the Council's decisions to cluster development in the south western corner of the precinct; to relocate the harbour operations area so as to maximise public access to the harbours southern edge; to cluster parking into nodes; and to seek to define height and built form controls that ensure that development does not dominate the existing southern skyline, are appropriate. However, the Panel shares the view expressed by some submitters that reliance upon simple land use, height and materials controls is unlikely to ensure a satisfactory outcome.

In the Panel's view a creative urban design process that draws upon a careful landscape, heritage and local character analysis will be central to achieving the best outcome from the redevelopment of the harbour precinct. Such an urban design process can protect and enable both locals and visitors to enjoy the special visual character of the harbour precinct. It should clearly be central to efforts to achieve the Council and community's competing goals for the harbour precinct.

Good urban design input to the preparation of the Development Plan will enable the most valued features of the harbour landscape to be protected, key view corridors to be identified and viable development sites identified. Importantly it should result in the production of a design framework and ongoing design guidelines that enable achievement of the working

harbour and seaside village character sought by submitters while also allowing for creative design input at the level of individual development projects as development is staged over a number of years.

4.1.4 Conclusions

The harbour precinct is a key feature of the Apollo Bay landscape. It contributes much to Apollo Bay's 'sense of place'. Its iconic views and opportunities for casual recreation are clearly strongly valued by the community and have the potential to be a significant drawcard for tourists to the Great Ocean Road Region.

It is important that preparation of the Development Plan for Apollo Bay Harbour is carefully managed so as to protect the landscape, views, heritage and recreation opportunities so valued by the local community while also providing for the changing functional needs of a working and recreational harbour and facilitating the investment required to make the precinct a tourism and community asset for Apollo Bay and the region.

Reliance only on relatively simple land use, building envelope, height and material controls is unlikely to be sufficient, either to protect the landscape of the harbour precinct or achieve the seaside village sought by Council and submitters, while also providing the guidance, flexibility and clear decision making criteria required to encourage development. If they fail to do so, the risk for the Apollo Bay community will be either that the special character of the landscape they value is effectively devalued or that a protracted and uncertain planning process will result in little development in the precinct and hence economic or community benefit to Apollo Bay.

In the Panel's view the input of very good urban design skills to the Development Plan process is likely to be critical both to shaping a vision for the harbour precinct that can be shared by most if not all submitters and to achieving a successful outcome from the redevelopment of the harbour.

An appropriate urban design input will start with a thorough landscape, environment and functional analysis of the precinct and its relationship to the township. It will also input to the planning process a good understanding of the practical aspects of development. Importantly it will recognise that development is likely to be staged and undertaken by multiple parties. It will therefore seek to provide an overarching vision for development in the harbour precinct, define critical views and public spaces and set out clear guidelines and performance requirements for individual development projects while allowing flexibility and encourage good design.

The Panel makes some suggested changes to the SUZ2 Schedule and this is discussed further in Chapter 5.

4.2 Built form, height and design excellence

Establishing appropriate development controls and guidelines to ensure that development in the harbour precinct reflects the 'coastal village' character valued by the local community, and encouraging excellence in the design of individual development projects within the precinct, are key concerns of Council and most submitters. A central question for the Hearing was whether the provisions of the Amendment as currently drafted will be adequate to achieve this aim.

4.2.1 Submissions

A good deal of Council's submission focussed on addressing proposals in submissions for changes to the proposed scale of development within the harbour precinct and to the development principles included in the SUZ Schedule. Mr Merrett in Council's submission also addressed the proposal raised in a number of submissions for involvement of an expert committee in preparation of the Development Plan and the consideration of development proposals in the harbour precinct.

In particular the Council submission highlighted the extent to which Council had responded to community concerns by winding back both the extent of land use change and the proposed scale of development in the harbour precinct compared to the earlier planning studies.

In particular Mr Merrett pointed out that:

- The 2007 Draft Master Plan has assumed the relocation of the golf course and relocation of the boat ramp as well as incorporation of a hotel, other tourist accommodation and substantial commercial development along the southern shore of the harbour and over portion of the golf course
- The results of the enquiry by design process undertaken as part of the lead up to the preparation of the 2008 Master Plan retained the golf course and boat ramp in their current locations, significantly reduced the proposed scale of development and opened more of the harbour foreshore up to public access
- In 2012 Council had decided to delete the hotel component of the proposed harbour development, use the proposed hotel site for public open space and prepare design guidelines for development in the harbour precinct
- In 2013 a revised Master Plan provided for relocation of the Sailing Club and expansion of the boat ramp while retaining provision for some commercial development and public access along the foreshore together with some enhancement of the Fishermans Co-op building
- The current Amendment reflected the main features of the 2013 Master Plan and in Council's view provided the certainty that the community was seeking in relation to the scale of future development in the harbour precinct
- The 9 metre height limit specified in the proposed SUZ Schedule for development within the harbour precinct would permit developments of up to three stories. This is consistent with the heights existing in the main shopping strip within the Apollo Bay town centre (although the specified height limit within the town centre is 10.5 metres)

Mr Merrett emphasised that the development principles and guidelines set out in clause 5 of the proposed SUZ Schedule are intended to inform the preparation of the Development Plan and Council does not support changes to the Principles that would provide flexibility for proposed building heights to be varied during the Development Plan process.

He further emphasised that the proposed Development Plan would look to develop a realistic vision for development in the harbour precinct that might be staged over twenty to thirty years.

Mr Tony Webber and Ms Ngaira Smith, in their submissions, both suggested that there would not be broad community support for the scale of development that would be permitted under the current Amendment.

Ms Smith proposed that the extent of retail development in the harbour precinct should be strictly limited so as not to compete with shops in the town centre. She argued that the provision in the Amendment for buildings of up to three stories was inappropriate and that generally buildings of more than one story in height were not required in the harbour precinct. She was concerned that buildings of the scale proposed would cut off views from the proposed deck north of the Fishermans Co-op and be out of character with a working harbour. She did acknowledge that there may be an opportunity for construction of a two storey building 'in a small area'.

Mr Andrew Buchanan and Mr Chris Dance, in their submission, emphasised that there is a need to achieve a balance between guarding against overdevelopment and incorporating sufficient commercial activity to make the harbour precinct viable and able to provide a flow of funds to Council to support ongoing maintenance of the precinct. Mr Buchanan said that many community members considered that the most recent Master Plan did not make sufficient allowance for development within the harbour precinct.

Messrs Buchanan and Dance suggested that the preparation of the Development Plan provided an opportunity to develop a real vision for the harbour precinct but that the community will be concerned that the principles and other controls incorporated into the Amendment would place too many constraints on what the Development Plan could consider. They argued that because of the topography of the southern shore of the harbour (he stated that the deck level outside the Fishermans Co-op was about 8 metres above the harbour and that the roof of the Fishermans Co-op rose another 8 metres from this level) there should be scope to allow development beyond the 9 metre height limit currently specified in the Amendment.

They suggested that there was an opportunity in this part of the precinct to create '*a little nautical hamlet*' which '*should be expressed dramatically in design terms with clear visual links between it and the surrounding landscape*'. They proposed that an architectural competition be conducted for the design of this 'hamlet'.

Finally, they argued that the Amendment needed to '*encourage high quality design outcomes by setting down parameters for design without being excessively prescriptive. History shows that good design cannot be manufactured by way of the planning controls*'.

Mr Riches submission on behalf of the Apollo Bay Chamber of Commerce and Tourism argued that the style and architectural quality of buildings constructed at the harbour would

be critical to the success of the precinct. He argued for the development of an iconic building within the precinct. Speaking in support of the Chamber's submission, Mr Wayne Kayler-Thomson also argued that the harbour development needed to be of a quality that resulted in *'an iconic destination that makes Apollo Bay a must visit location'* for tourists.

4.2.2 Discussion

While a variety of views were expressed by submitters as to the appropriate scale and height of development there does seem to be substantial agreement with the notion of constructing a relatively small development node with a 'seaside village' character in the south western corner of the harbour precinct. There was wide agreement that, seen from the north, the existing skyline must be protected although a number of submitters argued for much lower height limits. There was also broad agreement that the largest and highest buildings should be in the node at the south west corner of the harbour with building bulk and heights progressively decreasing to the east.

There was also broad agreement as to the importance of achieving a high quality of design and construction in any development and of ensuring that the development related well to its surroundings.

There was however a distinct difference of view as to how best to achieve the desired quality of outcome. Some argued that the Amendment needed to be quite specific and proscriptive so as to ensure that inappropriate development did not get approved in future while others argued that the Amendment should set out the broad outcomes that are sought and the key development principles that are to apply but leave the specification of more detailed guidelines and requirements to the Development Plan process.

Those who argued for the latter view emphasised the difficulty of changing overly detailed provisions and the need for flexibility given that development may be staged over an extended period.

4.2.3 Conclusions

The Panel agrees with Council and the majority of submitters that the major development node should be in the south-east corner of the harbour, that it is important to protect the southern skyline, and that building bulk and height should gradually decrease from west to east across the southern shoreline.

Appropriate massing and scale of development along the southern shore likely to be critical to the achievement of the objectives Council has set for the harbour precinct.

In line with the above conclusions, the Panel also believes that Council should seek to encourage development of a small number of multi-purpose buildings rather than allowing the construction of a larger number of smaller, single purpose, structures.

The Panel is not convinced that it is necessary or appropriate to specify height limits in the SUZ Schedule as currently proposed by Council. In the Panel's view it would be more appropriate, in the development principles section of the Schedule, to clearly set out the design outcomes that are sought (for example in relation to protection of the southern skyline when viewed from the north of the harbour) and to leave detailed guidelines for building scale, height and form to be specified following detailed urban design analysis as

part of the Development Plan process. Among other things, this would allow for clearer specification of appropriate built forms and heights in particular parts of the site in place of the general 9 metre height limit and broad reference to 'tapering down' towards the east that is currently included in the Schedule.

The Panel strongly endorses the emphasis placed on the importance of quality and consistency of design of development within the harbour precinct. The Panel shares the doubts expressed by a number of submitters as to whether the type of development principles and controls proposed to be included in SUZ Schedule 2 will be adequate to achieve the desired quality of design outcomes. The Panel believes that it would be more appropriate to rely on a detailed urban design process undertaken as part of the preparation of the Development Plan to specify appropriate development guidelines for each sub-area of the harbour precinct and also to propose specific design criteria against which proposed developments should be assessed.

In the Panels view, it will be critical to ensure that suitable urban design advice is available to Council from the commencement of preparation of the Development Plan. In this context the Council may consider it appropriate to approach the State Government for assistance in accessing suitable design advice given the potential regional tourism significance of the Apollo Bay harbour development. The submission of Chairman of the Great Ocean Road regional Tourism Board is relevant in this regard.

Changes to the SUZ Schedule are discussed in Chapter 5 and shown in Appendix C.

4.3 Car parking

Car parking requirements and their impact on the capacity to accommodate development within the harbour precinct was a central focus on a good deal of the submissions presented to the Panel. Those arguing to exclude an aquatic centre from the precinct focussed much of their case upon the associated parking demand and the potential for it to 'crowd out' of parking required by boaters and other harbour users. A variety of views were also expressed regarding the design of parking areas and the provision in the Amendment for Council to proceed with car park works prior to completion of the Development Plan. This section of the report addresses those issues.

4.3.1 Submissions

Mr Merrett for Council acknowledged that parking is a key issue for the harbour precinct. There has not yet been a formal assessment of the future requirement for car parking within the harbour precinct beyond that associated with use of the boat ramp. Council's view was that such an assessment could only be prepared once there was an agreed vision for development within the precinct – that is as part of the Development Plan process that would more clearly define the scale of development and mix of uses proposed for the harbour area.

Mr Merrett stated that the boat ramp car park is currently used to capacity for limited periods a few days per year. At other times there is spare capacity to accommodate parking for non-boating users of the harbour precinct.

However, he acknowledged that if an aquatic/wellness centre was to be located within the harbour precinct it would bring with it a relatively high peak parking requirement and that *in a geographically constrained area (this) may compete with parking allocated to uses that require a coastal location*. Council therefore proposed that the capacity of the harbour to accommodate the parking requirements associated with an aquatic/wellness centre should be assessed as part of the preparation of the Development Plan. The aquatic centre is discussed further in Section 4.6.

Prior work has led the Council to propose a precinct-based approach to parking provision with two main parking areas – the large car park adjacent to the boat ramp and a somewhat smaller parking node adjacent to Mothers Beach. In addition a small number (six or seven) public car parks will be associated with the Fishermans Co-op. Individual developments within the harbour precinct would not be required to provide on-site parking.

Council has prepared a design for the boat ramp car park and this was reflected in the Master Plan that has now been superseded by the current Amendment. Council stated in its submission that the lack of line marking in the current informal car park meant that it was used inefficiently and that formalising the car park will provide approximately 45 additional car spaces as well as improving traffic and pedestrian safety.

A detailed plan for the Mothers Beach car park has not yet been prepared.

The Amendment provides for some public works, including car parking improvements, to proceed prior to approval of a Development Plan without a requirement for a planning permit.

In response to questions from the Panel as to how Council would avoid the risk of unnecessarily closing off options for the Development Plan by proceeding with such works, Mr Merrett argued that the proposed location of the main car parking areas had been well established through the previous Master Plan process and that there are no other viable locations for the car parking nodes given the constraints on the harbour precinct imposed by the boundaries of the golf course lease. In practical terms proceeding with early works to improve car parking would therefore involve minimal risk. This is especially the case as funds have been allocated by Council to enable the Development Plan process to be commenced as soon as the current Amendment is finalised but there is currently no specific provision in the Council budget for upgrading the car parks.

Mr Tim Cobb, in his submission on behalf of the Apollo Bay Sailing Club, confirmed that the boat ramp car park was only filled to capacity on a few days a year under present conditions. Mr Cobb emphasised however that planning for future development in the harbour precinct should ensure that planning for all proposed land based activities should be consistent with protecting the parking and other functional requirements of core water based activities. Mr Cobb expressed the concern that if this is not done there will be a risk that the working harbour character considered essential for the precinct would be lost.

Mr Cobb also emphasised that the design of the boat ramp car park area needed to provide space for the parking of Flying Fifteen sailboats. He did not consider this to be a problem as only relatively small areas would be required.

In his submission in support of the Sailing Club, Mr Steve Walker, Chief Executive of Yachting Victoria, pointed out that, in his view, the Apollo Bay Sailing Club is the most disadvantaged in the State in terms of its existing facilities and that any substantial upgrading of those facilities would be likely to result in a significant increase in participation in sailing at Apollo Bay. Any such increase in participation would obviously tend to reduce the amount of 'spare' capacity in the boat ramp car park.

Mr Philip Lawson, in his submission, emphasised that the number of recreational boaters using the Apollo Bay harbour varies widely with the time of year and weather. He pointed out that there are no alternatives to Apollo Bay harbour as a sheltered boat launching site along long stretches of the Otway coast. He argued that because of the harbour's safe boat ramp boat owners specifically choose to visit and stay in Apollo Bay. He stated that at peak periods there is already some overflow of cars and boat trailers into streets surrounding the harbour precinct and expressed the view that a minimum of 80 car/trailer parking spaces need to be reserved for recreational boaters in any development plan. Mr Lawson also expressed the view that the current 'nose into the car park edge' parking arrangement, though informal, was very efficient. He was not convinced of the adequacy of the design for the boat ramp car park prepared by Council's consultants.

In her submission, Ms Ngaira Smith stressed that urgent upgrading of car parking areas both at Mothers Beach and the boat ramp is essential for the safety of users of the harbour precinct. She called for all car parking areas to be bituminised with designated parking areas for cars, trailers, buses and bikes and with the use of indigenous planting to break up the large area of car parking.

Ms Smith also pointed out that fishermen park their vehicles at the rear of the Co-op building to load up with ice when going to sea. There will therefore be a need for a marked loading bay lane along the southern wall of the Co-op to ensure that such vehicles do not block the entry road to the precinct.

Ms Smith proposed that time limits on parking be established to help manage demand on parking spaces.

Other submitters such as Mr Finch from the Otway Region Sportfishing Club did not attend the Hearing but, in his written submission, expressed concern that any moves to 'downsize' car parking in the vicinity of the boat ramp could impact on economically important recreational fishing. Mr Hunter from the Bellarine Light Game and Sportfishing Club made a similar written submission.

Mr Andrew Buchanan and Mr Chris Dance, in their submission, emphasised the importance of creating a visual link between the harbour and the town centre through the Mothers Beach car park.

They stated that if Apollo Bay was to obtain the maximum economic benefit from the harbour it would be essential to encourage visitors to extend the time they spent in the township. The harbour was potentially central to achieving this aim but it was critical to provide a visual link from the main street/foreshore reserve to the harbour. They envisaged a view corridor that provided '*an iconic view of a boat on the harbour slipway*'. They expressed concern that if the Council proceeded with work to upgrade the Mothers Beach

car park prior to the preparation of the Development Plan this could close off the option for such a view corridor.

Mr John Riches, President of the Apollo Bay Chamber of Commerce and Tourism, said that the Chamber had drawn up a notional plan to illustrate the type of development that could potentially be located at the south western corner of the harbour and that this could include an iconic, privately developed salt water pool/spa complex. Mr Riches said that he believed there had been too much emphasis on the limitations of car parking and that this could be addressed as part of the Development Plan process.

Mr Geoff Brooks for DEPI pointed out that to help contain the spread of the marine pest plant Japanese Kelp (*Undaria pinnatifida*) there will be a need to provide a boat wash down facility close to the boat ramp. This will need to be taken into account in the design of the boat ramp car park.

4.3.2 Discussion

It is clear that the capacity to provide car parking will be one of the key factors limiting the scale and mix of development within the Apollo Bay harbour precinct and will therefore need to be a central factor to be addressed in preparation of the Development Plan.

The information provided to the Panel was not sufficient to enable us to form any clear view as to potential conflicts in peak parking demands or the appropriateness of the detailed design prepared by Council's consultants for the boat ramp car park.

As peak parking demand will clearly be a limiting factor for development in the harbour precinct, the Panel believes that the preparation of the Development Plan should include active consideration of options for managing parking demand at peak times. In addition to limiting the mix and scale of development this might for instance include time limits on parking at peak periods (as suggested by one submitter), and the possibility of encouraging (non-boating) visitors to park in the town centre and walk rather than drive to the harbour precinct. The latter option would clearly be more possible if there was a clear view corridor and pedestrian path between the town centre/foreshore reserve and the south-eastern corner of the harbour.

4.3.3 Conclusions

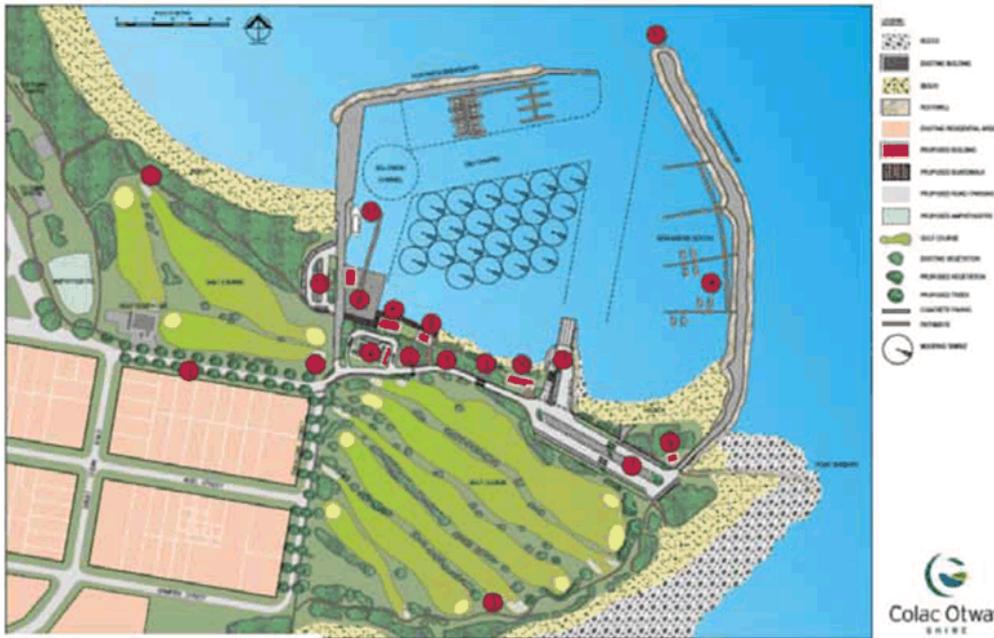
The Panel accepts that the ability to provide appropriate car parking will be a key factor limiting development capacity within the harbour precinct. A detailed analysis of likely parking requirements for activities associated with the working harbour, recreational boating and other potential activities such as an aquatic centre within the harbour precinct will therefore be an important step in preparing the Development Plan.

The Panel supports Council's proposal to cluster car parking into two main nodes, with the potential for very limited additional parking adjacent to the Fishermans Co-op and possibly along the entry road to the precinct. The Panel also accepts that the two main parking nodes should be in the general locations (adjacent to the boat ramp and Mothers Beach) proposed by the Council.

The Panel believes that it would be desirable to delay final design and construction work on the car park nodes until the completion of the Development Plan. This will avoid putting

The decision not to approve the Great Ocean Green development and to retain the golf course in its present location has meant that subsequent planning has been based essentially on maintaining the current road access and supplementing this with improved pedestrian and cycle access from the town centre fitted into the foreshore dunes north of the golf course. This upgraded pedestrian link would enter the harbour precinct through the edge of the proposed Mothers Beach car park. This revised access arrangement was shown in the 2013 Apollo Bay Harbour Master Plan shown in Figure 14.

Figure 14 Draft 2013 Master Plan



The revised planning proposals do not envisage a direct visual link between the town centre and the harbour precinct. This is because existing vegetation on the golf course and in the foreshore reserve acts as a visual barrier between the two areas. Instead it is envisaged that the attention of visitors would be drawn to the harbour through enhanced signage and promotion.

In their submission, Messrs Buchanan and Dance emphasised the importance of creating a visual link between the harbour and the town centre through the Mothers Beach car park.

They stated that if visitors are to be encouraged to extend the time they spent in the township so that Apollo Bay can obtain the maximum economic benefit from the harbour it will be essential to provide a visual link from the main street/foreshore reserve to the harbour. They envisaged a view corridor that provided *an iconic view of a boat on the harbour slipway*. In this context they suggested that *the most significant potential view is via a corridor that can be fashioned between the Port's western car park and the Great Ocean Road close to the Collingwood Hotel ...* (see Figure 15).

Figure 15 Foreshore view to harbour across golf course



They suggested that *'to reveal (this) view to the Harbour some vegetation removal and minor earthworks would be required. To further improve this view, some modification to the western car park would be needed; reformation of grades and relocation of the large refuelling tank would allow for sight lines to the slip and localised views of masts and rigging of boats "on the dry"'*.

Messrs Buchanan and Dance argued that no decision should be made on the design and development of the Mothers Beach car park until the scope for creating such a visual link between the harbour and the town centre has been properly explored as part of the Development Plan process.

The submission of Mr Riches of the Apollo Bay Chamber of Commerce and Tourism, supported by Mr Wayne Kayler-Thomson, stressed the opportunities for encouraging visitors to extend their stay in Apollo Bay and to achieve synergies between activities in the harbour and the town centre. In this context (as noted earlier in this report) Mr Kayler-Thomson emphasised that the Regional Tourism Board considers the harbour precinct *'pivotal to future tourism development in the Apollo Bay region'*. Improving visual and pedestrian linkages between the harbour precinct and the town centre would contribute to achieving this goal.

4.4.2 Discussion

To maximise the benefits of redevelopment of the harbour precinct to the economy of the region it will be important to ensure that the greatest possible number of visitors are made aware of and attracted to the harbour and encouraged to spend more time enjoying both the harbour precinct and the town centre. The existence of clear and direct visual and pedestrian linkages between the town centre, foreshore reserve and the harbour precinct is likely to be central to achieving this goal.

A direct pedestrian linkage between the town centre and the proposed development hub in the south-western corner of the harbour precinct may also encourage locals to combine visits to the harbour and the town centre and to consider parking in the town centre and walking to the harbour in periods of peak parking demand.

4.4.3 Conclusions

Although the current Amendment does not incorporate the area between the harbour precinct and the Apollo Bay town centre, the Panel believes that there would be significant benefit, as part of the Development Plan process, in further exploring the scope for creating more direct visual and pedestrian linkages between the two areas. Doing so would not require any fundamental change in the nature of the development currently envisaged for the harbour precinct. Indeed it would potentially support the Council's goal of establishing in the south western corner of the harbour a viable 'activity hub' that functioned in synergy, rather than in competition, with businesses in the town centre.

This would require cooperation between Council, the Otway Coast Committee and the Golf Course Management. The Panel has included revised provisions in the Schedule to the SUZ Schedule in Appendix C.

4.5 Cultural heritage

The harbour has both recognised Aboriginal and European cultural heritage.

4.5.1 Aboriginal cultural heritage

(i) Submissions

In their submission, Council noted that a report was commissioned in 2006 into Aboriginal heritage around the harbour. This report was provided to the Panel²¹ and was an investigation into Aboriginal cultural heritage, namely a natural spring known as Blackfella's Well, and midden sites.

The location of the well is not clearly known but the executive summary of the Luebbers report notes the well:

...most probably remains at the west end of the depot, specially under the office building at a depth of about 2.0 metres.

According to the report some middens have been destroyed during foreshore works over time but archaeological sensitivity remains east of the Fishermans Co-op Building.

The well and middens are registered with Aboriginal Affairs Victoria (AAV 7620-0192). Aboriginal heritage is protected under the *Aboriginal Heritage Act 2006* and any removal or disturbance of sites will require a Cultural Heritage Permit or Cultural Heritage Management Plan (CHMP) under the provisions of that Act.

²¹ *Archaeological Investigation Blackfella' Well and Middens Apollo Bay Harbour Victoria, 2006, Luebbers and Associates. This report, and submitter Mr Fillmore, drew on work undertaken in an earlier Luebbers report in 2003. This earlier report was not able to be located by Council.*

Mr Fillmore, in his written submission delivered by Mr Webber, outlined a number of historical observations of the well and drew on the work undertaken by Luebbers. He submitted that the well and middens are important sites and could be a feature of a redeveloped harbour precinct.

Mr Fillmore also expressed the view that there has been no progress on the issue of protecting Aboriginal heritage at the harbour and '*Now is the time for this to happen!*'.

In his submission, Mr Buchanan also commented on the significance of the well and how it could form a landscape feature given its central location.

(ii) Discussion and conclusions

No submitter sought to argue that the Aboriginal cultural sites in the harbour precinct do not exist or are not important. The well and midden sites are protected under State legislation and recognised as significant by AAV. Regardless of the plans for the harbour, the Aboriginal cultural heritage must be protected and managed in accordance with the *Aboriginal Heritage Act 2006*.

The Panel notes that Aboriginal heritage is mentioned in the SUZ Schedule 2 in a number of places including an *Aboriginal and Cultural Centre* as a potential key project; and a social principle in Clause 5.0 to:

- *Respect, protect and interpret the Aboriginal and European history of the Precinct.*

The Panel notes these elements of the Schedule and considers they are appropriate in their planning context, but do not take away or modify the existing legal protections; these will still need to be complied with to their full extent.

Essentially the Panel agrees with Mr Fillmore, in that protection and interpretation of Aboriginal cultural heritage in the precinct, with the guidance and approval of Aboriginal people, has the potential to be a feature of the precinct.

Establishing the exact location of the well to ensure its protection will be critical as the projects proceed.

The Panel does recommend that the principle above be modified to include 'heritage and history' as both are relevant to the harbour precinct. The Panel has made the change in the recommended schedule in Appendix C.

4.5.2 European cultural heritage

(i) Submissions

The harbour is currently covered by Heritage Overlay (HO) 300 – Pier Precinct, Apollo Bay. HO300 has the following statement of significance in the planning scheme:

The Apollo Bay Pier Precinct has evolved following the first settlement of the town in the mid - nineteenth century and now includes the Apollo Bay Fisherman's Co-operative building, breakwaters, piers, and beacons. The Apollo Bay Pier Precinct is of historic and social importance to the Shire of Colac Otway. The Apollo Bay Pier Precinct is of historic importance through association with

the development of the fishing industry which played an important role in the Shire's economic development and is demonstrated in the size of the fisherman's co-operative building, constructed at various stages, and the evolution of the piers and breakwaters. The Apollo Bay Pier Precinct is of social and historic importance through association with coastal trade, the life-blood of the settlement of Apollo Bay which had developed outward-looking to the sea and in isolation from the rest of the Shire for the first seventy years of its existence. The Apollo Bay Pier Precinct is historically important through association with the development of a leisure industry in the Shire, demonstrated by the number of berths provided for pleasure crafts.²²

And the associated policy statement to:

- *Ensure the conservation of the Apollo Bay Fishermen's Co-operative building, the timber and stone piers, breakwaters and beacons in a manner that does not detract from the heritage significance of the area.*

As with Aboriginal cultural heritage, the importance of protecting and building on the European heritage significance of the harbour was not disputed by any submitter. A number of submitters noted the importance of the harbour to the town and its links to the development of Apollo Bay.

The original Amendment C73 had proposed to alter the HO300 scheduled to exempt buildings and works consistent with the Master Plan from requiring a permit, but this has not been continued in the re-exhibited amendment. Rather, the exhibited SUZ2 included a general exemption from notice and review rights for buildings and works generally in accordance with the approved Development Plan.

(ii) Discussion and conclusion

Protection and enhancement of European heritage will be an important element in the future redevelopment of the harbour. In the Panel's view, no submitter sought to argue that European heritage was not present and should not be protected.

The Panel notes that European heritage is included in the SUZ2 including in the social principle quoted in Section 4.5.1(ii) above.

In relation to the general exemption included in the SUZ2, the Panel considers this clause is not drafted appropriately. This technical planning issue is discussed further in Chapter 5. In the Panel's view a planning permit application will still be required under the HO.

The broader heritage issues can be considered in the Development Plan preparation, which Council is proposing to put on exhibition.

²² Clause 22.01-4.

4.6 Aquatic/wellness centre in the harbour precinct

Whether a swimming pool²³ and related facilities in some form or another might be included in the harbour redevelopment was an issue for many submitters. This included questions of coastal dependency amongst others.

4.6.1 Submissions

Mr Merrett for Council addressed the issue at length in the Hearing. He submitted that an indoor swimming pool for Apollo Bay has been sought by the community for a number of years.

He outlined the detailed process over a long period of time involving Council and the community led Apollo Bay Aquatic Centre Committee (the Committee). Ms Gross from the Committee also provided a detailed overview of the process to date.

Mr Merrett in submissions noted that Council at its 26 February 2014 meeting resolved amongst other things that it:

Supports an indoor swimming pool/sea baths and other health and wellbeing uses being included within the harbour precinct as part of the adopted Apollo Bay Harbour Master Plan as proposed by submitters through the exhibition process (Resolution 8).

Also supports this as a permitted use at any relevant panel hearing (resolution 9).

At the 23 April 2014 Council meeting, where the resolution to re-exhibit the Amendment was passed, Council resolved amongst other things, to include 'Leisure and recreation' as a Section 2 – Permit required use; and to reiterate:

That Council confirms it supports provision being made within the Special Use Zone (Schedule 2) for a permit application to be made for an indoor swimming pool /sea baths an other health and wellbeing uses, and that provision be made for the potential for such facilities to be included within any Development Plan prepared for the Harbour precinct under those controls (Resolution 3).

In the exhibited schedule, Leisure and recreation was included as a Section 2 – Permit required use, but no mention of a swimming pool was made in the list of potential development plan projects.

At the 22 October 2014 meeting sending submissions to a Panel, the Council did not address the issue specifically but did resolve to support those submissions seeking allowance for a pool in the harbour precinct.

Ms Gross from the Aquatic Centre Committee provided a detailed submission on the role and process the Committee has undertaken in the community in advocating for improved swimming pool facilities. In support of her submissions she called on Mr Damian Gorman who outlined the likely needs and requirements for such a facility.

²³ Aquatic centre, sea baths, indoor swimming pool, wellness centre were all various terms used in the Hearing.

The Panel notes that a majority of submissions to the Amendment supported a swimming pool facility at the Harbour, albeit many of these submissions being form letters.

Other submitters such as Mr Lawson objected to a pool proposal in the Harbour precinct on the basis it is a non-harbour use. He suggested an alternative site for tourist development on the southern side of the Golf Course overlooking the Barham Estuary.

Ms Smith also objected to such facilities in the harbour precinct as it is not a harbour use. She submitted that she has travelled extensively and never seen a pool within a harbour. She further submitted that there are other better locations within Apollo Bay including the existing pool site.

In relation to a 'Health and Wellbeing Centre', she submitted that it would never be viable without linked accommodation.

Mr Walters, in his written submission, supported the work of the Aquatic Centre Committee but did not support a harbour location on the basis that it was consistent with the needs of the existing fishing fleet.

4.6.2 Discussion

In essence the issues appear to be that:

- The community desires a new indoor aquatic centre for year round use by residents and visitors which may also include other elements such as sea baths and a 'wellness centre'
- Council is supportive in principle but is concerned about the capital and operating costs
- There is disagreement about the best location, generally considered to be either the harbour or the P-12 College, where the existing pool is located; but Council's most recent position is to support a harbour location.

Whether a swimming pool or aquatic centre or sea baths should be built in the harbour precinct is ultimately not a question for the Panel, except to the extent that the Amendment might either provide for, or facilitate, such a facility.

One of the issues the Panel, and presumably the community, is grappling with, is what might actually be proposed. This could include a 25m indoor pool and associated facilities²⁴ or it might include some form of more passive recreational sea baths or other tourist targeted features. The final choice and design will determine the level of space and other services required such as car parking²⁵, all of which will have implications for the other Development Plan elements.

As noted before, it is not the Panel's role to decide on the future of such facilities. However it is clear to the Panel that the SUZ2 schedule does not entirely express Council's wishes in relation to consideration of such issues during Development Plan preparation. The Panel has recommended the list of projects be expanded in the schedule to include reference to consideration of a swimming pool and sea baths.

²⁴ Suggested in the 2011 Feasibility Study.

²⁵ Discussed in Section 4.3.

On the issue of coastal dependency, the Panel considers this is not a clear cut 'yes' or 'no' issue. It must consider the broader harbour development issues within the community and how such a facility relates to the harbour and town environment. Integration with harbour recreation, and possibly a salt water element in the proposal, may well overcome coastal dependency issues. Again, this is not a question that can or should be answered decisively by the Panel.

However, it is clear that harbour operations should not be compromised by the development of such a facility.

The Panel notes there are other aquatic centre uses on Crown foreshores, and the Pelican Park facility in Hastings is one that springs to mind.

4.6.3 Conclusions

The Panel concludes that the schedule to the SUZ should be modified to include reference to a swimming pool/sea baths in the harbour precinct as a potential project. The exact wording is provided in Appendix C.

5 Schedule to the proposed Special Use Zone

There were a number of submissions that were related directly to the wording in the SUZ Schedule, notably from Mr Riches of the Apollo Bay Chamber of Commerce and Tourism, Mr Brooks of DEPI, Ms Gross from the Aquatic Centre Committee and others.

For significant changes to the schedule, Table 2 below outlines the section of the SUZ Schedule, the issue and commentary, and a suggested change. A new revised recommended Schedule is included in Appendix C in 'track changes'.

The base version of the SUZ Schedule used is the one with Council's agreed changes from the 22 October 2014 Council meeting included. The post exhibition changes from Council are supported unless otherwise indicated below.

The Panel has focused primarily on Clauses 1-4 of the Schedule. The Design Principles in Clause 5.0 the Panel broadly accepts as generally reasonable but they do contain a mix of generic and specific principles that can be repetitive and will require considerable effort to balance in the Development Plan process. Some comments and changes requested by submitters are included.

Table 2 Changes to the Special Use Zone Schedule 2

| No. | Section | Issue and commentary | Suggested change |
|-----|---------------------------|--|---|
| 1 | 2 nd purpose | Reference to wrong boating plan. | Correct references supplied by DEPI included. |
| 2 | Purposes | Mr Riches suggested a number of changes related to strengthening the focus of harbour development on economic growth, architectural excellence and the establishment of a special planning committee. The Panel has included minor wording changes relating to architecture and urban design but does not support the other changes. | See revised wording of purposes. |
| 3 | Table of uses, Section 1. | DEPI suggested the condition for Outdoor recreation could be expanded to include any boating club. The Panel agrees. | Condition amended. |
| 4 | Table of uses, Section 1 | Ms Gross has suggested that some uses may be allowed prior to the approval of a Development Plan as they do not require a permit. The Panel has clarified the wording in the Schedule. If a Development Plan has not been approved then the Section 1 uses do not meet their condition and become Section 2, Permit required use. | Changes made in Section 1. |
| 5 | Table of uses, Section 1. | The Ministerial Direction on Form and Content of Planning Schemes requires, in Clause 10, reference to Clause 62.01. | Included in Section 1. |

| No. | Section | Issue and commentary | Suggested change |
|------------|--|--|--|
| 6 | Table of uses, Section 2. | Ms Gross suggested that the pool use could be listed as 'Minor sports and recreation facility'. Until the scope of the project is better understood, the Panel considers the broader term as exhibited is appropriate. | No change. |
| 7 | Table of uses, Section 3. | Mr Riches suggested a change to allow for limited dwellings in the zone related to surveillance and vandalism. The Panel does not support this change. | No change. |
| 8 | Clause 3.0, Building and works | As discussed in Section 4.3, the Panel does not consider the redevelopment of major car parks should be allowed in the absence of the Development Plan. | Car parks removed from the first dot point. |
| 9 | Clause 3.0, last paragraph before Application requirements | See Panel discussion on mandatory height controls in Section 4.2. | Delete mandatory height controls and replace with new development principle in Clause 5.0. |
| 10 | Clause 3.0, Exemption | The clause seeks to exempt an application 'under any provision of this scheme' from notice. This broad exemption is not provided for in the SUZ head clause. The Council acknowledged this in correspondence dated 6 February 2015. | Amend exemption. |
| 11 | Clause 4.0, Development Plan | Council approved a 'Development Plan may be approved in stages' clause at its meeting on 22 October 2014. The Panel does not support this. Integrated planning of the whole precinct is critical. In addition minor development may occur prior to the approval of the Development Plan. | Delete reference to staging. |
| 12 | Clause 4.0, Development Plan | Ms Gross identified that an indoor pool/health and well being facilities are not included as key projects. The Panel discusses this in Section 4.6 and supports the inclusion of appropriate wording. | See changes in Key Project 5. |
| 13 | Clause 4.0, Development Plan | DEPI suggested a regular review of the Development Plan to accommodate changing policy settings and community need. The Panel agrees. | Include a review clause at the end of Clause 4.0 requiring five yearly review. |

| No. | Section | Issue and commentary | Suggested change |
|-----|---|--|---|
| 14 | Clause 4.0, Development Plan | DEPI noted that the Development Plan requirements in the SUZ Schedule are sometimes very specific and could result in sub-optimal design outcomes. The Panel agrees. | Clause 4.0 has been edited by the Panel to ensure it does not prevent good design outcomes. |
| 15 | Clause 4.0, Development Plan | DEPI requested the inclusion of a new key project for open space. The Panel supports this as open space provision will be a key element in the harbour redevelopment. | Added in Clause 4.0. |
| 16 | Clause 4.0, Supporting requirements of the Development Plan | DEPI have requested the inclusion of a framework Environmental Management Plan (EMP). The Panel supports this as an EMP will be required for harbour development elements and this will make the requirement clear. | Dot point added to clause. |
| 17 | Clause 4.0, Supporting requirements of the Development Plan | The Panel has included a requirement for an urban design analysis consistent with the discussion in Section 4.4. | Dot point added to clause. |
| 18 | Clause 4.0, Supporting requirements of the Development Plan | Council has included a clause relating to Coastal Hazard Vulnerability Assessment which the Panel supports. The Corangamite Catchment Management Authority in their written submission suggested words on this issue to be included in the Development Plan. The Panel recommends the CCMA be involved in the preparation of the Development Plan. | Words added to CHVA clause. |
| 19 | Clause 4.0, Development Plan | Mr Riches made a number of suggested changes to the form and content of Clause 4.0 including reference to a Special Planning Committee. The Panel has reviewed these changes and does not support them as they do not add to the Development Plan process. | No changes |
| 20 | Clause 5.0, Development Principles and Guidelines | DEPI suggested a range of additional documents be included for reference. The Panel supports the list. | Documents included. |
| 21 | Clause 5.0, Integration with Existing Development. | DEPI suggested that the qualifier 'within the harbour' could be included in the title. The Panel does not support this as the clause is meant to have wider application beyond the harbour. | No change. |

| No. | Section | Issue and commentary | Suggested change |
|-----|---|---|--------------------------------|
| 22 | Clause 5.0, Integration with existing development, 2 nd dot point. | Mr Riches suggested that good architecture should stand out and not be screened by vegetation. The Panel agrees to some extent. | Change to wording recommended. |
| 23 | Clause 5.0, Built Form and Siting, 4 th dot point and 7 th dot point. | Mr Riches suggested it is not clear what these points are trying to achieve. The Panel agrees. | Delete. |
| 24 | Clause 5.0, Building materials, 4 th dot point. | Mr Riches suggested that this may predetermine a particular architectural solution. Whilst building in the salty marine environment has particular requirements, the Panel agrees in principle. | Delete. |
| 25 | Clause 5.0, Economic sustainability new dot point. | Mr Riches suggested a new dot point that broadens the principles beyond fishing and the Co-op. The Panel supports the suggested change. | New dot point added. |
| 26 | Clause 6.0, Decision guidelines, General, 5 th dot point. | Mr Riches suggested the test should be consistency with the approved Development Plan, not the Key Projects in Clause 4.0. The Panel agrees. | Modify clause. |

Appendix A List of submitters

| Submitter | Exhibition 1 | Exhibition 2 |
|---|--------------|--------------|
| Wannon Water | | X |
| Barwon Water | | X |
| Otway Region Sportfishing Club | | X |
| Corangamite CMA | X | X |
| Ken and Judi Forrester | | X |
| Apollo Bay Aquatic Centre | X | X |
| Southern Rural Water | | X |
| Apollo Bay Chamber of Commerce and Tourism | X | X |
| Phillip Lawson | | X |
| James Walters | | X |
| Annika Toussaint | | X |
| Apollo Bay Hotel | | X |
| R Pithie | | X |
| Alan Wood | | X |
| Liz Haughton | | X |
| Skenes Creek Advancement Association Inc. | X | X |
| Apollo Bay Sailing Club Inc. | X | X |
| J and C Marriner | X | X |
| Otway Health and Community Services | | X |
| Apollo Bay Lions Club | | X |
| Robert Knight | | X |
| Tricia Goodlet | | X |
| Amy Harbour | | X |
| Holiday Great Ocean Road | | X |
| Greg Woodward | | X |
| C and J Coleman | | X |
| Yvette Hill | | X |
| Bellarine Light Game and Sportfishing Club Inc. | | X |

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| | | |
|---|---|---|
| Doris Hansen | | X |
| Hiroko Yamamura | | X |
| David Rees | | X |
| Carole and David Webley | | X |
| Ngaira Smith | X | X |
| Andrew Buchanan | X | X |
| Tony Webber | X | X |
| Apollo Bay Golf Club Inc. | | X |
| Apollo Bay and District Health
Foundation | | X |
| EPA Victoria | X | X |
| Lynne and Kevin Ross | | X |
| Pat Macdonald | | X |
| ABGC Nominees Pty Ltd | X | |
| R Kanngaiser | X | |
| R Telford | X | |
| Robert Chenhall | X | |
| Cate Cousland | X | |
| Janette Lewis | X | |
| Peter Fillmore | X | * |
| Edward Stuckey | X | |
| Carol Wilmink | X | |
| John Spencer | X | |
| Paul Johnson | X | |
| Colin Jevons | X | |
| John Howell | X | |
| Southern Otway Landcare Network | X | |
| Mick Heland | X | |
| Department of Environment and
Primary Industries | X | * |
| Yvonne Francis | X | |
| Apollo Bay Fishermans Co-op | X | |
| Great Ocean Road Properties | X | |
| Di Clark | | |

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| | |
|-----------------|---|
| Mark Kininmonth | X |
| Otway Forum | X |
| Ron Clark | X |

* Did not put in a second submission but the Panel gave leave for appearance.

Appendix B Hearing document list

| Doc | Date | Document | Tabled by: |
|-----|---------|---|--|
| 1 | 8/12/14 | Colac Otway Shire submission | Mr Merrett for Colac Otway Shire |
| 2 | 8/12/14 | DEPI submission | Mr Brooks for Department of Environment and Primary Industries |
| 3 | 8/12/14 | Overheads and notes | Mr Cobb for Apollo Bay Sailing Club |
| 4 | 8/12/14 | Mr Fillmore submission | Mr Webber |
| 5 | 9/12/14 | Ms Smith submission | Ms Smith |
| 6 | 9/12/14 | Mr Buchanan submission | Mr Buchanan |
| 7 | 9/12/14 | Apollo Bay and District Health Foundation | Ms McMahon and Mr Holbery for the Foundation |
| 8 | 9/12/14 | Apollo Bay Aquatic Centre Committee submission | Ms Gross for the Committee |
| 9 | 9/12/14 | Mr Gorman submission | Mr Gorman |
| 10 | 9/12/14 | Mr Lawson submission | Mr Lawson |
| 11 | 9/12/14 | Apollo Bay Chamber of Commerce and Tourism Inc. | Mr Riches for the Chamber |

Appendix C Panel recommended SUZ Schedule 2

The recommended changes by the Panel are shown in 'tracked changes' using the version of the schedule adopted by Council at its meeting of 22 October 2014.

COLAC OTWAY PLANNING SCHEME

DD/MM/YYYY
Proposed C73

SCHEDULE 2 TO THE SPECIAL USE ZONE

Shown on the planning scheme map as **SUZ2**

APOLLO BAY HARBOUR

Purpose

To provide for the co-ordinated redevelopment of the Apollo Bay Harbour that is consistent with an adopted Development Plan.

To upgrade the boating facilities to a regional level facility in accordance with the [Western Victoria Boating Coastal Action Plan \(2010\) and Boating Coastal Action Plan Interim Review \(2013\)](#)⁰⁷.

To accommodate Port Operations in new and improved facilities.

To contribute to the development and growth of regional tourism.

To ensure that the combination of uses, their overall density and the scale, character and level of development are compatible with:

- The amenity of the surrounding area.
- The skyline as seen from the northern and eastern breakwater.
- The capacity of the existing road system and any proposed modifications to accommodate an increase in traffic.
- The need to provide for ongoing commercial port operations and commercial tourism uses.

To encourage small scale commercial development that does not adversely impact retailing in the Apollo Bay shopping centre.

To provide safe and efficient vehicle, pedestrian and cycling movements and linkages between the Harbour and the shopping centre.

To encourage a high standard of coastal urban design [and architectural excellence in the public realm](#).

To ensure development is protected from climate change impacts such as sea level rise and storm surge.

To ensure any redevelopment is consistent with the heritage values of the precinct.

To provide for the ongoing operation of the Apollo Bay golf course.

1.0

Table of uses

DD/MM/YYYY
Proposed C73

Section 1 - Permit not required

| Use | Condition |
|---|--|
| Any use associated with the Port Operations or the management of the Harbour by a public authority | Must be generally in accordance with the approved Apollo Bay Harbour Development Plan |
| Car park | Must be generally in accordance with the approved Apollo Bay Harbour Development Plan
Must meet the requirements of Clause 4 of this schedule |

COLAC OTWAY PLANNING SCHEME

| | |
|---|--|
| Food and drink premises (other than Hotel) | Must be generally in accordance with the an approved Apollo Bay Harbour Development Plan
Must not exceed 200sqm in leasable floor area per premises
Any proposal for a Tavern must not include Accommodation |
| Industry (other than Materials recycling, Refuse disposal, Transfer station, Rural industry, Car wash, Dry cleaner, Motor repairs and Panel beating) | Must be generally in accordance with the an approved Apollo Bay Harbour Development Plan
Must be directly related to Harbour or marine based activity |
| Outdoor recreation facility | Must be for a boating or sailing the Apollo Bay Sailing Club and must be generally in accordance with an approved Apollo Bay Harbour Development Plan |
| Place of assembly | Must be for the Aboriginal and Cultural Centre or community spaces in the Harbour edge buildings in accordance with the an approved Apollo Bay Harbour Development Plan |
| Pleasure boat facility | Must be generally in accordance with the an approved Apollo Bay Harbour Development Plan |
| Primary produce sales | Produce must be derived from the sea |
| Utility installation | |
| Any use listed in Clause 62.01 | Must meet the requirements of Clause 62.01 |

Section 2 - Permit required

| Use | Condition |
|---|---|
| Aquaculture | |
| Food and drink premises (other than Hotel) | If the Section 1 condition is not met
Any proposal for a Tavern must not include any form of Accommodation |
| Leisure and recreation | Must not be for Motor Racing Track and Major Sports Ground Facility |
| Market | |
| Museum | |
| Office (other than Medical centre) | |
| Place of assembly (other than Amusement parlour and Aboriginal and Cultural Centre) | |
| Renewable energy facility | |
| Shop (other than Adult sex bookshop, Department store, Restricted retail premises, Supermarket) | |
| Zoo | Must be for a salt water aquarium |

Section 3 - Prohibited

| Use |
|-------------------------------|
| Any use not in Section 1 or 2 |
| Accommodation |
| Hotel |

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2.0 Use of land

DD/MM/YYYY
Proposed C73

Application requirements for Section 2 uses

An application to use land must be accompanied by the following information, as appropriate:

- For any use that is not consistent with the *Apollo Bay Harbour Development Plan* a report must be prepared that addresses the need for the use in the Harbour and demonstrate that it is consistent with the objectives for, and will not impede any use that is supported by, the *Apollo Bay Harbour Development Plan*.
- The purpose of the use and the types of activities that will be carried out.
- Plans drawn to scale showing the layout of the proposed use, including all buildings and works, landscaping and works.
- The likely effects, if any, on adjoining land including noise levels, traffic, the hours of operation, light spill, and delivery and despatch of goods, materials and waste management.
- Details of all infrastructure, drainage and effluent disposal requirements.

3.0 Buildings and Works

DD/MM/YYYY
Proposed C73

A permit is not required for:

- The ~~redevelopment of the Mother's Beach car park~~, extension of the boat ramp, ~~the boat trailer/car park~~ and car parking associated with the Fisherman's Co-op building.
- Minor public works such as construction of pathways, trails, seating, picnic tables, drinking taps, shelters, sculptures and art work, barbeques, rubbish bins, security lighting, irrigation, drainage or underground infrastructure provided these are in locations that are generally consistent with the *Apollo Bay Harbour Development Plan*.
- Dredging works undertaken by or under the authority of the relevant Harbour management authority to maintain navigable depths in existing channels or waterways and the Harbour to ensure continued access to facilities.
- Works undertaken by or on behalf of the public land manager.

All buildings and works must be generally in accordance with an approved Development Plan required under Clause 4.0.

~~All new buildings must not be more than 2 storeys in height, or more than 9m above natural ground level. New harbour front buildings should taper down from two storey buildings towards the east, in line with the taper of the landscape.~~

Application requirements

An application for buildings and works must be accompanied by the following information, as appropriate:

- A report that addresses the design guidelines of this schedule and consistency with any approved Development Plan.
- Plans and elevations drawn to scale showing the location, height, colour, material and finishes of all buildings and location of public lighting to improve security and safety.
- The effect of the development on visual and landscape character.
- A traffic and parking assessment dependent on the scale of the use proposed.

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- Built form and character, and the relationship of the proposed development to existing development on or near the site.
- Means of accommodating storage requirements and the effect of any outdoor storage.
- Means of providing safety and security enclosures and the effect of any fencing or enclosure.
- A Coastal Hazard Vulnerability Assessment (CHVA) that is appropriate to the scale of the development proposed.
- Commercial waste removal arrangements and infrastructure servicing.

Exemption from notice and review

An application under [this schedule](#) ~~any provision of this scheme~~ which is generally in accordance with the approved Development Plan is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0 Development Plan

DD/MM/YYYY
Proposed C73

Prior to the granting of a permit for the construction of buildings and works a Development Plan must be approved by the responsible authority. ~~The Development Plan may be approved in stages.~~

The purpose of the Development Plan is to provide a development framework upon which the Apollo Bay Harbour will be developed over the next 20-30 years. The Development Plan should include the following key projects:

1. The **Harbour Precinct Entry Road** upgraded to straighten the 'dog leg' south of the Fisherman's Co-Op building and provide safer access to the Harbour.
2. **Breakwater Road** upgraded to a [suitable standard to allow two-way vehicle movement with allowance for 7 metre wide pavement to provide a two-way vehicle movement with asphalt, linemarking and edge treatment that includes flush kerbs with vegetated drainage swales.](#) ~~Parallel parking will be provided along Breakwater Road~~ if there is sufficient space.
3. The provision or upgrade of **pedestrian/bicycle paths** throughout the harbour, which includes:
 - A new boardwalk along the Harbour water's edge ~~at least 6 metres in width east of Mother's Beach car park.~~
 - [New linkages between the Harbour and the town centre that consider modes and capacity, environmental and cultural values and the opportunity for vistas. A new 3 metre \(approx\) wide promenade to connect the Harbour to the town centre via Mother's Beach carpark. This will link with existing paths between the golf course and beach.](#)
 - ~~A new path from the existing paths that terminate west of the golf club house, along Nelson Street to the new entry road.~~
 - Pedestrian access ~~will continue~~ to the [two breakwaters western and northern breakwaters](#) ~~these paths could be separated from operational vehicles or operate as shared zones with priority for pedestrians depending on detailed assessment of public safety issues with due consideration given to public safety.~~

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- ~~A new path along the dunes at the southern end of the golf course. This would take the form of a nature walk highlighting coastal vegetation and providing beach access and coastal viewing points.~~
 - ~~If achievable with vegetation and topographical constraints, a new path along the north side of Breakwater Road, connecting the existing Co-op building to eastern parts of the precinct.~~
4. Retain and provide for the eastern extension of the **Fisherman's Co-Op building** for fish processing operations. The western and southern façades to be upgraded due to their high visibility to the Harbour Entry Road. Increase opportunities for public use such as tables and chairs associated with the retail fish shop and viewing area where possible. ~~Provide outdoor tables and chairs north of the existing carpark for public use.~~
 5. Construct new **Harbour Edge Buildings** located on the water's edge south of the boardwalk ~~which could cater for a mix of:~~ Ground floor to have retail, hospitality, aquatic based recreation, health and wellbeing facilities ~~focus with active edges to the boardwalk promenade. Uses in these buildings could include shops, cafes and restaurants at ground level~~ and multi-use event and meeting spaces ~~above~~ for club, community and commercial activities. ~~Upper floor could also have potential for commercial, interpretive or community space, with balcony to maximise Harbour views.~~ The design ~~will~~ could include:
 - Use of balconies to maximise Harbor views;
 - East, west and north facades to be glazed and openable to provide flexibility and views to the Harbour;
 - Shading device to be provided on north facades; and
 - Provide for temporary wind protection for outdoor dining areas and use materials that are highly transparent (e.g. glass).
 6. Provide a **new recreational marina mooring** along the eastern breakwater wall based on appropriate mooring types. ~~This is to be based on Mediterranean berths and swing moorings or other appropriate mooring mechanism or types.~~
 7. Increasing the capacity of the ~~Widening of the~~ **boat launching facility** to at least three ~~3~~ lanes. Upgrading of the boat trailer parking area to maximise the number of parking spaces available for both trailers and vehicles. ~~This includes a new formal access arrangement to the boat launching facility, including~~ sealed pavement surfaces, linemarking, drainage, landscaping and signage. ~~Curtail spread of informal parking and gravel paved areas to improve visual appearance. Southern parking bays (rear in) to feature grassed surfaces for the rear half of each bay to reduce impact and improve infiltration. Central parking bays can either be rear in or forward parking and also be configured to cater for vehicles only during non-peak boat use times. Allow for two-way access lanes around parking area. Provision~~ Provide a reasonable amount of non-trailer parking for ~~of some car parking bays to only service~~ other users of the precinct, such as beach goers, coastal walkers and sightseers.
 8. Relocate and expand the **Port Operations** and boat repair facility to the north to provide for the pedestrian boardwalk. A Harbour Master's Office will be provided, together with new buildings for equipment storage, staff amenities and administration. Transparent fencing will be provided to enable public viewing of the working Harbour. Building to be used for storage and repair of boats. Maximum building height of 7 metres. Building to be highly articulated and to demonstrate design excellence and use of high quality coastal materials. Upper level mezzanine to house Harbour Master's office where clear view of Harbour can be obtained, including space for toilets, offices and tearoom. Provide access to office directly off the western breakwater to improve safety for visitors by avoiding need to enter the boat yard.

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9. A new **Sailing Club Facility** to be provided generally in its current position ~~west of the boat launching ramps~~ to provide clubrooms, race control facilities, toilets and storage space, roll-in access to the Harbour Beach and grassed surrounds for small craft laydown. ~~Surrounds to club to be grassed to allow for casual small sailing craft to be temporarily parked. Sailing Club location to provide roll-in access to the Harbour Beach. Building facades should be articulated with emphasis on light and shade with screens and light weight coastal materials~~
10. The potential for an **Aboriginal and Cultural Centre** ~~to the west of the Sailing Club~~, subject to a detailed business case and the support of the Aboriginal community. This ~~building could be a double-storey construction featuring extensive glazing to all facades and shading devices to north side~~ centre could provide Aboriginal and ~~C~~cultural interpretation ~~to be provided through displays and other temporary structures and installations. Building could incorporate externally accessed public toilets to service the boat ramp area.~~
11. Provision for other **commercial water based development** (~~e.g. floating restaurant~~)
12. Provide for an **extension of the eastern breakwater** by approximately 60 metres to enable safe access to the Harbour.
13. Upgrading of the **Mother's Beach car park** to maximise the number of parking spaces available. This includes a new toilet/change facility building, sealed pavement surfaces, linemarking and landscaping.
14. Retention and enhancement of Public Open Space within the Harbour that contributes to the use and enjoyment of the harbour and vistas to the Otway Coast. Landscape design, construction and maintenance of open space alongside the proposed built form elements must be undertaken to a high standard. The open space should be enhanced by the provision of informal spaces and connectivity that encourages informal recreational use by the public.

Supporting requirements of the Development Plan

The Development Plan will resolve a complex set of site issues and must include the following, as appropriate:

- Location of existing buildings and landscape features based on accurate cadastre showing contours to 0.2m;
- ~~A site context plan and report that is based upon accurate cadastre showing contours to 0.2 metre and demonstrates how the buildings and works satisfy the design guidelines contained in this schedule; An Urban Design analysis that responds to the Development Principles and Guidelines in Clause 5.0 and the proposed Key Projects to ensure a coherent basis for the Development Plan and harbour development. This analysis should include internal harbour precinct design considerations and options for creating a clear visual and pedestrian link between the Apollo Bay town centre, the foreshore reserve and the harbour precinct;~~
- ~~Location of existing buildings and landscape features and their relationship to proposed key projects;~~
- A Coastal Hazard Vulnerability Assessment (CHVA) prepared in consultation with the Corangamite Catchment Management Authority that establishes the design height of buildings to be protected from the sea level rise and storm surge impacts of climate change;
- An assessment of the operational requirements of the Apollo Bay Harbour Port Operations and the representation of these requirements on the Development Plan;

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- An implementation plan that identifies how the Port Operations area will be redeveloped and whether this can be achieved in stages. The plan should also address any interim measures for the Port Operations that will not undermine the ability to achieve the Development Plan outcomes for this area;
- The final design of the expanded Boat Ramp;
- The final designs of the Mothers Beach and Boat Trailer car parks;
- Pathways / pedestrian access including separation of Harbour traffic from pedestrians;
- A traffic and parking assessment;
- The identification of and management of native vegetation and weed control;
- ~~The location, height, dimensions and cross sections for all buildings and works associated with key projects showing natural ground levels and any areas of cut and fill;~~
- The proposed use of each building;
- Locations of all BBQs, toilets and shower facilities; **and**
- The indicative materials and finishes that are considered appropriate for of all buildings within the harbour precinct; and
- A framework Construction Environmental Management Plan (EMP).

Notice requirements

The draft Development Plan must be placed on exhibition for a period of 6 weeks for community comment prior to its consideration by the Responsible Authority. The Development Plan may be amended by the responsible authority as required. Any significant change to the Development Plan may require further exhibition.

Car parking – Precinct approach

The approach to parking is precinct based. Parking is to be provided at the following four nodes:

- Mother’s Beach carpark;
- Fisherman’s Co-Op;
- Boat trailer car park; and
- Breakwater Road.

The upgrading of these parking facilities will be used as justification to waive the Clause 52.06 parking requirements generated by individual projects or uses identified in the *Apollo Bay Harbour Development Plan*.

Review of Development Plan

The Development Plan should be reviewed every five years from the date of approval to reflect changing policy settings and community views. The review must include notice as described in the Notice requirements clause above.

5.0 Development Principles and Development Guidelines

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All buildings and works should be consistent with:

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- *Siting and Design Guidelines for Structures on the Victorian Coast*, Victorian Coastal Council (May 1998),
- *Good Design and the Coast, Issue 2003*, by the Office of the Victorian Government Architect,
- *Victorian Coastal Strategy 2008/14* or subsequent version.
- [Western Victoria Boating Coastal Action Plan \(2010\) \(Western Coastal Board\)](#)
- [Boating CAP Interim Review \(July 2013\) \(Western Coastal Board\)](#)
- [Local Port of Apollo Bay Sand Management Study – Peer review \(June 2010\) \(Oldfield Consulting\)](#)
- [Port of Apollo Bay Future Capacity Study \(July 2006\), \(Meyrick and Associates\)](#)
- [Apollo Bay Harbour Coastal Hazards Vulnerability Assessment \(June 2013\) \(Water Technology\)](#)
- [Port of Apollo Bay Safety and Environment Management Plan 2008-2011 \(revised 2013\) \(Colac Otway Shire\)](#)

Development Principles

All buildings and works should be consistent with the following Development Principles:

Integration with Existing Development

- Reinforce the qualities of the [working](#) Harbour and facilitate integration with the Apollo Bay town centre in a staged and orderly manner.
- Use landscape screening to minimise the visual impact of buildings from surrounding [key](#) vantage points, where [appropriate and](#) practicable.
- Integrate and enhance buildings that have service functions (e.g. Fisherman’s Co-operative, boat storage and maintenance), and also minimise visual intrusion whilst maximising natural surveillance.
- Develop a physically and visually coherent built form within the coastal landscape setting and character.

Built Form [and](#) Siting

- [Built form should be of a height which does not intrude upon the southern skyline of the harbour precinct as viewed from north of the harbour and the town centre. The height of development should taper down towards the east, in line with the taper of the landscape.](#)
- [A smaller number of multi-use or agglomerated buildings is to be preferred to development of a larger number of single purpose, stand alone, structures.](#)
- Minimise impact on the marine ecosystem and surrounding coastal setting.
- Protect sensitive habitat zones (i.e. native vegetation and marine life) by locating higher intensity development in low environmental value zones.
- Maximise coastal, sea and green open space views and vistas towards and from all commercial and community buildings/ activities on site.
- ~~Minimise visual blocking of boardwalks by and from structures.~~
- Minimise car intrusion into sensitive habitat.

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- Ensure minimal visual intrusion ~~of~~by required car parking.
- ~~Minimise built form impact (scale and footprint) by responding to surrounding context and character.~~
- Maximise northerly solar access and use of natural light and ventilation.
- Reduce vandalism by maximising surveillance (e.g. use of boardwalks, pathways and articulated built form).
- Ensure public access to public land is maintained except where the interests of security or safety predominate.
- New buildings will be setback and elevated consistent with the Coastal Hazard Vulnerability Assessment (CHVA).
- Development should not cause any detrimental affects to natural coastal processes.
- Avoid continuous overshadowing of foreshore areas.

Building Fabric

- Encourage retention and re-use of buildings of heritage and community value, including the heritage listed Fisherman's Co-Op building, thereby reducing the embodied energy and waste in creating a new future for the site.
- Adopt best practice energy reduction for new development where possible.

Building Materials

- Utilise, where appropriate, materials from the local area and consistent with local coastal architectural character.
- Encourage subdued palettes.
- Select durable high quality materials with enduring finishes that are appropriate to a coastal environment.
- ~~Ensure self washing of roofs greater than 5 degrees.~~
- Use glass to maximise views whilst avoiding glare/reflection (by utilising elements such as eaves overhang, pergolas, louvres and other types of screening system).

Landscape & Public Realm

- Minimise disturbance to indigenous vegetation and use local indigenous species that are resistant to coastal exposure in landscaping where possible and appropriate.
- Maintain and enhance the open views to the ocean, Harbour and hills.
- Reinforce the established landscape character, including:
 - The structure of tall open trees that are well established on the golf course
 - The landscape of open lawns for ease of public use and maintenance
 - Indigenous vegetation providing shelter and stability to the dune systems.
- Replace substandard road pavements & other port infrastructure with new pavements, furniture and other public infrastructure that is consistent, well designed, high quality and complements the coastal environment.

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Water Sensitive Design Measures - Precinct Based

- Ensure stormwater harvesting and reuse from all major roof surfaces.
- Support grey water treatment and recycling for use in landscaping.
- Minimise the area of impervious surface finishes.

Waste Minimisation

- Minimise construction waste through a construction management plan for each stage of the development.
- Provide on-going waste management and sorting areas.

Reduce Carbon Footprint

- Adopt leading practice passive solar energy principles to minimise energy demand of new or extended buildings.
- Include on-site renewable energy sources, e.g. photovoltaic panels and thermal solar water heating where practical and where this does not adversely impact upon visual appearance (e.g. glare).
- Maximise north facing façades as renewable energy collectors.
- Support alternative sustainable transport initiatives, including new walking and cycling networks both through the precinct and at the street interfaces with the site.
- Provide high quality and secure short-term bicycle facilities in all commercial areas and lockers/ showers within the waterfront area.

Social Principles

Cultural and Aesthetic

- Respect the setting and visually complement the surrounding coastal landscape and development by reinforcing and integrating the boating precinct and the town centre.
- Respect, protect and interpret the Aboriginal and European [heritage and](#) history of the Precinct.
- Promote local ecological values by potentially building an on-site Cultural Centre and developing nature walks to contain and direct visitor access.
- Provide dedicated areas for both fishing and boating, free of constraints that might arise when located in close proximity to tourism.
- Ensure the design language highlights the public elements of the precinct and promotes active ground floor uses in buildings.

Sustainable Mobility

- Extend walking and bike trails into and through the site.
- Improve shared pathway to provide for safer cycling and walking alternatives linking the Apollo Bay Town Centre and Harbour.
- Integrate new road design with landscape design to ensure provision of safe and convenient paths, street trees to provide shade and bicycle parking facilities.

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- All public and commercial spaces are to be designed to allow access for all abilities.
- Ensure compliance with the Disability Discrimination Act throughout the Precinct.

Economic Principles

Economic Sustainability

- Support the on-going viability of fishing operations and the Co-op.
- [Provide for a mix of activities and experiences by developing a lively and diverse precinct that will attract visitors to the region and service community needs.](#)

6.0 Decision guidelines

DD/MM/YYYY
Proposed C73

Before deciding on a permit application or to approve a Development Plan, in addition to the decision guidelines in Clause 65, the responsible authority must consider as appropriate:

General

- The State Planning Policy Framework and the Local Planning Policy Framework including the Municipal Strategic Statement;
- The purpose of this Schedule;
- The comments of the Department of Environment, [Land, Water and Planning Primary Industries](#) (DELWPI);
- The effect of the use and development on the operations of the Harbour;
- The consistency of the use and development with the [approved Development Plan identified key projects in Clause 4.0 of this schedule](#).

Use

- The effect of the use on the Apollo Bay Shopping Centre.
- Providing and connecting to services and utilities.
- The impact of traffic generated by the proposed use and development and whether any additional traffic management or traffic control works are likely to be required in the area.

Subdivision

- Consistency with the *Apollo Bay Harbour Development Plan*.
- The effect the subdivision will have on the potential of the area to accommodate the uses supported by the *Apollo Bay Harbour Development Plan*.

Building and works

- The impact of coastal processes;
- Wastewater and stormwater disposal and discharge and infrastructure provision;
- The impact upon any areas of pre or post contact heritage;
- The use of lighting to increase security for the precinct; and
- The visual impact of the development when viewed from surrounding land and water.

7.0 Advertising signs

DD/MM/YYYY
Proposed C73

Advertising sign requirements are at Clause 52.05. This schedule is in category 3.

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DD/MM/YYYY
Proposed C73

SCHEDULE 2 TO THE SPECIAL USE ZONE

Shown on the planning scheme map as **SUZ2**

APOLLO BAY HARBOUR

Purpose

To provide for the co-ordinated redevelopment of the Apollo Bay Harbour that is consistent with an adopted Development Plan.

To upgrade the boating facilities to a regional level facility in accordance with the Western Victoria Boating Coastal Action Plan (2010) and Boating Coastal Action Plan Interim Review (2013).

To accommodate Port Operations in new and improved facilities.

To contribute to the development and growth of regional tourism.

To ensure that the combination of uses, their overall density and the scale, character and level of development are compatible with:

- The amenity of the surrounding area.
- The skyline as seen from the northern and eastern breakwater.
- The capacity of the existing road system and any proposed modifications to accommodate an increase in traffic.
- The need to provide for ongoing commercial port operations and commercial tourism uses.

To encourage small scale commercial development that does not adversely impact retailing in the Apollo Bay shopping centre.

To provide safe and efficient vehicle, pedestrian and cycling movements and linkages between the Harbour and the shopping centre.

To encourage a high standard of coastal urban design and architectural excellence in the public realm.

To ensure development is protected from climate change impacts such as sea level rise and storm surge.

To ensure any redevelopment is consistent with the heritage values of the precinct.

To provide for the ongoing operation of the Apollo Bay golf course.

1.0

Table of uses

DD/MM/YYYY
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Section 1 - Permit not required

| Use | Condition |
|---|--|
| Any use associated with the Port Operations or the management of the Harbour by a public authority | Must be generally in accordance with an approved <i>Apollo Bay Harbour Development Plan</i> |
| Car park | Must be generally in accordance with an approved <i>Apollo Bay Harbour Development Plan</i>

Must meet the requirements of Clause 4 of this schedule |

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| | |
|---|---|
| Food and drink premises (other than Hotel) | Must be generally in accordance with an approved <i>Apollo Bay Harbour Development Plan</i>

Must not exceed 200sqm in leasable floor area per premises

Any proposal for a Tavern must not include Accommodation |
| Industry (other than Materials recycling, Refuse disposal, Transfer station, Rural industry, Car wash, Dry cleaner, Motor repairs and Panel beating) | Must be generally in accordance with an approved <i>Apollo Bay Harbour Development Plan</i>

Must be directly related to Harbour or marine based activity |
| Outdoor recreation facility | Must be for a boating or sailing club and must be generally in accordance with an approved <i>Apollo Bay Harbour Development Plan</i> |
| Place of assembly | Must be for the Aboriginal and Cultural Centre or community spaces in the Harbour edge buildings in accordance with an approved <i>Apollo Bay Harbour Development Plan</i> |
| Pleasure boat facility | Must be generally in accordance with an approved <i>Apollo Bay Harbour Development Plan</i> |
| Primary produce sales | Produce must be derived from the sea |
| Utility installation | |
| Any use listed in Clause 62.01 | Must meet the requirements of Clause 62.01 |

Section 2 - Permit required

| Use | Condition |
|--|---|
| Aquaculture | |
| Food and drink premises (other than Hotel) | If the Section 1 condition is not met

Any proposal for a Tavern must not include any form of Accommodation |
| Leisure and recreation | Must not be for Motor Racing Track and Major Sports Ground Facility |
| Market | |
| Museum | |
| Office (other than Medical centre) | |
| Place of assembly (other than Amusement parlour and Aboriginal and Cultural Centre) | |
| Renewable energy facility | |
| Shop (other than Adult sex bookshop, Department store, Restricted retail premises, Supermarket) | |
| Zoo | Must be for a salt water aquarium |

Section 3 - Prohibited

| Use |
|--------------------------------------|
| Any use not in Section 1 or 2 |
| Accommodation |
| Hotel |

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2.0 Use of land

DD/MM/YYYY
Proposed C73

Application requirements for Section 2 uses

An application to use land must be accompanied by the following information, as appropriate:

- For any use that is not consistent with the *Apollo Bay Harbour Development Plan* a report must be prepared that addresses the need for the use in the Harbour and demonstrates that it is consistent with the objectives for, and will not impede any use that is supported by the *Apollo Bay Harbour Development Plan*.
- The purpose of the use and the types of activities that will be carried out.
- Plans drawn to scale showing the layout of the proposed use, including all buildings and works, landscaping and works.
- The likely effects, if any, on adjoining land including noise levels, traffic, the hours of operation, light spill, and delivery and despatch of goods, materials and waste management.
- Details of all infrastructure, drainage and effluent disposal requirements.

3.0 Buildings and Works

DD/MM/YYYY
Proposed C73

A permit is not required for:

- The redevelopment of the Mother's Beach car park, extension of the boat ramp, the boat trailer/car park, and car parking associated with the Fisherman's Co-op building.
- Minor public works such as construction of pathways, trails, seating, picnic tables, drinking taps, shelters, sculptures and art work, barbeques, rubbish bins, security lighting, irrigation, drainage or underground infrastructure provided these are in locations that are generally consistent with the *Apollo Bay Harbour Development Plan*.
- Dredging works undertaken by or under the authority of the relevant Harbour management authority to maintain navigable depths in existing channels or waterways and the Harbour to ensure continued access to facilities.
- Works undertaken by or on behalf of the public land manager.

All buildings and works must be generally in accordance with an approved Development Plan required under Clause 4.0.

Application requirements

An application for buildings and works must be accompanied by the following information, as appropriate:

- A report that addresses the design guidelines of this schedule and consistency with any approved Development Plan.
- Plans and elevations drawn to scale showing the location, height, colour, material and finishes of all buildings and location of public lighting to improve security and safety.
- The effect of the development on visual and landscape character.
- A traffic and parking assessment dependent on the scale of the use proposed.
- Built form and character, and the relationship of the proposed development to existing development on or near the site.
- Means of accommodating storage requirements and the effect of any outdoor storage.

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- Means of providing safety and security enclosures and the effect of any fencing or enclosure.
- A Coastal Hazard Vulnerability Assessment (CHVA) that is appropriate to the scale of the development proposed.
- Commercial waste removal arrangements and infrastructure servicing.

Exemption from notice and review

An application under this schedule which is generally in accordance with the approved Development Plan is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

4.0 Development Plan

DD/MM/YYYY
Proposed C73

Prior to the granting of a permit for the construction of buildings and works a Development Plan must be approved by the responsible authority. The Development Plan may be approved in stages.

The purpose of the Development Plan is to provide a development framework upon which the Apollo Bay Harbour will be developed over the next 20-30 years. The Development Plan should include the following key projects:

1. The **Harbour Precinct Entry Road** upgraded to straighten the 'dog leg' south of the Fisherman's Co-Op building and provide safer access to the Harbour.
2. **Breakwater Road** upgraded to a suitable standard to allow two-way vehicle movement with allowance for parallel parking if there is sufficient space.
3. The provision or upgrade of **pedestrian/bicycle paths** throughout the harbour, which includes:
 - A new boardwalk along the Harbour water's edge.
 - New linkages between the Harbour and the town centre that consider modes and capacity, environmental and cultural values and the opportunity for vistas.
 - Pedestrian access to the two breakwaters with due consideration given to public safety.
4. Retain and provide for the eastern extension of the **Fisherman's Co-Op building** for fish processing operations. The western and southern façades to be upgraded due to their high visibility to the Harbour Entry Road. Increase opportunities for public use such as tables and chairs associated with the retail fish shop and viewing area where possible.
5. Construct new **Harbour Edge Buildings** located on the water's edge south of the boardwalk which could cater for a mix of retail, hospitality, aquatic based recreation, health and wellbeing facilities and multi-use event and meeting spaces for club, community and commercial activities. The design could include:
 - Use of balconies to maximise Harbor views;
 - East, west and north façades to be glazed and openable to provide flexibility and views to the Harbour;
 - Shading device to be provided on north façades; and
 - Provide for temporary wind protection for outdoor dining areas and use materials that are highly transparent (e.g. glass).

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6. Provide a **new recreational marina mooring** along the eastern breakwater wall based on appropriate mooring types.
7. Increasing the capacity of the **boat launching facility** to at least three lanes. Upgrading of the boat trailer parking area to maximise the number of parking spaces available for both trailers and vehicles including sealed pavement surfaces, linemarking, drainage, landscaping and signage. Provide a reasonable amount of non-trailer parking for other users of the precinct, such as beach goers, coastal walkers and sightseers.
8. Relocate and expand the **Port Operations** and boat repair facility to the north to provide for the pedestrian boardwalk. A Harbour Master's Office will be provided, together with new buildings for equipment storage, staff amenities and administration. Transparent fencing will be provided to enable public viewing of the working Harbour. Building to be used for storage and repair of boats. Maximum building height of 7 metres. Building to be highly articulated and to demonstrate design excellence and use of high quality coastal materials. Upper level mezzanine to house Harbour Master's office where clear view of Harbour can be obtained, including space for toilets, offices and tearoom. Provide access to office directly off the western breakwater to improve safety for visitors by avoiding need to enter the boat yard.
9. A new **Sailing Club Facility** to be provided generally in its current position to provide clubrooms, race control facilities, toilets and storage space, roll-in access to the Harbour Beach and grassed surrounds for small craft laydown.
10. The potential for an **Aboriginal and Cultural Centre**. subject to a detailed business case and the support of the Aboriginal community. This centre could provide Aboriginal and cultural interpretation..
11. Provision for other **commercial water based development**.
12. Provide for an **extension of the eastern breakwater** by approximately 60 metres to enable safe access to the Harbour.
13. Upgrading of the **Mother's Beach car park** to maximise the number of parking spaces available. This includes a new toilet/change facility building, sealed pavement surfaces, linemarking and landscaping.
14. Retention and enhancement of **Public Open Space within the Harbour** that contributes to the use and enjoyment of the harbour and vistas to the Otway Coast. Landscape design, construction and maintenance of open space alongside the proposed built form elements must be undertaken to a high standard. The open space should be enhanced by the provision of informal spaces and connectivity that encourages informal recreational use by the public.

Supporting requirements of the Development Plan

The Development Plan will resolve a complex set of site issues and must include the following, as appropriate:

- Location of existing buildings and landscape features based on accurate cadastre showing contours to 0.2m;
- An Urban Design analysis that responds to the Development Principles and Guidelines in Clause 5.0 and the proposed Key Projects to ensure a coherent basis for the Development Plan and harbour development. This analysis should include internal harbour precinct design considerations and options for creating a clear visual and pedestrian link between the Apollo Bay town centre, the foreshore reserve and the harbour precinct;
- A Coastal Hazard Vulnerability Assessment (CHVA) prepared in consultation with the Corangamite Catchment Management Authority that establishes the design height of

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buildings to be protected from the sea level rise and storm surge impacts of climate change;

- An assessment of the operational requirements of the Apollo Bay Harbour Port Operations and the representation of these requirements on the Development Plan;
- An implementation plan that identifies how the Port Operations area will be redeveloped and whether this can be achieved in stages. The plan should also address any interim measures for the Port Operations that will not undermine the ability to achieve the Development Plan outcomes for this area;
- The final design of the expanded Boat Ramp;
- The final designs of the Mothers Beach and Boat Trailer car parks;
- Pathways/pedestrian access including separation of Harbour traffic from pedestrians;
- A traffic and parking assessment;
- The identification of and management of native vegetation and weed control;
- The proposed use of each building;
- Locations of all BBQs, toilets and shower facilities;
- The indicative materials and finishes that are considered appropriate for within the harbour precinct; and
- A framework Construction Environmental Management Plan (EMP).

Notice requirements

The draft Development Plan must be placed on exhibition for a period of 6 weeks for community comment prior to its consideration by the Responsible Authority. The Development Plan may be amended by the responsible authority as required. Any significant change to the Development Plan may require further exhibition.

Car parking – Precinct approach

The approach to parking is precinct based. Parking is to be provided at the following four nodes:

- Mother's Beach carpark;
- Fisherman's Co-Op;
- Boat trailer car park; and
- Breakwater Road.

The upgrading of these parking facilities will be used as justification to waive the Clause 52.06 parking requirements generated by individual projects or uses identified in the *Apollo Bay Harbour Development Plan*.

Review of Development Plan

The Development Plan should be reviewed every five years from the date of approval to reflect changing policy settings and community views. The review must include notice as described in the Notice requirements clause above.

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5.0 Development Principles and Development Guidelines

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All buildings and works should be consistent with:

- *Siting and Design Guidelines for Structures on the Victorian Coast*, Victorian Coastal Council (May 1998),
- *Good Design and the Coast, Issue 2003*, by the Office of the Victorian Government Architect,
- *Victorian Coastal Strategy 2014* or subsequent version.
- *Western Victoria Boating Coastal Action Plan* (2010) (Western Coastal Board)
- *Boating CAP Interim Review* (July 2013) (Western Coastal Board)
- *Local Port of Apollo Bay Sand Management Study – Peer review* (June 2010) (Oldfield Consulting)
- *Port of Apollo Bay Future Capacity Study* (July 2006) (Meyrick and Associates)
- *Apollo Bay Harbour Coastal Hazards Vulnerability Assessment* (June 2013) (Water Technology)
- *Port of Apollo Bay Safety and Environment Management Plan 2008-2011* (revised 2013) (Colac Otway Shire)

Development Principles

All buildings and works should be consistent with the following Development Principles:

Integration with Existing Development

- Reinforce the qualities of the working Harbour and facilitate integration with the Apollo Bay town centre in a staged and orderly manner.
- Use landscape screening to minimise the visual impact of buildings from surrounding key vantage points, where appropriate and practicable.
- Integrate and enhance buildings that have service functions (e.g. Fisherman's Co-operative, boat storage and maintenance), and also minimise visual intrusion whilst maximising natural surveillance.
- Develop a physically and visually coherent built form within the coastal landscape setting and character.

Built Form and Siting

- Built form should be of a height which does not intrude upon the southern skyline of the harbour precinct as viewed from north of the harbour and the town centre. The height of development should taper down towards the east, in line with the taper of the landscape.
- A smaller number of multi-use or agglomerated buildings is to be preferred to development of a larger number of single purpose, stand alone, structures.
- Minimise impact on the marine ecosystem and surrounding coastal setting.
- Protect sensitive habitat zones (i.e. native vegetation and marine life) by locating higher intensity development in low environmental value zones.
- Maximise coastal, sea and green open space views and vistas towards and from all commercial and community buildings/activities on site.

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- Minimise car intrusion into sensitive habitat.
- Ensure minimal visual intrusion by required car parking.
- Maximise northerly solar access and use of natural light and ventilation.
- Reduce vandalism by maximising surveillance (e.g. use of boardwalks, pathways and articulated built form).
- Ensure public access to public land is maintained except where the interests of security or safety predominate.
- New buildings will be set back and elevated consistent with the Coastal Hazard Vulnerability Assessment (CHVA).
- Development should not cause any detrimental affects to natural coastal processes.
- Avoid continuous overshadowing of foreshore areas.

Building Fabric

- Encourage retention and re-use of buildings of heritage and community value, including the heritage listed Fisherman's Co-Op building, thereby reducing the embodied energy and waste in creating a new future for the site.
- Adopt best practice energy reduction for new development where possible.

Building Materials

- Utilise, where appropriate, materials from the local area and consistent with local coastal architectural character.
- Encourage subdued palettes.
- Select durable high quality materials with enduring finishes that are appropriate to a coastal environment.
- Use glass to maximise views whilst avoiding glare/reflection (by utilising elements such as eaves overhang, pergolas, louvres and other types of screening system).

Landscape & Public Realm

- Minimise disturbance to indigenous vegetation and use local indigenous species that are resistant to coastal exposure in landscaping where possible and appropriate.
- Maintain and enhance the open views to the ocean, Harbour and hills.
- Reinforce the established landscape character, including:
 - The structure of tall open trees that are well established on the golf course
 - The landscape of open lawns for ease of public use and maintenance
 - Indigenous vegetation providing shelter and stability to the dune systems.
- Replace substandard road pavements and other port infrastructure with new pavements, furniture and other public infrastructure that is consistent, well designed, high quality and complements the coastal environment.

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Water Sensitive Design Measures - Precinct Based

- Ensure stormwater harvesting and reuse from all major roof surfaces.
- Support grey water treatment and recycling for use in landscaping.
- Minimise the area of impervious surface finishes.

Waste Minimisation

- Minimise construction waste through a construction management plan for each stage of the development.
- Provide on-going waste management and sorting areas.

Reduce Carbon Footprint

- Adopt leading practice passive solar energy principles to minimise energy demand of new or extended buildings.
- Include on-site renewable energy sources, e.g. photovoltaic panels and thermal solar water heating where practical and where this does not adversely impact upon visual appearance (e.g. glare).
- Maximise north facing façades as renewable energy collectors.
- Support alternative sustainable transport initiatives, including new walking and cycling networks both through the precinct and at the street interfaces with the site.
- Provide high quality and secure short-term bicycle facilities in all commercial areas and lockers/ showers within the waterfront area.

Social Principles

Cultural and Aesthetic

- Respect the setting and visually complement the surrounding coastal landscape and development by reinforcing and integrating the boating precinct and the town centre.
- Respect, protect and interpret the Aboriginal and European heritage and history of the Precinct.
- Promote local ecological values by potentially building an on-site Cultural Centre and developing nature walks to contain and direct visitor access.
- Provide dedicated areas for both fishing and boating, free of constraints that might arise when located in close proximity to tourism.
- Ensure the design language highlights the public elements of the precinct and promotes active ground floor uses in buildings.

Sustainable Mobility

- Extend walking and bike trails into and through the site.
- Improve shared pathway to provide for safer cycling and walking alternatives linking the Apollo Bay Town Centre and Harbour.
- Integrate new road design with landscape design to ensure provision of safe and convenient paths, street trees to provide shade and bicycle parking facilities.

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- All public and commercial spaces are to be designed to allow access for all abilities.
- Ensure compliance with the Disability Discrimination Act throughout the Precinct.

Economic Principles

Economic Sustainability

- Support the on-going viability of fishing operations and the Co-op.
- Provide for a mix of activities and experiences by developing a lively and diverse precinct that will attract visitors to the region and service community needs.

6.0 Decision guidelines

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Before deciding on a permit application or to approve a Development Plan, in addition to the decision guidelines in Clause 65, the responsible authority must consider as appropriate:

General

- The State Planning Policy Framework and the Local Planning Policy Framework including the Municipal Strategic Statement;
- The purpose of this Schedule;
- The comments of the Department of Environment, Land, Water and Planning (DELWP);
- The effect of the use and development on the operations of the Harbour;
- The consistency of the use and development with the approved Development Plan.

Use

- The effect of the use on the Apollo Bay Shopping Centre.
- Providing and connecting to services and utilities.
- The impact of traffic generated by the proposed use and development and whether any additional traffic management or traffic control works are likely to be required in the area.

Subdivision

- Consistency with the *Apollo Bay Harbour Development Plan*.
- The effect the subdivision will have on the potential of the area to accommodate the uses supported by the *Apollo Bay Harbour Development Plan*.

Building and works

- The impact of coastal processes;
- Wastewater and stormwater disposal and discharge and infrastructure provision;
- The impact upon any areas of pre or post contact heritage;
- The use of lighting to increase security for the precinct; and
- The visual impact of the development when viewed from surrounding land and water.

7.0 Advertising signs

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Advertising sign requirements are at Clause 52.05. This schedule is in category 3.



FORREST MTB STRATEGIC PLAN

Prepared by:



THOMPSON BERRILL LANDSCAPE DESIGN P/L

in association with Simon McArthur & Associates

February 2015

| REVISION | DATE | AMENDMENT |
|----------|------------|---|
| C | 25/02/2015 | Final Report |
| B | 17/10/2014 | Final Draft for review |
| A | 30/06/2014 | Draft – Internal Project Steering Group Review and Comment Only |

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Acknowledgements

The Forrest MTB Strategic Plan project was guided by Colac Otway Shire and a Project Steering Group with contributions from key agencies and stakeholders.

Agency Representatives:

| | |
|-------------------|---|
| Michael Delahunty | Councillor, Colac Otway Shire (COS) |
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| Stewart Anderson | Manager Environment and Community, COS |
| Doug McNeill | Manager, Planning & Building, COS |
| Ian Seuren | Manager Recreation, Arts & Culture, COS |
| Jodie Fincham | Recreation and Events Coordinator, COS |
| Adrian Healey | Otway Tourism, COS |
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| Craig Woodbridge | Land & Fire Officer - Otway District, DELWP |
| Jared Scott | Catchment Coordinator, Barwon Water |
| Gwyn Hatton | Planning Officer, Barwon Water |
| Dave Jenkins | Acting Ranger in Charge - Otway National Park, Parks Victoria |
| Michelle Anstee | Ranger, Parks Victoria |



Stakeholder Representatives:

The Corner Store, Forrest
Forrest Brewery
Forrest MTB Club
Friends of Rail Trail
IMBA
World Trails

Valuable input was received from members of the local community who attended the community 'drop in' workshop in September 2013 and the boarder MTB community, who contributed to the project via an online survey in December 2013.

Executive Summary

Strategy scope and purpose

The Forrest MTB Strategic Plan will guide future planning, provision, design, management and maintenance to assist sustainable future development of the Forrest and Yaugher MTB trail network. The Strategic Plan provides direction for multi agency commitment to deliver sustainable MTB development opportunities in Forrest.

The strategic document identifies key actions and recommendations for the following core outputs:

- Develop a Strategic Plan that has the support of key stakeholders and the local community, as determined through extensive community and stakeholder consultation.
- Identify strategic directions for the planning, provision, development, preservation and management of the Forrest MTB trails networks and supporting infrastructure.
- Identify funding opportunities and revenue streams for ongoing management and maintenance of the trails, which supports the growth of MTB activity in Forrest.
- Consideration of cultural heritage and environmental values
- Identify development opportunities to strengthen the MTB offer in Forrest.
- Identify additional recreational opportunities for improved social health and wellbeing for a range of existing and future MTB users.

Key facts and strengths of MTB activity in Forrest

Key facts include:

- Forrest is located approximately 30kms south east of Colac and 36kms from Apollo Bay on one of the major tourist routes connecting the Princes Highway with the Great Ocean Road.
- Following the 2003 State Government 'A New Future for the Otways Tourism Initiative' Forrest has experienced an increased focus on tourism accommodation, product and unique MTB experience.
- The Forrest and Yaugher trail systems offer over 66kms of MTB trails across 16 different trails.
- There are two main trail heads with car parks, signs, toilets and shelter.
- A variety of MTB trail types are provided for, from beginner to advanced.
- DELWP has responsibility for maintenance, signage and risk management of the MTB trails.
- DELWP currently convenes the Forrest Mountain Bike Trails Committee, and coordinates operational matters e.g. maintenance, erosion control, alignment and possible new trails.
- DELWP work closely with the Forrest Mountain Bike and Cycling Club and organises the Forrest Mountain Bike Club Volunteer work program.

Key strengths include:

- Township welcomes and celebrates MTB activities.
- MTB trails are located within and in close proximity to the Forrest Township providing a 'ride from your door step to the start of the trail' experience.
- Extensive existing MTB trail network.
- The MTB trails have been designed around the village of Forrest and these trails showcase the natural beauty of the Otway Ranges.
- Visitors come to Forrest for its township character and not just MTB activities.

- Forrest is in close proximity to the coast offering a point of difference for people to visit.
- There has been a significant increase in commercial and economic development in the town making Forrest a key inland destination point within the Colac Otway region.
- Forrest town and locals proudly display its timber mining town history.
- Forrest MTB trails are home to a number of annual major cycling events.

Key issues associated with MTB in Forrest

Key issues include:

- Lack of diverse trails in proximity to Forrest.
- Lack of MTB Skills Park.
- Minimal facilities/opportunities for MTB beginners and families in close proximity to town.
- Limited DELWP funds for ongoing management and maintenance of MTB trails.
- Unsafe Colac-Forrest Road crossing (link between Tiger Rail Trail and Boundary Road to Yaughar Trail Head).
- Limited food and beverage options in Forrest during off peak seasons.
- Events held in Forrest can negatively impact on surrounding local residents, e.g. road closures.
- Inadequate signage of trails for users.
- MTB events impact negatively on the MTB trail corridors with limited agency/financial support for DELWP to maintain and manage the trails post events.
- Limited capacity of existing public toilets during events; town not sewered.
- No marketing strategy to identify areas for maximum growth and focus limited resources.

Key recommendations include:

- The formation on an Advisory Group with key agency representatives (from Colac Otway Shire, DELWP, Barwon Water, Parks Victoria and potential stakeholder representatives), to meet, discuss and provide direction for ongoing maintenance, management and funding options for the MTB trail network.
- Establish a new 'blue' trail '*Barwon Flow*' within the Forrest MTB Trail Network, providing a new loop trail suitable for intermediate rider ability, in close proximity to the Forrest Township.
- Undertake MTB trail upgrade/enhancement works to existing trails within the Forrest and Yaughar MTB trail network including; Rollercoaster, Red Carpet and Follow the Dog to provide more trail diversity.
- Establish a MTB skills development park adjacent to the Forrest Trail Head site.
- Undertake landscape improvement works at the Forrest Trail Head and improve overall trail entry experience, with enhanced planting, key visitor information, signage and future wash down facility.
- Undertake detailed design for a tunnel underpass (below Colac-Forrest Road) and realignment of the trail link from the Tiger Rail Trail to the Yaughar Trail Head.
- Undertake detailed design for a new off road trail (west of Boundary Road) '*Boundary Rider*' from the new tunnel underpass to the Yaughar Trail Head.
- Change the use of Vista Trail to dedicated equestrian use.
- Upgrade trail signage, including directional, wayfinding and interpretation, across the Forrest and Yaughar MTB trail network.
- Retain and protect existing areas of Native Vegetation and undertake revegetation works in targeted areas.

1. Strategic Vision

The Forrest MTB Strategic Plan has been prepared to provide strategic guidance and recommendations to present a sustainable future for Forrest's Mountain Bike Trails.

The project has been coordinated by Colac Otway Shire and guided by a Project Working Group (PWG) comprising representatives from key Council departments, Department of Environment, Land, Water and Planning (DELWP), Barwon Water (BW) and key local Stakeholders.

The overall strategic vision is to develop a Strategic Plan that guides and secures an economically sustainable future for the Forrest MTB industry, with the support from agencies, stakeholders and the local community; representing a balance between community and stakeholder views, State Government Policy, commercial opportunities and environmental values.

The development of the Strategic Plan for Forrest MTB Trails will provide an implementable framework to guide future actions and authority responsibilities. The plan will complement the MTB activities and culture as it continues to grow whilst retaining the essential attributes and elements which make visiting Forrest and riding the MTB trails a positive visitor experience.



1.1 Study Area and Context

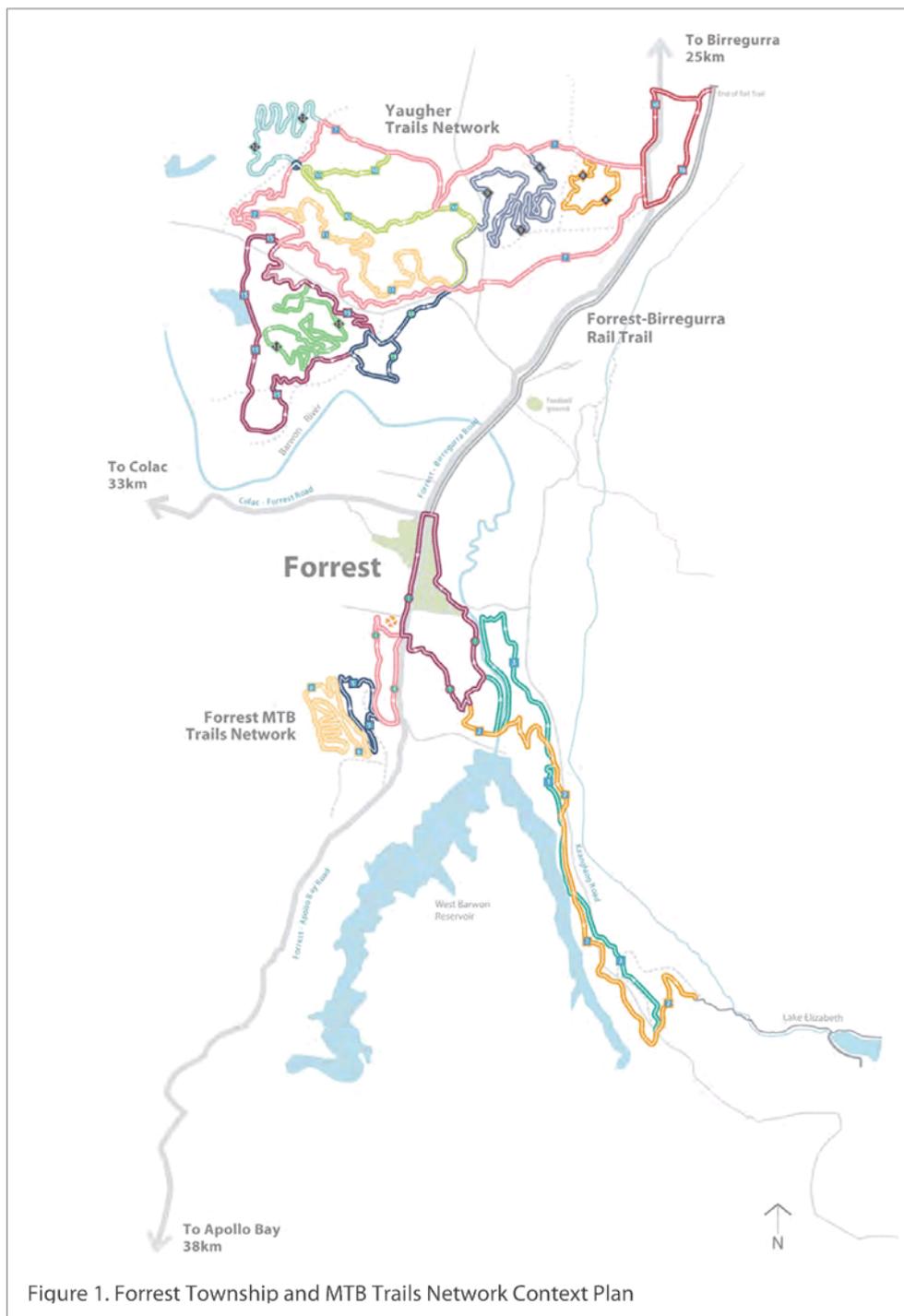
Forrest is a small rural township located in the Otway Ranges of Victoria approximately 160km south-west from Melbourne. The Forrest Mountain Bike Trails are located on Crown Land to the south and north of the Forrest Township, with the trails network crossing a number of land tenures including the Otway Forrest Park, Great Otway Forest Park and land managed by Barwon Water. The West Barwon River flows to the east of the township and is 1km north of the West Barwon Reservoir, which services Geelong.

Forrest is located approximately 30kms south east of Colac and 36kms from Apollo Bay on one of the major tourist routes connecting the Princes Highway with the Great Ocean Road, which is an iconic Australian tourism destination. It has a population of approximately 160 and is undergoing economic and social transformation resulting from the reduction in logging and agricultural activities and the increasing visitation to the Great Otway National Park, increasing focus on tourism and growth of the MTB industry.

Factors in Forrest's favor were the designation of the Forrest to Apollo Bay Road as a priority north-south Great Ocean Road access from the Princes Highway and the influence of 'Tree

Change Lifestyle' newcomers to the area. An increasing focus on tourism accommodation, product development and visitor experiences was becoming evident and the 2003 State Government, "A New Future for the Otways" Tourism Initiative proposed to further stimulate local economies within the Otway Ranges through the development of tourism. This was to offset the losses in employment and local economy resulting from the cessation of commercial hardwood harvesting. The Forrest Mountain Bike Trails are the product of this strategy.

Forrest Mountain Bike trails are a network of 16 trails that total approximately 66km in length. There are two trail heads with car parks, signs, toilets and shelter facilities. The trails also link to the recently completed Tiger Rail Trail Stage 1.



1.2 Summary of project works

1.2.1 Site Analysis

Comprehensive site analysis was undertaken to review existing on site conditions of the MTB Trails, the two MTB trail heads at Forrest and Yaughter, links and connections to town via trails and road network. Assessment was undertaken of the visitor experiences offered within the Forrest Township with consideration of the surrounding site context, site history, character, signage, food and beverage provisions and accommodation options.

The township of Forrest has a 'village' type feel and provides an excellent destination for visitors to stay and explore the MTB trails directly from the town.

Refer to Appendix A - Detailed Site Analysis Summary Report.

1.2.2 Consultation

The development of the Strategic Plan has been informed by detailed site visits, strategic workshops and meetings held with agency and stakeholder representatives, and technical workshops with specialist MTB industry representatives. Important input was gained from the local community via an informal 'drop in' session, and with the broader MTB community via an online survey.

Consultation and engagement with the local Forrest residential community, representatives of local groups, stakeholders and agencies was an important component of the project to ensure the strategy was well informed and reflected local interests.

Agency workshops and meetings were held with representatives from COS, DELWP, BW, PV and Tourism Victoria to discuss and identify strategic project goals and values with a focus on ongoing management and maintenance of the trail network.

Stakeholder workshops and meetings were held with representatives from COS, Forrest MTB Club, Tiger Rail Trail Association, Otway Hinterland Tourism Association/Forrest Brewery, and The Corner Store. Representatives actively contributed to discussions providing valuable insight into the community's historic and present involvement with the development of MTB facility, identifying key issues and opportunities for sustaining MTB activities in Forrest and improving visitation to Forrest to continue to drive the town's economy.

Specialist members from the MTB industry participated in a MTB strategic workshop, to evaluate, discuss and identify overall design principals, technical aspects and key considerations to enhance the existing MTB facility and establish Forrest as a premier MTB destination in Australia. The workshop provided invaluable insights and presented an opportunity to gain expert industry knowledge and information pertaining to the success of MTB industry in Forrest and on how to provide the project with the best technical strategic direction and vision from Australia's MTB experts.

To gain additional broader consultation from the MTB community, an online Forrest MTB survey was developed, and the survey was promoted via Colac Otway Shire website, Ride Forrest website, and relevant Facebook pages. The survey was positively received and provided an additional mechanism to gain additional broader information from MTB riders about what they like about Forrest and what could be improved (e.g. "more diverse trails closer to town"), to complement and enhance the vision and recommendations for the Strategic Plan.

The consultation phase was fundamental to the development of the Issues and Opportunities Paper and underpins the direction of the Strategic Plan. Refer Appendix B - Forrest Issues and Opportunities Paper.

1.2.3 Benchmarking Analysis

Benchmarking analysis was undertaken by Simon McArthur & Associates, in conjunction with TBLD. This work assessed commercially run MTB destinations including Thredbo MTB Trail and Whistler MTB Park (Canada), and non-commercial run MTB/cycle destinations including Ride Rotorua (New Zealand), You Yangs, Old Beechy Rail trail, Murray to Mountain, Lysterfield Park, and Lilydale - Warburton Trail. These destinations were benchmarked in comparison to Forrest in regards to the market, position, product, cost and promotion. Refer Appendix B- Forrest Issues and Opportunities Paper - Attachment 5

1.2.4 Economic and Tourism Benefits for Forrest

The online survey results indicated that 99% of respondents would encourage their MTB friends to visit Forrest and ride the MTB Trails. The importance of the Forrest Township to the MTB experience is highly regarded as Very important (35%) and Important (42%). This overwhelmingly positive response identifies that the Forrest MTB trails and Forrest Township are linked as one overall visitor experience, and that Forrest is recognised as a destination in itself and an interesting place to visit.

Continued sustainable development of the Forrest Township with the MTB experience as a core destination product provides customers and visitors with an enhanced experience, continually improving range of accommodation options, improved MTB experiences and ease of connectivity to the MTB trails, all benefitting the local economy. Promotion of the township and MTB trails as one integrated experience is also an important aspect in driving new and repeat visitation.

1.2.5 Barwon South West Regional Trails Master Plan, 2009

The Forrest Strategic Plan supports the vision of the Barwon South West (BSW) Master Plan where it states: 'the network of recreational trails in the south west Victorian coast, forests and hinterland will become recognised worldwide as offering social, environmental and economic benefits'.

The Forrest Strategic Plan addressed similar issues as identified including:

- The need for enhanced governance and policy
- The need for coordinated planning, development and ongoing management of trails
- Provide greater emphasis on the development of trail infrastructure to support the activity
- Enhance the user's experience Enhance opportunities for marketing and promotion
- Lack of funding required establishing and maintaining a quality trail network.

The BSW Master Plan identified Forrest as a key destination MTB hub as MTB hubs are to be strategically and equitably spaced across the Barwon South West region, each providing a range of riding experiences, in a variety of settings.

The key recommendations identified in the BSW Master Plan (as part of a 10 year action plan) are in-line with the recommendations identified in the Forrest Strategic Plan, including:

- Clear leadership, policy and appropriate governance structure for the trails.

- Experience-led trail development – including investigation of opportunities for the development of new shared use trails and/or the upgrade to shared use trails where appropriate.
- Expand opportunities for use of trails by people with varying abilities.
- Enhance marketing and promotion of the area and MTB activity through effective and coordinated websites, marketing plans, etc.
- Explore options for increased funding and resources for trail development, management and maintenance.

1.3 Implementation of the Plan

The Forrest MTB Strategy presents visionary, well-resolved, functional and implementable recommendations and actions to create an economically sustainable and exciting future for the MTB industry in Forrest.

The Strategy seeks to build on the substantial strengths of the MTB trails, facilities and associated support industries including MTB hire, accommodation, and food and beverage outlets.

Primarily, the strength of Forrest as a MTB destination lies in the diversification and strengthening of the expansive network of high quality MTB trails that directly adjoin the township of Forrest, providing exciting, year round MTB riding opportunities and experiences.

Secondarily, the commitment, drive and enthusiasm amongst the agencies, local stakeholders and the community, towards the success of the MTB industry is exceptional and extremely valuable.

The recommendations and actions outlined for implementation in the strategy focus on these strengths, defining and building on this clearly differentiated and quality MTB destination product in the expanding MTB market.

In a few short years the people of Forrest, with the support of various agencies, have built an enviable base for the growth of this unique MTB facility that is driving the revitalisation and transformation of the small country town of Forrest into a boutique country sporting visitor destination.

2. Strategic Recommendations and Implementation Plan

2.1 Strategic Project Objectives

The following is a list of strategic objectives for the project;

- Identify strategic and proactive opportunities for diversification and strengthening of the Forrest MTB trail system.
- Provide a mechanism for Forrest and the MTB community to continue to provide and develop sustainable growth of the Forrest township
- Retain the Forrest identity, character, and point of difference from other MTB destinations
- Celebrate Forrest as MTB destination #1 in Australia
- Promote and market the Township of Forrest as a key strength in conjunction with the diversity of MTB trails and experiences on offer
- Investigate broader recreational opportunities to compliment MTB activities
- Promote Forrest and market Forrest to other MTB trail users such as schools, ecological tours, commercial tour operators, etc
- Protect and enhance areas of indigenous and native vegetation.
- Identify a cohesive and feasible management structure to allow the Forrest MTB trails to grow and cater for future and repeat visitation.

2.2 Strategic Plan

The following table outlines the issues assessed and reviewed in detail. The strategic recommendations and actions that form the base of the Strategic Plan provide direction for future planning, provision, development, preservation and management of the Forrest Mountain Bike Trails and supporting infrastructure based on clear evidence of stakeholder and community demand and sustainable asset management.

The authorities responsible for addressing the strategic recommendations and actions have been acknowledged, and priorities have been identified. A technical workshop was held to determine priority trail opportunities.

Priority Key:

Very High: 0 - 1year

High: 1 - 2 years

Medium: 2-5 years

Low: 5 - 10 years

| No. | Issue | Strategic Recommendation / Action | Responsible Authority | Priority |
|---------|---|--|-----------------------|------------|
| 2.2.1.1 | <p>Forrest Township</p> <p>Forrest Character and History</p> <p>The township of Forrest has retained its old Victorian country town charm with its tall mature gum trees set against the pastoral setting of the rolling green hills.</p> <p>The main street focuses on core services such as the schools, church, pub, general store, etc, and the wide open quite and shady streets are welcoming without urban clutter. There is a relaxed village community feel combined with evidence of the new residents bringing energy and business opportunities to revitalise the town, while retaining a sense of pleasantness within the local community.</p> | <ul style="list-style-type: none"> Retain and protect the visual clues of the old Victorian country town character. Ensure new developments (incl buildings, civil works, public infrastructure, streetscape works, etc) do not detract from the country town theme and activities. Ensure the township is not genericised or gentrified and maintains its genuine 'Forrest' town character, as a point of difference from the other Colac Otway region small townships. Retain the wide relaxed feel of the main street, and ensure the town does not become over built or urbanised. Develop infrastructure that responds to and complements the town character e.g. bike racks, seating, signage, etc. | Colac Otway Shire | Med - High |

| No. | Issue | Strategic Recommendation / Action | Responsible Authority | Priority |
|---------|--|---|-----------------------|------------|
| 2.2.1.2 | <p>Remnants of the timber town working aesthetic are still visible and valued by (the community) as the foundations of the town. Evidence of old forestry machinery, vehicles, sheds, buildings, equipment etc are scattered amongst the streetscape and treasured within both private residences and a part of public buildings.</p> <p>Results of the online survey indicate that the element that MTB visitors like most about the Forrest Township is its 'rural township atmosphere' followed by its 'low key' culture.</p> <p>Pathway network</p> <p>There is a number of existing off road paths within the township, including:</p> <ul style="list-style-type: none"> The Tiger Rail Trail which provides an off road shared path link, south of Birregurra-Forrest Road, which extends from Station Street to Boundary Road. A sealed asphalt path (approx 1-1.5m wide) extends from Colac-Forrest Road to Turner Drive (north of the main street) providing a link to the Primary School and Forrest Hall. A wide gravel path (approx 2.5-3.5m wide) extends south from Henry Street and currently terminates approximately half way towards the General Store. Barwon Street is an unsealed local access road located parallel between the two main roads of the town, which provides an on-road path link via an under utilised roadway. | <ul style="list-style-type: none"> Establish a separate shared trail on the east of the main street (via upgrading and widening the recent path works to min 2.0m wide) to accommodate for the range of expected users including MTB riders, pedestrians, dog walking, children riding to school, etc. Provide a safe crossing point for users of the shared trail between the north and south of the main street. Promote Barwon Street as an additional cyclist/walker path through town, as little vehicle traffic utilises this road. Retain as unsealed to retain low speed and rural character of township. Seek opportunity for multi-use track design to accommodate MTB trails suitable for people with all abilities, e.g. in the redesign of the Roller Coaster Trail. | Colac Otway Shire | Med - High |

| No. | Issue | Strategic Recommendation / Action | Responsible Authority | Priority |
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| 2.2.1.3 | <ul style="list-style-type: none"> Other pathways within the township provide shortcuts through the town e.g. the path link opposite Hennigan Crescent west to Birregurra-Forrest Road. <p>Pathway upgrades have been established recently along the main street.</p> <p>Signage
The main Colac Otway Shire 'Forrest' township sign clearly identifies the entrance to the township and notes its MTB role.
At the intersection of Colac-Forrest Road and Birregurra -Forrest Road there is an excessive overload of signage and information for a visitor to take in. Rationalisation and consolidation is essential to improve visitor amenity and provide effective information.</p> | <ul style="list-style-type: none"> Review and consolidate existing chaotic and duplicated signage. Develop and design key visitor directional and historic signage at a key public open space site and destination, to be determined in consultation with the community. Undertake design upgrade to township interpretation signage. Signage form and materials to be developed to reflect the genuine country character and historic qualities of the township. | Colac Otway Shire
Otways Tourism
Vic Roads | Low -
Med |
| 2.2.1.4 | <p>Interpretation signage is dotted around the township. Green colored recycled plastic posts and boards display images and stories of the previous lifestyle and historic development of the Forrest Township. This material is aesthetically disconnected from the 'timber town' history, character and detracts from the towns' aesthetic character.</p> <p>Other sporting/recreational facilities
Other than the primary activity of Mountain Biking, visitors at Forrest can enjoy bushwalking, horse riding, and shared walking path to Lake Elizabeth, active sports and the oval and to the north of town.</p> | <ul style="list-style-type: none"> Highlight and promote other local recreational opportunities via public information/signage at a key public open space site. This will allow visitors who have not pre-planned their activities before arriving at Forrest to engage in other recreational activities offered in the area. | Colac Otway Shire
Otways Tourism
Parks Victoria
RDV | Medium |

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| 2.2.1.5 | <p>Forrest is unsewered with a predominant septic system. Unsecured 'dry' toilets are provided at the Forrest and Yaughner Trail Heads and intended for MTB rider use only.</p> <p>The Yaughner Trail Head toilet is regarded as sufficient, given its relatively isolated location off the main road.</p> <p>The Forrest Trail Head toilet is over used, acting as a surrogate public toilet used by tourists en route through Forrest. The public toilets on Station Street are also under capacity during events.</p> | <ul style="list-style-type: none"> Agencies to review toilet capacity and location for MTB use in light of longer term strategic opportunities for Forrest. | Colac Orway Shire
DELWP | Med - High |
| 2.2.2 | <p>Forrest MTB trails network</p> | | | |
| 2.2.2.1 | <p>The Forrest MTB trails network provides a range of MTB ride options only a short distance from the township, with 'ride in ride out' convenience. These trails provide visitors with the opportunity to ride and experience the spectacular natural beauty of the Oway Ranges, passing significant eucalypt forests, dry heathland scrub and dense spectacular fern gullies. These trails cater for a variety of MTB riders from family friendly, to beginner, intermediate with limited variety of trails for experienced riders.</p> | <ul style="list-style-type: none"> Ensure trail grading signage (e.g. easiest to extremely difficult) is appropriately identified at all entry points to each trail, to ensure riders are informed of the appropriate trail grading before commencing on a section of trail. Install additional sign bollards to clearly denote trail entries, routes and exits. Provide new and modified trails for implementation as identified in the Technical Workshop and site visit. Refer <i>Appendix C</i>. Construction of new trails/trail upgrades, ongoing maintenance, management and funding applications for the trail network is to be guided and driven with the formation of an 'Advisory Group'. Refer <i>Recommendation 2.2.13</i>. Agencies to review and undertake assessment of cultural heritage and flora and fauna impacts pertaining to new MTB works. | Colac Orway Shire
DELWP
Barwon Water | High |

| No. | Issue | Strategic Recommendation / Action | Responsible Authority | Priority |
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| 2.2.2.2 | Facilities at the Forrest Trail Head (located just south of the township) include a sealed car park, a large picnic shelter, picnic tables, low seating wall and a standard DELWP (not flush) public toilet.

No water or MTB wash down facilities are currently available. | <ul style="list-style-type: none"> Undertake detailed design and costing for upgrade works at the Forrest MTB Trail Head, to provide additional soft landscaping to carpark area, and provide key visitor and information signage, and improve overall trail entry experience. Following detailed site assessment, community consultation and discussion with agencies and stakeholders, it is recommended that the Forrest Trail Head is the most appropriate site for a drinking fountain and a MTB wash down facility. Detailed design to be informed by discussions with DELWP and the Advisory Group. The wash down facility was identified in the results of the online survey, as a critical improvement to be made to the Forrest MTB offer to encourage the user to stay longer. Following detailed site assessment and discussion with agencies and stakeholders, it is recommended that a Skills Park be established at the Forrest Trail Head site. Undertake detailed design and development of a Skills Park (for beginners to intermediate riders). Refer <i>Appendix C</i>. | Colac Otway Shire DELWP | High |
| 2.2.3 | Yaughter MTB trails network | | | |
| 2.2.3.1 | The Yaughter MTB trails network provides a diverse and exciting selection of well laid out MTB loops and trail routes, set within a bushland landscape. These trails cater for a variety of MTB riders from family friendly, to beginner, intermediate and up to experienced riders to undertake technical and challenging sections of trails. | <ul style="list-style-type: none"> Ensure trail grading signage (e.g. easiest to extremely difficult) is appropriately identified at all entry points to each trail, to ensure riders are informed of the trail grading before commencing on a section of trail. Install additional entry and directional sign bollards to clearly denote trail entries and nodes Provide new and modified trails for implementation as identified in the Technical Workshop and site visit. Refer | Colac Otway Shire DELWP | Med - High |

| No. | Issue | Strategic Recommendation / Action | Responsible Authority | Priority |
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| 2.2.3.2 | The Yaughner Trail Head is signed along Birregurra -Forrest Road and is located off Boundary Road. The car park is signed using a standard DELWP location and directional sign. Facilities at the trail head include an unsealed car park area, standard DELWP (drop) public toilet and a few picnic tables.

No water or MTB wash down facilities are currently available. | <ul style="list-style-type: none"> Undertake detailed design and costing for upgrade works at the Yaughner MTB Trail Head to provide additional landscaping to carpark area, additional picnic tables, and provide key visitor and information signage, and improve trail entry experience. | Colac Otway Shire
DELWP | Med -
High |
| 2.2.4 | Tunnel Underpass | | | |
| 2.2.4.1 | Tunnel Underpass

The crossing of Birregurra-Forrest Road (link to Boundary Road and access the Yaughner trail network), is at the apex of a curve and the road is currently designated at 100kms/hr. This presents significant safety crossing challenges and dangerous risks for both MTB cyclists and pedestrians. | <ul style="list-style-type: none"> Undertake detailed design for the tunnel underpass (south of Boundary Road) and realignment of the trail. Refer <i>Appendix C for tunnel underpass Indicative location and Appendix D for opinion of probable cost.</i> Completion of the tunnel underpass may reduce the need for road closures during events along Boundary Road. | Colac Otway Shire
VicRoads | Very high |
| 2.2.4.2 | Boundary Road alignment
MTB riders travelling from Forrest to the | <ul style="list-style-type: none"> Undertake detailed trail design for the new off road MTB track alignment (to the west of Boundary Road) to | Colac Otway Shire
DELWP | High |

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| 2.2.5 | <p>Yaugher MTB trail network, cyclists must travel along Boundary Road (an unsealed steep roadway shared with vehicle traffic).</p> <p>Future trails</p> | <p>provide safe off road access (where possible) from the new tunnel to the Yaugher Trail Head. Refer Appendix C for tunnel underpass indicative location and Appendix D for opinion of probable cost of new trails.</p> | | |
| 2.2.5.1 | <p>The extent of the current MTB trails is currently not used to capacity (especially within the Yaugher trails network). Sections of trails continually need to be upgraded, and sections rejuvenated to enhance the rider experience and to maintain the status and attraction of Forrest as a key MTB destination.</p> <p>Market research indicates that successful MTB destinations offer the majority of their product in the intermediate to advanced skill markets, but manage to attract wider markets by having a minority of trails set aside for beginners / skill development / low fitness and / or deviations from the main route that present some interest and respite before returning to the challenging main route.</p> <p>Results of the online survey and discussions with MTB specialists indicated that the Forrest MTB system is to build on its existing strengths, and that Forrest requires more 'trails that are different' which are 'in close proximity to the township'.</p> | <ul style="list-style-type: none"> • Provide new or modified MTB trails/upgrades to existing trails at Forrest and Yaugher, for implementation as identified in the Technical Workshop and site visit. Refer Appendix C and Appendix D for opinion of probable cost. • Construction of new trails/trail upgrades, ongoing maintenance, management and funding applications for the trail network is to be guided and driven with the formation of an 'Advisory Group'. Refer Recommendation 2.2.13. | <p>Colac Otway Shire
DELWP
Barwon Water
MTB stakeholder community
Parks Victoria</p> | <p>Med - High</p> |

| No. | Issue | Strategic Recommendation / Action | Responsible Authority | Priority |
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| 2.2.6 | Accommodation | | | |
| 2.2.6.1 | There is insufficient accommodation during peak holiday periods and events. | <ul style="list-style-type: none"> Council to continue to actively support local MTB friendly accommodation development opportunities. While awaiting expected development of new boutique accommodation, a number of smaller accommodation places could be refurbished and updated to fit unmet target market demand. Investigate the feasibility for the development of an ecolodge within 20 minutes drive of Forrest to meet demand for complementary eco accommodation. | Otway Tourism, DELWP, Parks Victoria, Tourism Victoria, Colac Otway Shire | Med - Med |
| 2.2.7 | Events | | | |
| 2.2.7.1 | There are a number of recreational, MTB, food and wine and tourism events held within the township of Forrest. A number of other events are held within the Colac Otway Region across the calendar year.

The events held in Forrest are generally perceived as well managed and provide positive social and economic impacts for the town and surrounding Colac Otway region | <ul style="list-style-type: none"> Continue to work with the existing event operators to continue to improve the management of the events and further enhance the visitor (and local community) experience during the event. Actively seek financial contributions from event organisers commensurate with the repairs required, to assist in the maintenance and management of the MTB trails following events. Expand the range of events held in Forrest to provide additional tourism drivers for the town. | Colac Otway Shire
Private Event Sector
DELWP
Barwon Water | Ongoing |
| 2.2.8 | Competitive position | | | |
| 2.2.8.1 | Competitor support services
Market research indicates that the support services and infrastructure (carparks, accommodation, food and beverage, hire and retail) is typically based close to the trail starting / finishing point, making the experience simple. | <ul style="list-style-type: none"> Colac Otway Shire to determine an optimum size at which the destination can operate at any one time, and design event-sized supporting infrastructure and services to this. Undertake the detailed design of a skills park (as noted in Recommendation 2.2.2). This facility could be | Colac Otway Shire
Otway Tourism
RDV
DELWP | Medium |

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| | convenient and efficient.

In addition, the scale of these facilities make it easy for an event to service most if not all of the participants 'on site' and close together – further enhancing the atmosphere and effectiveness. | designed to include rest / picnic areas for accompanying friends and family to watch and photograph them in a picturesque and fun setting. Refer Appendix C for Skills Park proposed location and Appendix D for opinion of probable cost. | | |
| 2.2.9 | Marketing/tourism products | | | |
| 2.2.9.1 | Competitor product

The alpine mountain resorts have several strategic advantages over Forrest as a mountain bike destination, and they have subsequently built highly competitive propositions.

There is significant diversity in product (including grades, technical skills and landscape) in these locations. These destinations don't just provide lots of trails; they provide lots of differentiated trails.

There is also much value adding of the base trail product, including guided tours, camps, events and ancillary services, including bike hire of different types of bikes. | <ul style="list-style-type: none"> Forrest MTB marketing should create differentiation and avoid unnecessary and disadvantageous duplication of MTB experiences. Promote Forrest MTB activities and promote the Forrest Township as a destination to visit and stay. Provide resources and support for Forrest driven web based MTB product promotion. | Colac Otway Shire
Otway Tourism
Private Sector | Medium |
| 2.2.9.2 | Competitor marketing

Marketing undertaken by alpine resort mountain bike destination managers is significantly superior to most of the non-commercial sites, evidenced by superior imagery of the experience and landscape, use of well produced videos and effective use of social media and events to drive brand awareness.

Logistical information is often also superior, such | <ul style="list-style-type: none"> Position the Forrest mountain biking destination to: <ul style="list-style-type: none"> – lead with its signature trails and place supporting product behind this, in less detail – include accommodation and food and beverage that matches the needs of the target market supported by web based information. Promote the genuine country ambience of the Forrest township experience as a destination value add for riders and visitors: | Colac Otway Shire
Otway Tourism
Private Sector | High |

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| 2.2.9.3 | <p>as the way trails are presented on interactive maps and the way they are profiled so the customer could easily compare and choose. These destinations do not shirk from pricing and present comprehensive pricing information with competition profiled and opportunities for discounts available.</p> | <ul style="list-style-type: none"> - recognise and promote MTB as an important activity - social and health benefits - promote MTB as an all ages and multi skill activity for visitors. Advisory Group to pursue opportunities for creating user funding streams, e.g. donations from trail users to contribute to the maintenance of the trails and upgrade of MTB visitor infrastructure at trail heads. Within the trail network identify the lead experience that brands the overall destination and is used to generate the main awareness and motivation to visit. Provide resources to develop a destination marketing strategy for mountain biking in the area. | Colac Otway Shire
Otway Tourism | High |
| 2.2.9.4 | <p>Promotion. The marketing of Forrest as a mountain biking destination is largely done through the website (http://www.rideforrest.com.au) that blends the township of Forrest with the bike riding opportunities.</p> <p>The website includes some You Tube videos showing intermediate bikers on some sections of the trail doing small jumps and fast turns, a short a description for each route and its level of difficulty, whether the trails are open or closed, and a map of the region trails that can be downloaded.</p> <p>There is no social media used on the main website</p> | <ul style="list-style-type: none"> Support and encourage sustainable/manageable MTB events in Forrest. Provide resources to generate a set of quality images and videos with soundtracks for the most competitive mountain bike experiences, and allocate responsibility to someone to regularly refresh them. Ensure Forrest and the MTB trails are identified as a key MTB visitor destination in future government trails website. | Colac Otway Shire
DELWP
Otway Tourism | Medium |
| | <p>Target markets Most MTB destinations are targeting intermediate to advanced riders less than 35 years of age, almost always with pictures</p> | <ul style="list-style-type: none"> Grow visitation by designing and enhancing trails to provide A and B lines for a diversity of skills sets. Trails closer to the township of Forrest are designed with | | |

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| | <p>of male riders.</p> <p>Skill development trails are often under-marketed or non-existent.</p> | <p>lesser difficulty while as you travel further out the trails will increase in difficulty. <i>Refer Appendix C for new trail locations.</i></p> <ul style="list-style-type: none"> The key target market based on survey feedback, consultation and gap analysis is women and families that may be new to the mountain biking experience. Opportunity to improve skill set via skill development clinics and general skill development via diversified skill development trails and a new skills park area. Growth in this target market will enhance the economic outcome for the township and increase spending and visitation by diversifying the attraction and experience that Forrest has to offer. | | |
| 2.2.10 | <p>Future Economic Growth and Township development</p> | | | |
| 2.2.10.1 | <p>Employment As outlined in the Tourism Economic Analysis 2012 Report, The Great Ocean Road region supports an estimated 126,404 jobs, of which 8,639 jobs are in the Colac Otway Shire.</p> <p>Of the 8,639 people employed in the Colac Otway Shire, it is estimated that demand by visitors for goods and services supports 643 jobs (highlighted in yellow above). That is, the Tourism sector represents 7.4 % of total employment in the Colac Otway Shire; compared to 5.6 % for the broader Great Ocean Road region. The major tourism related activities in Colac Otway include accommodation & food services (455 jobs), retail trade (86 jobs) and arts & recreation services (35 jobs).</p> | <ul style="list-style-type: none"> Continue to provide support for commercial activities that support this emerging trend, including MTB industry, bed and breakfast accommodation, food and beverage outlets, the microbrewery and other comparable industries for employment opportunities in the town. | Colac Otway Shire Private Sector | Ongoing |

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| | <p>Since the cessation of the timber industry in the Otways, a new focus for Forrest has emerged in terms of lifestyle, recreation and eco-tourism opportunities. This includes mountain biking, horse riding, hiking, camping and other nature-based recreational activities. The expansion of these recreational elements will be critical to the ongoing economic and employment sustainability of the town.</p> | | | |
| 2.2.10.2 | <p>Annual Economic Output As outlined in the Tourism Economic Analysis 2012 Report, the Tourism sector in Colac Otway Shire is estimated to generate \$104.7 Million in annual economic output. The industries contributing most to the Colac Otway Shire's Tourism sector are those that are the greatest beneficiaries of expenditure by visitors.</p> <p>On average, for each dollar spent by a visitor to Colac Otway Shire \$0.61 is for accommodation & food services, \$0.09 for ownership of dwellings, \$0.08 for retail trade, and \$0.08 for transport.</p> <p>For every dollar of direct expenditure by visitors to Colac Otway Shire, the broader local economy is estimated to benefit by a further \$0.65 once flow-on industrial and consumption effects are taken into consideration. This can also be expressed as a tourism output multiplier of 1.650.</p> | <p>The region's economy is an interconnected system where direct benefits to one tourism sector industry will flow on to other sectors to varying degrees.</p> <ul style="list-style-type: none"> Continue to support, promote and market the MTB industry in Forrest with activities and events, and accommodation and food and beverage development options compatible with the Forrest Township. | Colac Otway Shire
DELWP
Otways Tourism | Ongoing |

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| 2.2.10.3 | <p>Commercial Structure Overall Forrest's commercial structure is limited by the lack of banking facilities, secondary school, petrol station (the closest available fuel is 7km away in Barwon Downs) and limited cell phone coverage. Supermarkets are located in Winchelsea, Colac and Apollo Bay, although the general store does stock some limited grocery items.</p> <p>As identified in the Forrest Structure Plan, in terms of commercial opportunities, given the success of the local Forrest microbrewery and Forrest Corner Store, there are other opportunities within Forrest and the local area for like developments to establish. This could lead to enhanced tourist focus for Forrest.</p> | <p>Overall commercial and industrial infrastructure should be scaled to reflect the population and needs of the town and tourist population.</p> <ul style="list-style-type: none"> The upgrade of public facilities within the township e.g. sewer connection, increased public toilet amenities, etc would significantly improve the visitor's experience whilst visiting Forrest, especially during long weekends and during events, when the town is on show. | Private Sector
Colac Otway Shire | Med - High |
| 2.2.11 | <p>Social/healthy wellbeing</p> | | | |
| 2.2.11.1 | <p>The G21 Region Health and Wellbeing Plan (2013-2017) identifies key priorities for actions to improve the health and wellbeing of people across the G21 region.</p> <p>The Plan was developed concurrently with the five local governments, City of Greater Geelong, Colac Otway, Surf Coast, Golden Plains and Queenscliffe, within the region.</p> | <p>The Local Action Plan for the Colac Otway Shire identifies a list of actions that are drawn from the Council Plan 2013/17 and/or Public Health and Wellbeing Plan 2013/17 that have been identified for Council and that also align with the region wide priorities. In summary these relevant to Forrest MTB activities include:</p> <ul style="list-style-type: none"> Provide supportive environments and services to encourage a more active lifestyle. Develop a strategy that will specifically promote walkability and cycling. Promote active service opportunities for older people. Provide opportunities for people of all abilities to participate in physical activities. | Colac Otway Shire | Ongoing |

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| 2.2.12 | Environmental considerations | | | |
| 2.2.12.1 | The MTB trails have been designed and established around the village of Forrest. The trails showcase the natural beauty of the Otway Ranges, passing through tall eucalypt forests dry healthy scrub and dense fen gullies. | <ul style="list-style-type: none"> Promote and protect the unique environmental qualities of Forrest given its proximity to the Great Otway National Park and Otway Forest Park that makes it an attractive town for people visiting the area to experience the area's mountain bike trails. Actively promote the environmental qualities and attractiveness of the Forrest tracks from a local perspective as well as to a broader tourist market, to emphasise Forrest's role as a unique recreation asset for the Shire. | Colac Otway Shire
DELWP
Barwon Water | Ongoing |
| 2.2.12.2 | Phytophthora cinnamomi is a foreign plant pathogen, which attacks the root systems of susceptible plant species. <i>Phytophthora cinnamomi</i> does not spread quickly by itself across the landscape; instead people are the prime movers of the pathogen. | <ul style="list-style-type: none"> Ensure legible and adequate risk and warning signage is provided at the trail heads alerting visitors to the potential spread of <i>Phytophthora</i>, and methods riders should take to avoid the spread of the pathogen across the site. Investigate and establish a MTB wash down facility at the Forrest Trail Head. Refer recommendation 2.2.2 | DELWP
Barwon Water
Parks Victoria | High |
| 2.2.12.3 | Protection of the Forrest landscape and surrounding environments where the MTB trails are located is paramount, as there is currently no means to eradicate it in the field. | <ul style="list-style-type: none"> Conserve and enhance endangered Ecological Vegetation – Class (EVC) 83 Swampy Riparian Woodland is an important local and regional asset that should be conserved. Retain and protect existing areas of Native Vegetation and undertake revegetation works in targeted areas | DELWP
Colac Otway Shire
Barwon Water
Corangamite CMA
Land owners | Ongoing |
| | Large parcels of land within and surrounding the Forrest Township are densely vegetated. As promoted through State and Local Planning Policy, where possible native vegetation is to be retained alongside any future development. | | | |

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| | <p>Native Vegetation is classed into Ecological Vegetation Classes (EVC), a range of which are present in Forrest, including the following:</p> <ul style="list-style-type: none"> Endangered EVC 83 Swampy Riparian Woodland, occurring in two patches along the Barwon River West Branch; The vulnerable EVC 18 Riparian Forest occurs on the Barwon River West Branch adjacent to the township starting at Hennigans Crescent extending southwards, upstream in two patches to the base of the Barwon West Reservoir dam. A second linear patch occurs on the Barwon River East Branch North of Kents Road. The depleted EVC 16 Lowland Forest occurs in numerous patches in and around the township and on the perimeter of the cleared farm land to the North and East of the town. | <ul style="list-style-type: none"> Investigate options for local schools, community and friends groups, etc to participate in planting/revegetation days of targeted bushland areas. <p>The presence of numerous records of many fauna species illustrates the value in retaining native vegetation in large tracts. Retention and enhancement of native vegetation should continue to be a part of the vision for Forrest.</p> | Local Friends Groups | |
| 2.2.13 | Land ownership, land management and maintenance | | | |
| 2.2.13.1 | <p>The Forrest Mountain Bike Trails are located on Crown land to the south and north of the township. DELWP maintain over 3 land tenures (DELWP, Barwon Water and Parks Victoria).</p> <p>DELWP has responsibility for maintenance, signage, and risk management of approximately 66kms of MTB trails, with field staff at the Forrest depot.</p> <p>DELWP currently convenes the Forrest</p> | <ul style="list-style-type: none"> Following meetings with key agencies and stakeholders, in principle support was gained to establish an 'Advisory Group' for the Forrest MTB trails. The Advisory Group is to be developed at a strategic level with government agency representatives from COS, DELWP, BW and PV. The Advisory Group will convene as required to discuss and provide direction for ongoing maintenance, management and funding options for the trail network. It is identified that the formation of the Advisory Group is an urgent and critical action in order to guide implementation of this strategy. | Colac Otway Shire
DELWP
Barwon Water
Parks Victoria | High |

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| | <p>Mountain Bike Trails Committee, a stakeholder committee to coordinate operational matters in respect to maintenance, erosion control, alignment and possible new trails. DELWP Forrest has limited dedicated DELWP budgetary allowances or funding for specific MTB maintenance or new works. DELWP works closely with the Forrest MTB and Cycling Club and organises the Forrest Mountain Bike Club Volunteer work program.</p> | | | |

3. Management Structure

The fundamental importance that underpins the delivery of the Forrest MTB Strategy and all the opportunities identified is the commitment to, and the development of, an Advisory Group.

During the preparation of this strategy The Colac Otway Shire and DELWP have acknowledged a commitment to work collaboratively to develop an interagency management model to take responsibility for the Forrest MTB trails Final Strategic Plan. COS and DELWP are to form an Advisory Group to determine agency responsibilities, and this group would explore options for management structure, funding for staffing and operational funding and for who will undertake the works.

It is recommended that representatives from other agencies including Barwon Water and Parks Victoria be included as part of the Advisory Group. Key stakeholders may also be invited to participate once the group and terms of reference are established at a government governance level.

4. Funding Models

The following is a summary list of potential funding avenues and opportunities to be further explored by the Advisory Group;

4.1 First tier funding opportunities

4.1.1 Regional Infrastructure Development Fund

The Regional Infrastructure Development Fund was established to improve the competitive capacity of regional Victoria and enhance economic development through investment, job creation and the promotion of export opportunities. Coupled with the enhancement of regional cultural assets, the Building Strategic Tourism and Cultural Assets sub-program aims to encourage varied and quality experiences for those visiting and residing in regional and rural Victoria.

Projects seeking funding will need to demonstrate that they will have a beneficial impact on communities and surrounding regions, and support an improved experience for those who visit and reside in regional Victoria.

Infrastructure grants may be provided for tourism and cultural projects that:

- facilitate nature-based tourism opportunities including iconic walks and cycling trails;
- develop major eco-tourism opportunities including alpine and coastal areas;
- support or develop new iconic regional attractions;
- drive increased visitation and yield; and
- support and expand performing arts centres or regional art galleries and/or enhance cultural opportunities for regional residents and visitors.

4.1.2 2013 Tourism Industry Regional Development Fund

Victoria's regional tourism operators are invited to apply for round two of the Federal Government's Tourism Industry Regional Development Fund (TIRF) Grants Program. The competitive merit-based TIRF Grants Program offers grants from \$50,000–\$250,000 (GST exclusive) on a matched dollar-for-dollar funding basis, to assist tourism operators increase the quality and range of visitor experiences in regional Australia. Funding of approximately \$9.6 million is available for eligible projects, with approximately 50 projects across Australia expected to be funded in the current round.

4.1.3 T-QUAL

T-QUAL Grants is a competitive merit-based grants program aimed at stimulating sustainable economic growth in the Australian tourism industry. The program provides tourism businesses with matched dollar for dollar funding \$15,000 to \$100,000 (excl GST) for both large and small-scale tourism projects with the aim of increasing Australia's supply of quality tourism products and experiences and actively encouraging private sector investment in the development of Australia's tourism industry products, services and experiences.

4.1.4 Tourism Victoria Events Program

Tourism Victoria's Events Program assists event organisers, local government and other relevant groups to increase visitor numbers, length of stay and visitor yield at events. Funding is allocated through a cooperative marketing agreement. The primary aims are to enhance the event's tourism marketing strategy as well as to assist with event delivery and development.

4.1.5 Country Victoria Events Program (CVEP)

The CVEP provides up to \$10,000 per event to assist events have the potential to develop into significant regional events able to attract intrastate and interstate visitors. Closing dates for applications for the CVEP are 1 April and 1 October annually.

4.1.6 Other sources and strategies to help match above grant programs

The following are other potential sources and strategies for consideration for matching funding:

- Philanthropy Australia, the national peak body for philanthropy and a not-for-profit membership organisation.
- G21 Trails Strategy
- Victoria Trails Strategy
- Bicycle Victoria
- Colac Otway Shire
- DELWP

4.1.7 Other funding for local business development

- Business Victoria's website features a useful interactive search function to help businesses to look for funding avenues and support services available through the Victorian Government, from research and development to management and export available through the Victorian Government.
- The Federal Government's Grants LINK online directory provides information on funding available under a variety of grant programs for individuals, businesses and communities.
- Enterprise Connect's National Tourism Industry Network is a specialist national group. It brings together Business Advisers with expertise to help Australian businesses in the tourism sector. It is targeted at improving operations, productivity, competitiveness and sustainability by providing business advice, development services and industry linkages. Tourism accommodation providers, tourist attractions, tour operators, travel agencies and businesses offering local food and beverage tasting experiences are eligible to apply for Enterprise Connect services, which include matched grant funding. To be eligible, tourism businesses must also be solvent, have an annual turnover between \$1 million and \$100 million, possess an Australian Company Number and have a trading history of at least three years. Groups of tourism businesses may apply for assistance as well. The group must have minimum revenue or expenditure of at least \$2 million combined, with the revenue or expenditure of each business at least \$500 000.

4.1.8 Events charges

The Advisory Group is to discuss options for additional charges to be levied on event operators. Funds generated are to return directly to the Advisory Group to undertake essential maintenance of MTB trails and associated infrastructure utilised during events.

Additional event charges (above any existing/management fee events already contribute) are to be determined by the Advisory Group, in consultation with the event managers. Costs per competitor for each event or a % of event fees would be directed to the Advisory Group. The Advisory Group is to determine the most appropriate model.

4.2 Second tier funding opportunities

The following are second tier funding opportunities to be actively pursued by the Advisory Group in the context of the Strategy:

- Contributions from event organisation (hosting events in Forrest)
- Contributions from private sector
- Contributions from branding / product promotion
- Profits from special events.

4.3 Third tier funding opportunities

The following are third tier funding opportunities for further consideration by the Advisory Group:

- Contributions (funding or in kind labor) from local cycling groups
- Contributions (funding or in kind labor) from local community groups.

5. Implementation of the Strategy

The recommendations and actions in the Forrest MTB Strategy have gained endorsement by relevant agencies that have participated and have an interest in the project to ensure whole of government support across different land tenures and management responsibilities. The future implementation of the Forrest MTB Strategy has been determined by the PWG to rest with the formation of an Advisory Group (AG).

The formation of the Advisory Group was supported unanimously by the Project Working Group, and regarded as critical to the successful and timely implementation of the plan recommendations - primarily due to many actions (be they strategic, management or operational) require joint agency support to see them realised with best practice outcomes.

It is noted by the that differences in agency management approaches, strategic and resource priorities and variations in funding sources and allocations for capital works and recurrent budgets, results in a less than coherent and coordinated delivery of the current MTB facility. Therefore, the formation of an Advisory Group was regarded as an urgent priority to coordinate the implementation of actions, and realisation of the strategic vision, for the Forrest MTB facility. Stakeholders and the community supported this management direction in general.

Determining a proven and robust Advisory Group structure for implementation of the Forrest MTB Strategy will be a priority action for the Advisory Group, using a standard government management mechanism. The plan has identified a range of preliminary objectives that the Advisory Group will determine:

- governance
- terms of reference
- format and structure for decision making
- resource and budget bids, allocations and priorities
- structure for employees and contractors carrying out works
- insurance and OHS
- agreement between agency(s) regarding roles and responsibilities
- community and stakeholder consultation and input
- review of the strategic plan and actions.

The Forrest MTB Strategy defines the recommendations and actions pertaining to the strategy. Further technical detailed trail design and site assessment regarding environmental and cultural heritage values is required. Implementation of these works would be managed by the newly created Advisory Group.

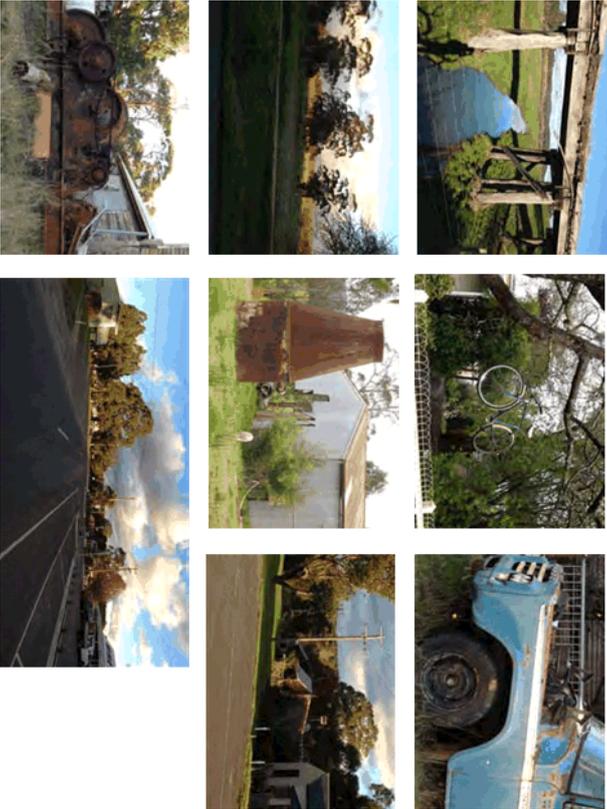
6. Strategic Trails Plan



Appendix A.
Detailed Site Analysis Summary

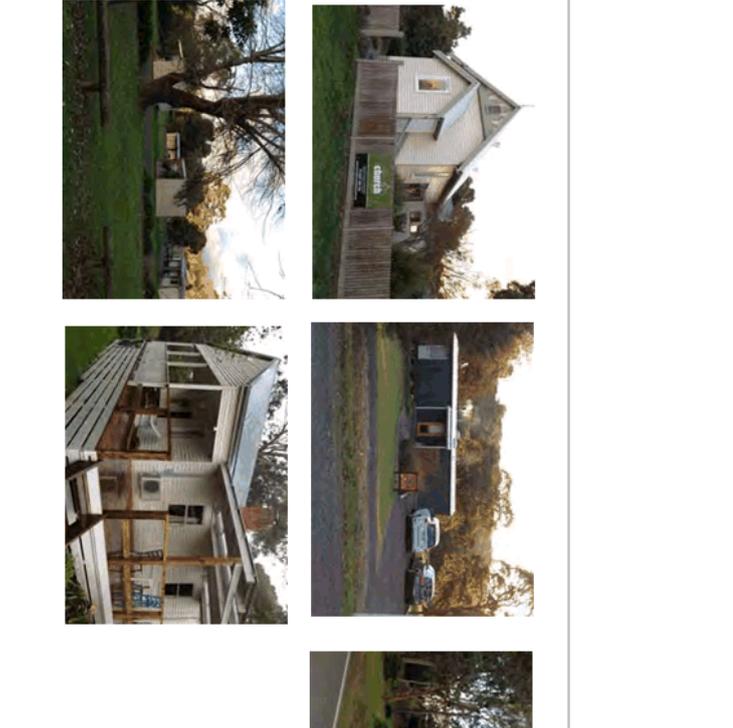
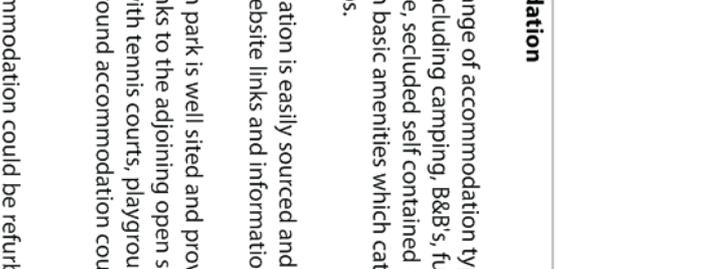
DETAILED SITE ANALYSIS SUMMARY

The following detailed site analysis summary focuses on the three key areas of the Forrest Township, the Yaugher MTB Trails and the Forrest MTB Trails. Site investigation and analysis was undertaken by TBLD in late 2013 during the analysis and consultation phases. This summary of detailed site analysis will directly inform the Issues and Opportunities Paper and subsequent Strategic Plan.

| No. | Analysis | Photo |
|-----|--|--|
| 1.0 | <p>Forrest Township</p> | |
| 1.1 | <p>Forrest Character and History</p> <p>The township of Forrest has retained its 'old Victorian country town' charm with its tall mature green trees set against the pastoral green hills, the main street focuses on core services such as the schools, church, pub, general store, etc, and the wide open streets are welcoming without urban clutter. There is a relaxed village community feel with evidence of the new young residents bringing an energy and business to revitalise the town, while retaining a strong sense of authenticity and genuineness.</p> <p>Remnants of the timber town working aesthetic are still visible and valued as the foundations of the town within the local community. Evidence of the former mill, old machinery, vehicles, sheds, buildings, equipment, etc are scattered amongst the town streetscape and treasured within both private residences and public buildings.</p> |  |

| No. | Analysis | Photo |
|-----|---|---|
| 1.2 | <p>Pathway network</p> <p>There is a number of existing off road paths within the township, including;</p> <ul style="list-style-type: none"> • Tiger Rail Trail which provides an off road shared path link, south of Birregurra-Forrest Road, which extends from Boundary Road to Station Street. • A sealed asphalt path (approx 1-1.5m wide) extends from Colac-Forrest Road to Turner Drive (north of the main street) providing a link to the Primarily School, the Forrest Hall, etc. • A wide gravel (approx 2.5-3.5m wide) extends south from Henry Street and currently terminates approximately half way towards the General Store. • Barwon Street is an unsealed local access road located parallel between the two main roads of the town, which provides an on-road path link via this underutilised roadway. • Other pathways within the township provide shortcuts through the town e.g. the path link opposite Hennigan Cres west to Birregurra-Forrest Road. <p>Following a meeting with COS Traffic Engineer Officer, it was confirmed that COS will undertake an upgrade of the pathway system along the eastern side of the main street to a shared trail width (2.5m).</p> |  |

| No. | Analysis | Photo |
|-----|---|---|
| 1.3 | <p>It was discussed that the Forrest MTB Strategic project is to identify enhanced improvements to the proposed path upgrade works to ensure a shared trail can be established to accommodate for the range of users including pedestrians, dog walking, children riding to school, MTB etc</p> <p>Signage</p> <p>The Colac Otway Shire 'Forrest' township sign clearly identifies the entrance to the township.</p> <p>At the intersection of Colac-Forrest Road and Birregurra -Forrest Road there is an overload of signage and information for a visitor to take in. Rationalisation and consolidation would improve legibility.</p> <p>Interpretation signage is dotted around the township. Green recycled plastic posts and boards display images and stories of the previously lifestyle and historic development of the Forrest Township. The use of green recycled plastic material appears out of place, weathering and looks cheap. This material is aesthetically disconnected from the timber town history and should be replaced with thematic signage reflecting the genuine country character and qualities of the township</p> |    |

| | |
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| <p>1.4</p> <p>Accommodation</p> <p>There is a range of accommodation types available in Forrest including camping, B&B's, full service Guest House, secluded self contained cabins and houses with basic amenities which cater for small to large groups.</p> <p>Accommodation is easily sourced and booked via excellent website links and information.</p> <p>The caravan park is well sited and provides excellent links to the adjoining open space of the township with tennis courts, playground. Some of the campground accommodation could be upgraded.</p> <p>Some accommodation could be refurbished, updated and target higher end visitors.</p> |  |
| <p>1.5</p> <p>Food and beverage options</p> <p>There are four key food and beverage outlets within Forrest (within certain operational hours/season basis)</p> <ul style="list-style-type: none"> - Forrest Brewery - The Wonky Donkey - The Corner Store - Forrest Country Guest House |  |

| | | |
|-----|---|--|
| 1.6 | <p>Ability to purchase provisions</p> <p>The ability to purchase general provisions in Forrest township is limited. The General Store stocks basic commodities, milk, bread, etc but the range is limited.</p> <p>General provisions are purchased prior to staying overnight at Forrest.</p> <p>There is no bakery in town. Coffee is available at the Brewery, Corner Store and the Forrest Guest House.</p> |  |
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| 1.7 | <p>Other sporting/recreational facilities</p> <p>Other than the primary recreational activity of Mountain Biking, visitors at Forrest can enjoy bushwalking, horse riding and a shared walking path to Lake Elizabeth. Paintball activities are also offered in the area.</p> |  |
| 2.0 | <p>Yaughter MTB Trails</p> | |
| 2.1 | <p>Yaughter Trail Head facilities and signage</p> <p>The Yaughter Trail Head is signed along Birregurra - Forrest Road and is located off Boundary Road. The car park is signed using a standard DEPI location and directional sign.</p> <p>Facilities at the trail head include an unsealed car park, standard DEPI public toilet (no water) and a few picnic tables.</p> |  |

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| <p>No portable water or MTB wash down facilities are currently available.</p> <p>At the beginning of the trails is a comprehensive display map of the Yaughner trail network which includes a description of the types of trails, distance of trail and a difficulty rating. Other DEPI information is attached to the sign informing visitors of fire risks, track works, etc.</p> | |
| <p>2.2</p> <p>Yaughner MTB Trails</p> <p>The Yaughner MTB trails provide a diverse and exciting selection of well laid out MTB loops and trail routes, set within a bushland landscape. These trails cater for a variety of MTB riders from family friendly, to beginner, intermediate and up to experienced riders to undertake technical and challenging sections of trails.</p> <p>There are 10 dedicated MTB trails which make up the 44km's of the Yaughner trail network which include:</p> <ul style="list-style-type: none"> • Yaughner Super Loop
Rating: More Difficult Distance: 11.5kms • Grasstreets
Rating: Very Difficult Distance: 1.5kms • Marriners Run
Rating: Very Difficult Distance: 4.5kms • FOxtail
Rating: More Difficult Distance: 3.5kms • J2 |  |

| | |
|--|--|
| <ul style="list-style-type: none"> • Rating: More Difficult Distance: 4kms • Yoyo <ul style="list-style-type: none"> Rating: Very Difficult Distance: 3kms • Casper Black <ul style="list-style-type: none"> Rating: Very Difficult Distance: 5.5kms • Barre Warre <ul style="list-style-type: none"> Rating: Easy Distance: 2kms • Vista <ul style="list-style-type: none"> Rating: More Difficult Distance: 5.5kms • Tiger Loop <ul style="list-style-type: none"> Rating: More Difficult Distance: 3kms | |
| <p>2.3 Other Trails (horse & fire tracks)</p> <p>Sections of trails to the outer fringe of the Yaughar MTB trail network have been created by DEPI for use as fire breaks and management vehicle tracks.</p> <p>These fire break tracks are also utilised by the local horse riding club. Some trails are marked as horse trails only (not MTB).</p> |  |
| <p>2.4 Trail Signage</p> <p>The existing trail signage utilises timber posts with directional arrows, numbering and pictorial system. The trail map readily available online or from the Corner Store allows the visitor to easily read the directional signage and navigate the trails. It is assumed without a copy of the trail map on hand navigating the trails could be challenging.</p> |  |

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| <p>2.5</p> <p>Birregurra-Forrest Road crossing</p> <p>The point at which MTB cyclists and pedestrians currently cross the Birregurra-Forrest Road (to link to Boundary Road and access the Yaughar trail network), is at the apex of a curve and the road is currently signed at 100kms. The crossing point is informal, unmarked and unsigned with poor sightlines in both directions. Reaction time is very poor as vehicles appear within seconds without warning. This is particularly unsuitable for young people, beginners and older people. Given the critical importance of this link to the Yaughar MTB network from Forrest, a safe solution is required.</p> <p>Following a meeting on site with the COS Traffic Engineer Officer, it was agreed that the crossing of Birregurra-Forrest Road presents unacceptable safety crossing challenges for both MTB cyclists and pedestrians. This has also been highlighted in community consultation and feedback from Forrest Event organisers.</p> <p>It was discussed and agreed with COS Officer that the Forrest MTB Strategic project is to recommend that a tunnel underpass be investigated to provide a safe crossing point for all users.</p> |  |
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| <p>2.6</p> <p>Boundary Road alignment</p> <p>For MTB riders travelling from Forrest to the Yaughner MTB trail network, cyclists must travel along Boundary Road (an unsealed steep roadway shared with vehicle traffic).</p> <p>This is not the ideal MTB visitor experience and the shared use presents significant safety risks to young children riding on the road, visitors who may have limited cycling skills, and to vehicles as well.</p> <p>An off road cycle trail is to be investigated.</p> |  |
| <p>3.0</p> <p>Forrest MTB Trails</p> | |
| <p>3.1</p> <p>Forrest Trail Head facilities and signage</p> <p>The Forrest Trail Head is located just south of the township on the main street (Forrest-Birregurra Road).</p> <p>Facilities at the trail head include a sealed car park, a large picnic shelter, picnic tables, low seating wall and a standard DEPI public toilet (no water).</p> <p>No water or MTB wash down facilities are currently available.</p> <p>At the beginning of the trails is a comprehensive display map of the Yaughner trail network which includes a description of the types of trails, distance</p> |  |

| | |
|--|---|
| <p>of trail and a difficulty rating. Other DEPI information is attached to the sign informing visitors of fire risks, track works, etc.</p> | |
| <p>3.2</p> <p>Forrest MTB Trails</p> <p>The Forrest MTB trails provide a range of MTB ride options only a short distance from the township. These trails provide visitors with the opportunity to ride and experience the spectacular natural beauty of the Otway Ranges passing significant eucalypt forests, dry heathland scrub and dense fern gullies. These trails cater for a variety of MTB riders from family friendly, to beginner, intermediate and up to experienced riders.</p> <p>There are 6 dedicated MTB trails which make up the 22km's of the Forrest trail network these include:</p> <ul style="list-style-type: none"> • Forrest Loop
Rating: Easy Distance: 3kms • Barildjaru
Rating: More Difficult Distance: 6kms • Red Carpet
Rating: More Difficult Distance: 4.5kms • Roller Coaster
Rating: Easy Distance: 2kms • Third Time Lucky
Rating: More Difficult Distance: 2kms • Follow the Dog
Rating: More Difficult Distance: 4.5kms |  |

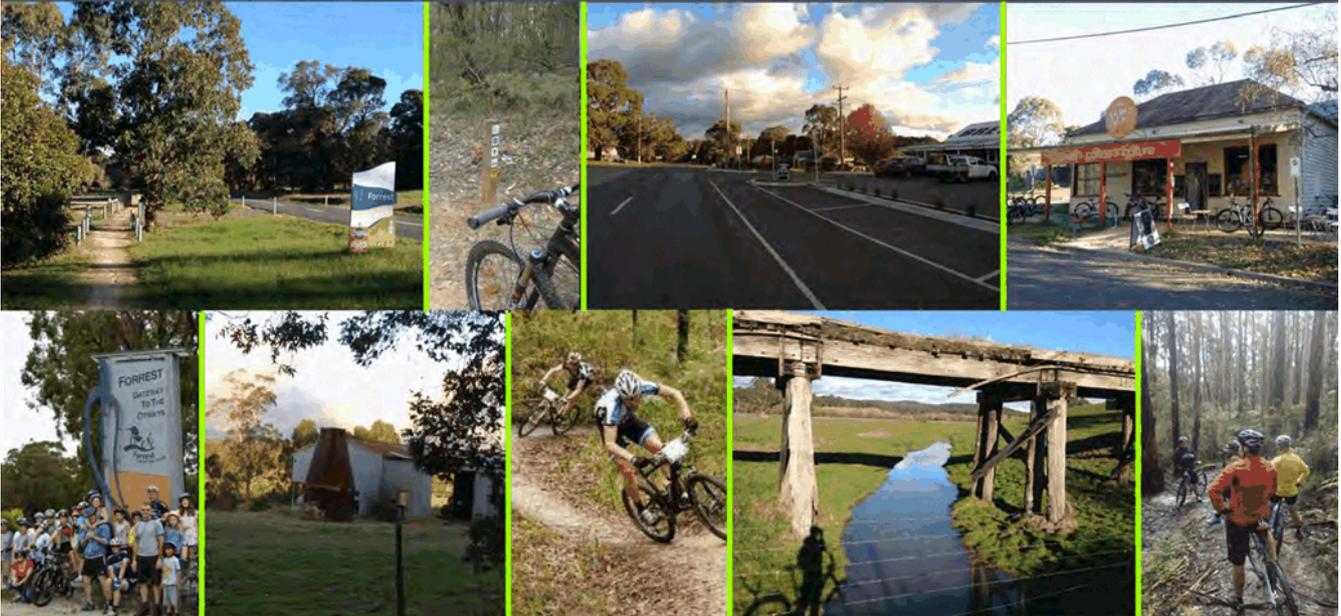
| | | | |
|-----|----------------------|--|--|
| 3.3 | Trail Signage | The existing signage along the trails utilises a mix of timber posts with directional arrows, numbering and pictorial system and timber routed signs |  |
|-----|----------------------|--|--|

Appendix B.
Forrest Issues and Opportunities Paper

FORREST MTB STRATEGIC PLAN

Issues and Opportunities Paper
To inform the Draft Forrest MTB Strategic Plan

Draft
For internal Project Steering Group review and discussion only



Prepared by:

 **THOMPSON BERRILL LANDSCAPE DESIGN P/L**

in association with Simon McArthur & Associates

February 2014

| REVISION | DATE | AMENDMENT |
|----------|------------|---|
| A | 27/02/2014 | Draft V1 – Internal Project Steering Group Review and Comment |

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Attachments

- Attachment 1. Forrest and Yaugher MTB Trail Network Map
- Attachment 2. Forrest MTB Online Survey Results
- Attachment 3. Community Letter
- Attachment 4. Background Documents Review
- Attachment 5. Benchmarking Analysis

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1. Introduction

The objective for this paper is to outline key directions to inform the strategic plan to guide the sustainable future of Forrest's Mountain Bike Trails. Key topics to be addressed include appropriate land management and maintenance for MTB trail use, potential MTB trail expansion, protection and management of environmental values, recognition, and an understanding of the historic values, social values of the Forrest community. The report will also address visitation to Forrest, accommodation, food and beverage, enhanced public recreational access, infrastructure, and provision of public facilities as required to provide sustainable future visitation to the Forrest MTB Trails. This paper will inform the development of the Draft MTB Strategic Plan.

The State Government, through DEPI and RDV, have agreed to support a Strategic Plan to establish a vision for the future development of the Forrest Mountain Bike Trails. The project is being coordinated by Colac Otway Shire and guided by a Project Steering Group comprising representatives from key Council Departments, DEPI, Barwon Water and key local Stakeholders. The paper will develop an exciting strategic vision for the Forrest MTB Trails, building on the current high profile of 'Forrest' as a key MTB destination within Victoria, and further explore the MTB trails connection, relationship and opportunities linked with the Forrest Township.

The development of this paper has also been informed by detailed site visits and conditions assessment, consultation meetings and workshops held with agency and stakeholders representatives including DEPI, Barwon Water, Parks Victoria, Tourism Victoria. A strategic workshop was held with specialist MTB industry representatives and input gained from the local community via an informal 'drop in' session, and via broader community consultation via an online survey.

1.1 Project Context

Forrest is located approximately 30kms south east of Colac and 36kms from Apollo Bay on one of the major tourist routes connecting the Princes Highway with the Great Ocean Road. It has a population of 160 and is undergoing economic and social transformation resulting from the reduction in logging and agricultural activities and the increasing focus on tourism and the Great Otway National Park.

A number of factors have influenced change in the Otways region, including the declining number of dairy farms and an increase in pine and blue gum plantations and the diminished role of small townships as service and supply centres. Logging activities on public land ceased in 2008 and by this time all timber mills in the town had closed.

Factors in Forrest's favor were the designation of the Forrest to Apollo Bay Road as a priority north-south Great Ocean Road access from the Princes Highway and the influence of 'Tree Change Lifestyle' newcomers to the area. An increasing focus on tourism accommodation, product development and visitor experiences was becoming evident and the 2003 State Government, "A New Future for the Otways" Tourism Initiative proposed to further stimulate local economies within the Otway Ranges through the development of tourism. This was to offset the losses in

employment and local economy resulting from the cessation of commercial hardwood harvesting. The Forrest Mountain Bike Trails are the product of this strategy.

Formal MTB trail development commenced in 2004, and the network was opened in 2007. The project was led by the former Department of Sustainability and Environment (DSE) – now Department of Environment and Primary Industries (DEPI), with input from Parks Victoria and Colac Otway Shire. DEPI currently has responsibility for maintenance, signage, and risk management of the total network. DEPI convenes the Forrest Mountain Bike Trails Committee, a stakeholder committee to coordinate operational matters in respect to maintenance, erosion control, alignment and possible new trails. DEPI work closely with the Forrest Mountain Bike and Cycling Club and organises the Forrest Mountain Bike Club Volunteer work program, who provide invaluable assistance to the DEPI staff.

Forrest Mountain Bike trails are a network of 16 trails that total approx. 66km in length. There are two trail heads with car parks, signs, toilets and shelter facilities. The trails also link to the recently completed Tiger Rail Trial Stage 1, a shared access trail that is planned to eventually link Forrest to the township of Birregurra over a distance of 42kms, subject to funding and planning constraints.

The Forrest Mountain Bike Trails are home to a number of major cycling and other recreational and social events including the Forrest Festival that is staged over two days and includes a 3 stage Mountain Bike event. Other events include the Forrest 6 Hour, the Geelong Mountain Bike Club 3 Hour and the popular Otway Odyssey, considered one of the toughest most grueling one day Mountain Bike races in the world. The Forrest Mountain Bike & Cycling Club also regularly runs social rides for members. The popular Corner Store also provides MTB hire, tours, and lessons for visitors.

1.2 Project Vision

The vision for the project is to develop a Strategic Plan that secures a sustainable future of Forrest MTB industry and that has the support of key stakeholders and the local community, representing a balance between community and stakeholder views and State Government Policy.

The project is to establish justifiable strategic directions for the planning, provision, development, preservation and management of the Forrest Mountain Bike Trails networks and supporting infrastructure based on clear evidence of stakeholder and community demand and sustainable asset management.

The Forrest Township is located at the heart of the MTB trail system, physically located between the two MTB trail networks of the northern trails (Yaughter) and southern trails (Forrest). Survey findings indicate that the facilities and services provided by the town to MTB visitors are crucially linked to MTB rider visitation and to the town.

The development of the Strategic Plan for Forrest MTB Trails will provide a framework to address future opportunities to allow the MTB activates and culture to continue to grow whilst still retaining the essential attributes and elements which make visiting Forrest and riding the MTB trails a positive and successful visitor experience.

1.3 Study Area

Forrest, Victoria is a small rural township located in the Otway Ranges, located approximately 160km south west from Melbourne. The Forrest Mountain Bike Trails are located on Crown Land to the south and north of the Forrest Township, with the trails network crossing a number of land tenures including the Great Otway National Park and land managed by Barwon Water. The West Barwon River flows through the township and it is near the West Barwon reservoir, which services Geelong.

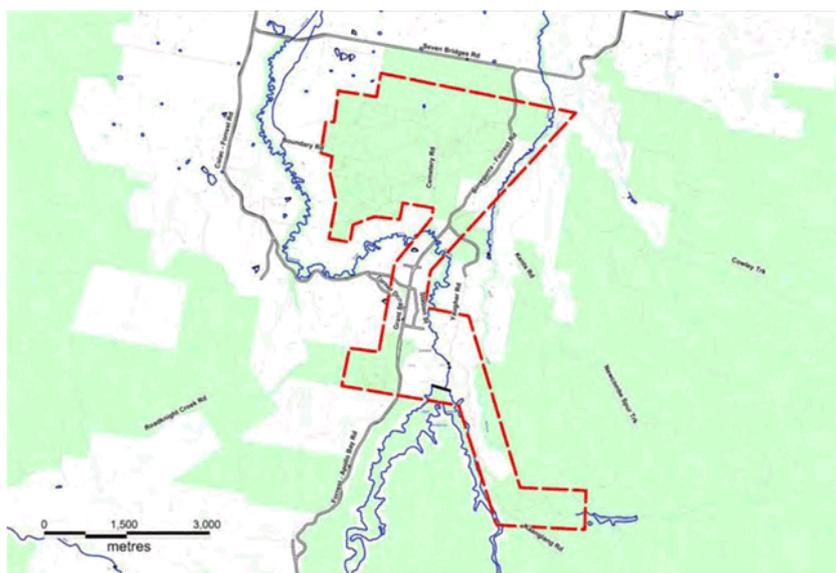


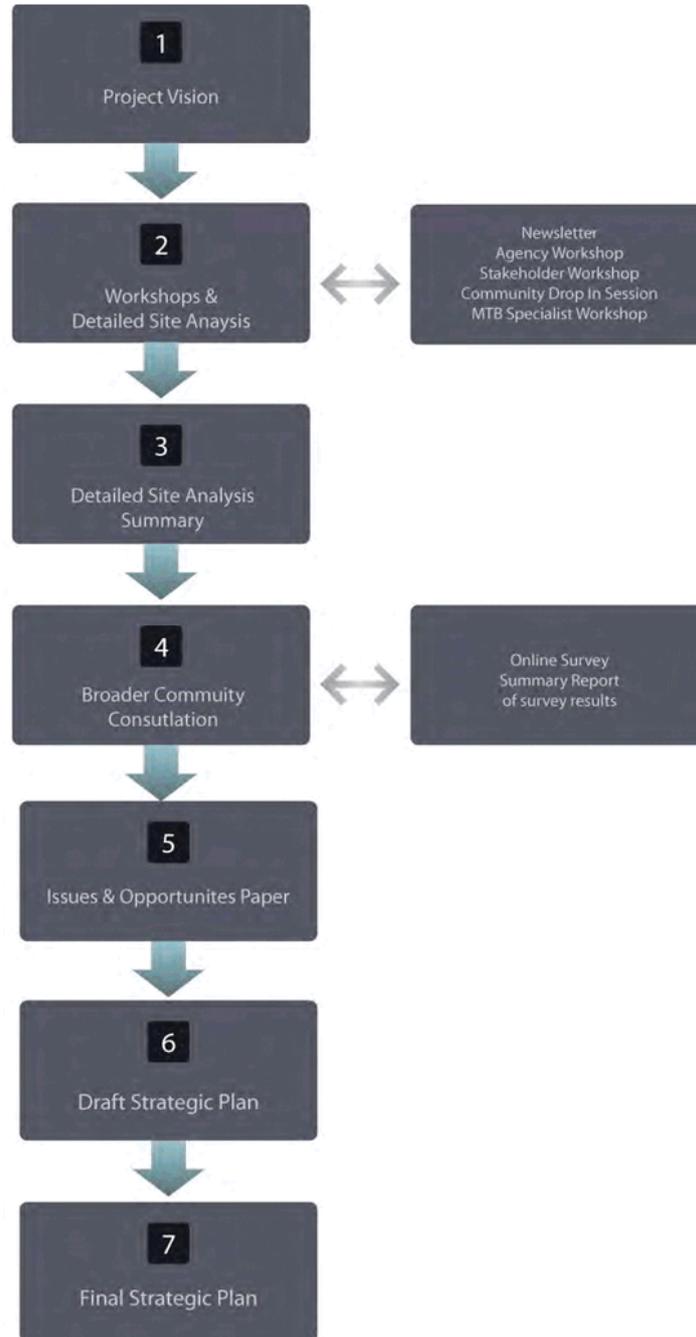
Figure 1. Study Area

Source: Colac Otway Shire Forrest Brief

Refer Attachment 1. Forrest and Yaugher MTB Trail Network Map

1.4 Project Methodology

Outlined below is a summary of the Project Methodology utilised for the project:



2. Consultation

2.1 Purpose of consultation

Consultation and engagement with key agencies, stakeholders the local community and the broader MTB community was undertaken during 2013. The consultation phase is considered to be fundamental to the development of the Issues and Opportunities Paper, to inform the Forrest MTB Strategic Plan. Consultation with land managers, local community and the broader community who currently use and visit Forrest to ride the MTB trails, provided an important opportunity to identify relevant issues and opportunities to be considered in conjunction with the Project Working Group, in the development of the Strategic Plan.

As the study area encompasses crown land and crosses a number of land tenures (managed by Colac Otway Shire, VicRoads, DEPI and Barwon Water) the consultation phase provides insight from the various government agencies on their current land management and maintenance procedures and policies, identifying key issues and opportunities to be considered as part of the Strategic Project, focusing on improved interagency shared tasks and responsibilities.

The consultation undertaken to date has been in accordance with the Colac Otway Shire Community Engagement Policy.

2.2 Consultation methods and outcomes

The following is a summary of the consultation works undertaken to date in preparation for the Draft Issues and Opportunities Paper and for consideration in the development of the Forrest MTB Strategic Plan.

2.2.1 Project Steering Group Meetings

As outlined in the brief, the project is under management by the Colac Otway Shire's Acting Manager Economic Development (Michael Swanson and previously by Mick Cosgriff and Mike Barrow). The project is guided by a Project Steering Group consisting of Colac Otway Shire Officers including Tourism Development Officer (Adrian Healey), Recreation & Events Coordinator (Jodie Finchman), Manager Recreation, Arts and Culture (Ian Seuran), DEPI Planning Manager Otway District (Craig Clifford), Barwon Water Catchment Coordinator (Jared Scott) and Parks Victoria Ranger (Michelle Anstee). The Steering Group will meet throughout the project to input and provide direction on key issues.

Following discussions with the Project Steering Group at the Project Inception Meeting, a list of the key stakeholders and community audiences were identified to engage with as part of the consultation phase of the project.

2.2.2 Agency Workshop

An Agency Workshop was held in September 2013, at the Forrest Hall, Forrest. Representatives from Colac Otway Shire, DEPI, Barwon Water and Tourism Victoria attended and actively contributed in discussions to identify the key project goals and values, events, key management and maintenance issues and potential co-management opportunities.

2.2.3 Stakeholder Workshop

A Stakeholder Workshop was held in September 2013, at the Forrest Hall, Forrest. Representatives from Colac Otway Shire, Otway Tourism, Forrest Mountain Bike Club, Tiger Rail Trail Association, Otway Hinterland Tourism Association/Forrest Brewery, and The Corner Store attended and actively contributed in discussions to provide valuable insights into the communities historic and current involvement with the development of the MTB facility, key issues and opportunities for sustaining the MTB trail and visitation to Forrest, and the Forrest Township economy and general community values.

2.2.4 Local Resident Letter

TBLD assisted the Project Manager in the preparation of a letter informing local residents of Forrest of the development of a MTB Strategic Plan and inviting the local residents/community to attend an Informal 'Drop In' Comment and Feedback Session, where the local residents can raise key concerns, issues or opportunities to be considered in the development of the Strategic Plan and future planning.

The Local Resident Letter was supplied to Council and Communications department for review and approval prior to being printed and distributed, by Council. Distribution of the letter was via letterbox drop to all local residents within the Forrest Township.

Refer Attachment 3: Local Resident Letter

2.2.5 Informal Community 'Drop In' Comment and Feedback Session

TBLD's Director (Glenn Berrill) and Senior Landscape Architect (Andrea Szymanski) with assistance from Colac Otway Shire (Mike Barrow) set up large scale printed plans for viewing and ran a 3 hour informal 'Drop In' Session in the Forrest Hall in September 2013, to gain consultation input from the local community. A large number of local residents attending the session and were happy to tell their stories, contribute input as to why they live in Forrest, discuss the changes witnessed in the town since the cessation of logging, and what they value about Forrest and the MTB activities. The community consultation session had a positive atmosphere and the community reinforced how important the MTB trails are to the town in conjunction with retaining the relaxed lifestyle and old timber town character of Forrest.

2.2.6 Specialist MTB Workshop

As part of the consultation phase of the project TBLD invited key specialist members from the MTB industry to participate in a MTB strategic workshop, to evaluate, discuss and identify overall design principals, technical aspects and key

considerations to enhance the existing MTB facility and establish Forrest as a premier MTB destination in Australia.

The Specialist MTB Workshop was held in September 2013, at the Forrest Brewery. Representatives from the International Mountain Bicycling Association Australian (IMBA), World Trails, Rapid Ascent Events and Forrest Bike Hire (The Corner Store) attended the workshop. The workshop provided invaluable insights and presented an opportunity to gain expert industry knowledge and information pertaining to the success of MTB industry in Forrest and on how to provide the project with the best technical strategic direction and vision from Australia's MTB experts.

2.2.7 Online Survey (survey monkey)

To gain additional broader consultation from the MTB community, following approval from Colac Otway Shire, TBLD developed a list of key questions to be used as part of an online Forrest MTB survey. TBLD prepared the online survey (via survey monkey) and the survey was promoted via Colac Otway Shire website, Ride Forrest website, and relevant Facebook pages (with web technical assistance from Norm Douglass) with a successful response.

Refer Attachment 2. Forrest MTB Online Survey Results

3. Issues and Opportunities

The following tables identify the issues and opportunities identified by stakeholders, agencies and the local community, and broader MTB community during the research, analysis and consultation phase that will directly inform the Strategic Plan. The opportunities will be refined into actions and priorities outlined in the Strategic Plan.

| No. | Issue | Opportunity |
|-------|---|--|
| 3.1 | Forrest Township | |
| 3.1.1 | <p>Forrest Character and History
 The township of Forrest has retained its old Victorian country town charm with its tall mature gum trees set against the pastoral setting of the rolling green hills. The main street focuses on core services such as the schools, church, pub, general store, etc, and the wide open quite and shady streets are welcoming without urban clutter. There is a relaxed village community feel combined with evidence of the new young residents bringing energy and business opportunities to revitalise the town, while retaining a sense of pleasantness within the local community.</p> <p>Remnants of the timber town working aesthetic are still visible and valued by (the community) as the foundations of the town. Evidence of old forestry machinery, vehicles, sheds, buildings, equipment etc are scattered amongst the streetscape and treasured within both private residences and a part of public buildings.</p> | <p>Retain and protect the visual clues of the old Victorian country town character. Ensure new developments (incl buildings, civil works, public infrastructure, etc) do not detract from the country town theme, without becoming folly or kitsch.</p> <p>Results of the online survey indicate that the element that MTB visitors like most about the Forrest Township is its 'rural township atmosphere' followed by its 'low key' culture.</p> <p>Ensure the township is not generisicied or gentrified and maintains its genuine 'Forrest' town character, as a point of difference from the other Colac Otway region small townships.</p> <p>Retain the wide relaxed feel of the main street, retain as kerb free and ensure the town does not become over built or urbanised.</p> |
| 3.1.2 | <p>Pathway network
 There is a number of existing off road paths within the township, including;</p> <ul style="list-style-type: none"> • The Tiger Rail Trail which provides an off road shared path link, south of Birregurra-Forrest Road, which extends from Station Street to Boundary Road. • A sealed asphalt path (approx 1-1.5m wide) extends from Colac-Forrest Road to Turner Drive (north of the main street) providing a link to the Primary School and Forrest Hall. • A wide gravel path (approx 2.5-3.5m wide) extends south from Henry Street | <p>Establish a separate shared trail on the east of the main street (via the upgrade and widening of recent path works to 2.0m wide) to accommodate for the range of users including MTB riders, pedestrians, dog walking, children riding to school, etc.</p> <p>Provide a safe crossing point for users of the shared trail between the north and south of the main street.</p> <p>Promote Barwon Street as an additional cyclist/walker path through town, as</p> |

| No. | Issue | Opportunity |
|-------|--|--|
| | <p>and currently terminates approximately half way towards the General Store.</p> <ul style="list-style-type: none"> Barwon Street is an unsealed local access road located parallel between the two main roads of the town, which provides an on-road path link via an under utilised roadway. Other pathways within the township provide shortcuts through the town e.g. the path link opposite Hennigan Cres west to Birregurra-Forrest Road. <p>Following a meeting with COS Traffic Engineer Officer, it was confirmed that COS will be undertaking an upgrade of the pathway system along the main street.</p> | <p>little vehicle traffic utilises this road. Retain as unsealed.</p> |
| 3.1.3 | <p>Signage
The main Colac Otway Shire 'Forrest' township sign clearly identifies the entrance to the township and notes its MTB role.</p> <p>At the intersection of Colac-Forrest Road and Birregurra -Forrest Road there is an excessive overload of signage and information for a visitor to take in. Rationalisation and consolidation is essential to improve visitor amenity and provide effective information.</p> <p>Interpretation signage is dotted around the township. Green colored recycled plastic posts and boards display images and stories of the previous lifestyle and historic development of the Forrest Township. This material is highly inappropriate and aesthetically disconnected from the timber town history, character and significantly detracts from the towns' aesthetic.</p> | <p>Undertake active review of existing of signage and consolidate.
Investigate opportunity to provide key visitor information, directional and historic signage at a key public open space site and destination.</p> <p>Undertake appropriate design to upgrade township interpretation signage. Signage form and materials to be developed to reflect the genuine country character and historic qualities of the township.</p> |
| 3.14 | <p>Other sporting/recreational facilities
Other than the primary activity of Mountain Biking, visitors at Forrest can enjoy bushwalking, horse riding, and shared walking path to Lake Elizabeth, active sports and the oval and to the north of town.</p> | <p>Highlight other local recreational opportunities via public information/signage at a key public open space site. This will allow visitors who have not pre-planned their activities before arriving at Forrest to engage in other recreational activities offered in the area.</p> |
| 3.2 | Forrest MTB Trails Networks | |
| 3.2.1 | The Forrest MTB Trails Network provides a range of MTB ride options only a short | The results of the online survey identified that the most popular trail |

| No. | Issue | Opportunity |
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| | <p>distance from the township, with 'ride in ride out' ease. These trails provide visitors with the opportunity to ride and experience the spectacular natural beauty of the Otway Ranges passing significant eucalypt forests, dry heathland scrub and dense fern gullies. These trails cater for a variety of MTB riders from family friendly, to beginner, intermediate and up to experienced riders.</p> <p>There are 6 dedicated MTB trails which make up the 22km's of the Forrest trail network these include;</p> <ul style="list-style-type: none"> • Forrest Loop
Rating: Easy Distance: 3kms • Barlidjaru
Rating: More Difficult Distance: 6kms • Red Carpet
Rating: More Difficult Distance: 4.5kms • Roller Coaster
Rating: Easy Distance: 2kms • Third Time Lucky
Rating: More Difficult Distance: 2kms • Follow the Dog
Rating: More Difficult Distance: 4.5kms <p>Refer Attachment 1. Forrest and Yaugher MTB Trail Network Map</p> | <p>within the Forrest system is Red Carpet, followed by Follow the Dog. These trails are exciting and challenging.</p> <p>The least popular trail is Baridjaru.</p> <p>The survey results suggest improvement of trails focusing on challenge and skills development. Refer item 3.2.5</p> <p>For a full summary of the online survey results pertaining to MTB trails, refer Attachment 2.</p> |
| 3.2.2 | <p>The Forrest Trail Head is located just south of the township on the main street (Forrest-Birregurra Road).</p> <p>Facilities at the trail head include a sealed car park, a large picnic shelter, picnic tables, low seating wall and a standard DEPI (not flush) public toilet.</p> <p>No water or MTB wash down facilities are currently available.</p> <p>At the beginning of the trails is a large display map of the Forrest trail network which includes a description of the types of trails, distance of trail and a difficulty rating. Other DEPI information is often attached to the sign informing visitors of fire risks, track works, etc.</p> | <p>Investigate design upgrade to the Forrest Trail Head, provide additional landscaping to carpark area, and provide key visitor and information signage, and improve trail entry experience.</p> <p>Investigate installation of a drinking fountain and a MTB wash down facility at the site. The wash down facility was identified in the results of the online survey, as a key improvement to be made to Forrest to encourage the user to stay longer.</p> <p>For a full summary of the online survey results refer Attachment 2.</p> |
| 3.2.4 | <p>The existing trail signage utilises a mix of timber posts with directional arrows, numbering and pictorial system and timber routed signs</p> | <p>Refer item 3.2.4</p> |
| 3.2.5 | <p>Additional MTB Trails
The extent of the current MTB trails is not</p> | <p>The online survey results clearly</p> |

| No. | Issue | Opportunity |
|-------|---|--|
| | <p>used to capacity (especially within the Yaugher trails network). Sections of trails continually need to be upgraded, and sections rejuvenated to enhance the rider experience and to maintain the status and attraction of Forrest as a key MTB destination.</p> | <p>indicate that there are enough trails available at Forrest, especially to accommodate a full two days of riding.</p> <p>If additional trails are to be established this could occur on the proviso that other less utilised MTB trails at Yaugher were rationalised and closed. This would provide improved quality trails in Forrest (to enhance MTB visitor satisfaction and challenge) over lower quantity of trails provided and assist in ongoing maintenance and management issues to improve visitor satisfaction.</p> <p>Investigate development of more technical trails/other MTB styles (e.g. long flowing downhill challenging trails)</p> <p>Investigate the development of a skills park to be located at the Forrest Trail Head.</p> |
| 3.3 | Yaugher MTB Trail Network | |
| 3.3.1 | <p>The Yaugher MTB Trails Network provides a diverse and exciting selection of well laid out MTB loops and trail routes, set within a bushland landscape. These trails cater for a variety of MTB riders from family friendly, to beginner, intermediate and up to experienced riders to undertake technical and challenging sections of trails.</p> <p>There are 10 dedicated MTB trails which make up the 44km's of the Yaugher trail network these include;</p> <ul style="list-style-type: none"> • Yaugher Super Loop
Rating: More Difficult Distance: 11.5kms • Grasstrees
Rating: Very Difficult Distance: 1.5kms • Marriners Run
Rating: Very Difficult Distance: 4.5kms • Foxtail
Rating: More Difficult Distance: 3.5kms • J2
Rating: More Difficult Distance: 4kms • Yoyo
Rating: Very Difficult Distance: 3kms • Casper Black
Rating: Very Difficult Distance: 5.5kms | <p>The results of the online survey identified that the most popular trail by far within the Yaugher system is Marriners Run, followed by Grasstrees and then closely by Yaugher Super Loop and Foxtail.</p> <p>The three least popular trails are Vista, Barre Warre and lastly is Tiger Loop.</p> <p>For a full summary of the online survey results refer Attachment 2.</p> |

| No. | Issue | Opportunity |
|-------|---|---|
| | <ul style="list-style-type: none"> • Barre Warre
Rating: Easy Distance: 2kms • Vista
Rating: More Difficult Distance: 5.5kms • Tiger Loop
Rating: More Difficult Distance: 3kms <p>Refer Attachment 1. Forrest and Yaugher MTB Trail Network Map</p> | |
| 3.3.2 | <p>The Yaugher Trail Head is signed along Birregurra -Forrest Road and is located off Boundary Road. The car park is signed using a standard DEPI location and directional sign.</p> <p>Facilities at the trail head include an unsealed car park area, standard DEPI (drop) public toilet and a few picnic tables.</p> <p>No water or MTB wash down facilities are currently available.</p> <p>At the beginning of the trails is a large display map of the Yaugher trail network which includes a description of the types of trails, distance of trail and a difficulty rating. Other DEPI information is often attached to the sign informing visitors of fire risks, track works, etc.</p> | |
| 3.3.3 | <p>New Trail Rationalisation</p> <p>Sections of trails to the outer fringe of the Yaugher MTB trail network have been created by DEPI for use as fire breaks and management vehicle tracks.</p> <p>These fire break tracks are also often utilised by the local horse riding club.</p> | <p>Remove dedicated MTB activities from the lesser used sections of trails; e.g. Tiger Loop, Barre Warre and or Vista and sign as dedicated horse trail.</p> |
| 3.3.4 | <p>The existing trail signage utilises timber posts with directional arrows, numbering and pictorial system. The trail map readily available online or from the Corner Store allows the visitor to easily read the directional signage and navigate the trails. It is assumed without a copy of the trail map on hand navigating the trails could be challenging.</p> | <p>Ensure trail grading signage (e.g. easiest to extremely difficult) is appropriately identified at all entry points to each trail, to ensure riders are informed of the trail grading before commencing on a section of trail.</p> <p>Install additional sign bollards to clearly denote trail entries.</p> |
| 3.4 | <p>Tiger Rail Trail (link between town and Yaugher MTB Trails)</p> | |
| 3.4.1 | <p>Birregurra- Forrest Road crossing</p> <p>The point at which MTB cyclists and pedestrians currently cross the Birregurra-Forrest Road (to link to Boundary Road and access the Yaugher trail network), is at the</p> | <p>It was agreed with COS Traffic Engineer that the Forrest MTB Strategic project is to recommend that a pedestrian/cycle tunnel underpass be investigated to provide a dedicated, safe crossing point</p> |

| No. | Issue | Opportunity |
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| | <p>apex of a curve and the road is currently designated at 100kms/hr.</p> <p>Following a meeting on site with COS Traffic Engineer Officer, it was agreed that the crossing of Birregurra-Forrest Road presents significant safety crossing challenges and risks for both MTB cyclists and pedestrians. This has also been already highlighted in community consultation and feedback from Forrest Event organisers.</p> | <p>for all users.</p> |
| 3.4.2 | <p>Boundary Road alignment
For MTB riders travelling from Forrest to the Yaugher MTB trail network, cyclists must travel along Boundary Road (an unsealed roadway shared with vehicle traffic).</p> | <p>This is not the ideal MTB visitor experience and the shared use presents safety risks to families with young children riding, visitors who may have limited cycling skills, and to vehicles as well. Investigate a new off road track alignment (to the west of Boundary Road).</p> |
| 3.5 | <p>Relevant directions from current plans</p> | |
| 3.5.1 | <p>Multiple commitments to trail upgrading
There are a number of recommendations within current plans directly focusing on improving and developing more mountain bike routes and associated experiences, facilities, services, events and promotion across the Region. Potential routes proposed included:</p> <ol style="list-style-type: none"> 1. A collection of half-day mountain bike routes along existing unsealed roads, designed to reflect varying levels of difficulty and remoteness 2. A much more challenging and specialised mountain biking experience 3. A two-day road tour circuit out of Colac that utilises the Old Beechy Line Rail Trail 4. Improvements to trails around Stevenson's Falls and Lake Elizabeth that include mountain biking options 5. A new Rail Trail between Forrest and Birregurra | <p>It is unlikely that every proposed new route / experience should or would be implemented. The development of mountain biking within the Forest area should be done within the context of regional mountain bike development initiatives identified. This means checking to determine the status and likely implementation of these initiatives, followed by consideration of the Forrest initiative in the context of those implemented or likely to be implemented. The goal should be to create differentiation and avoid unnecessary and disadvantageous duplication.</p> |
| 3.5.2 | <p>Recommended mountain bike facilities and services
In previous plans, a number of recommendations have been made to improve mountain bike facilities and services within Forrest, including:</p> | <p>The following recommendations should be considered for this Project:</p> <ol style="list-style-type: none"> 1. Town based equipment hire and tour groups to support tourist recreational needs 2. A mountain bike sales, hire and |

| No. | Issue | Opportunity |
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| | Mountain bike services | <p>repair store</p> <p>3. Mountain bike racks near commercial businesses and lockable storage space at the mountain trail heads.</p> <p>4. Public showers and lockers in association with existing public toilet and picnic area and at the mountain bike trail heads locations.</p> |
| 3.5.3 | <p>Recommended brand for region</p> <p>One target market for Forrest has been set by the Otway's Public Lands Tourism Plan using the Roy Morgan Values segment known as Socially Aware</p> | The regional brand should be taken into account for this MTB Project. The brand provides a useful frame of reference for considering what is and is not appropriate in new products and promotion |
| 3.5.4 | <p>Recommended target markets for region</p> <p>A regional brand / positioning has been set for the region</p> | The regional target markets should be taken into account when setting mountain bike development and marketing initiatives for this MTB Project |
| 3.6 | Township Facilities and Purchasing Provisions | |
| 3.6.1 | <p>There are four key food and beverage location within Forrest (within certain operational hours/season basis)</p> <ul style="list-style-type: none"> - Forrest Brewery - The Wonky Donkey - The Corner Store - Forrest Country Guest House | Colac Otway Shire to continue to support local business opportunities. |
| 3.6.2 | <p>The ability to purchase general provisions in Forrest township is very limited. The General Store does stock limited milk, bread, etc</p> <p>General provisions are generally purchased prior to staying overnight at Forrest.</p> <p>There is no bakery in town. Take away coffee is limited to the Brewery and Corner Store.</p> | Colac Otway Shire to explore opportunities to improve provisions in township. |
| 3.7 | Accommodation | |
| 3.7.1 | <p>There is a range of accommodation types available in Forrest including camping, B&B's, full service Guest House, secluded self-contained cabins and houses with basic amenities that cater for small to large groups.</p> <p>The caravan park is well sited and provides excellent links to the adjoining open space of the township with tennis courts, playground.</p> | <p>Colac Otway Shire to continue to support local accommodation development opportunities.</p> <p>While awaiting development of new accommodation, a number of accommodation places could be refurbished and updated to fit unmet target market demand.</p> |

| No. | Issue | Opportunity |
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| | There is insufficient accommodation during peak holiday periods and events. | |
| 3.7.2 | There is unmet demand for ecolodge accommodation within the region | Lobby for the development of an ecolodge within 20 minutes drive of Forrest |
| 3.8 | Events | |
| 3.8.1 | <p>There are a number of recreational, MTB, food and wine and tourism events held within the township of Forrest, these include;</p> <ul style="list-style-type: none"> • Forrest Festival (MTB event Nov/Dec 2013) • Giant Odyssey (MTB Marathon event April 2014) • Run Forrest (Marathon event June 2014) | <p>The events held in Forrest are generally perceived as well managed and provide positive social and economic impacts for the town and surrounding Colac Otway region.</p> <p>COS to continue to work with the existing event operators to continue to improve the management of the events and further enhance the visitor (and local community) experience during the event.</p> <p>COS should also look to expand events held in Forrest to provide additional tourism drivers for the town from other recreational and food & wine events.</p> |
| 3.8.2 | <p>Rapid Ascent are the event operator's and managers who host the 100km Otway Odyssey (previous title) titled Giant Odyssey for 2014 event in Forrest.</p> <p>This is a large scale event which attracts approximately 20,000 (tbc) visitors to the town and region.</p> <p>The MTB riders stay within accommodation in town, at the caravan park and a large number camp out at the Forrest Oval (north of the township) where all toilets, shower, food, merchandise stalls and other amenities are provided by club groups /volunteers.</p> <p>The event is generally perceived (by locals and the Shire) as a positive social and economic driver for the town. However the following issues have been identified which negatively impact on the MTB visitor experience and on local community during the event. These include;</p> <ul style="list-style-type: none"> • Lack of adequate public toilet capacity in Forrest Township. | <p>COS to work with Rapid Ascent to continue to promote and achieve the objectives of the event (as outlined by Rapid Ascent) including;</p> <ul style="list-style-type: none"> • To showcase the spectacular natural features of the region. • To educate the general public (particularly youth) on the benefits of being fit and healthy. • To educate people on the vast amount of options for enjoying off road leisure and sports activities in Australia's National Parks. • To promote the importance of environmental conservation and protection of our Parks for the enjoyment of future generations. • To assist local communities (Forrest & surrounds) to grow and develop. |

| No. | Issue | Opportunity |
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| | <ul style="list-style-type: none"> Closure of the Birregurra-Forrest Road. Impact on the condition of MTB trails with no financial compensation to assist with the maintenance and repair following the event. | |
| 3.9 | Competitive position | |
| 3.9.1 | <p>Competitor product</p> <p>The Alpine mountain resorts have several strategic advantages over Forrest as a mountain bike destination, and they have subsequently built highly competitive propositions.</p> <p>There is significant diversity in product (including grades, technical skills and landscape). These destinations don't just provide lots of trails, they provide lots of differentiated trails.</p> <p>There is also much value adding of the base trail product, including guided tours, camps, events and ancillary services, including bike hire of different types of bikes.</p> | <p>See Opportunity 3.5.1</p> <p>When planning any track reinvigoration, consider that single tracks that function as an integrated set of cascading and connected loop circuits present more powerfully than unconnected networks, and they present more powerfully than one way routes requiring the rider to turn around and ride the same route back to the start back to the start.</p> <p>Successful destinations offer the majority of their product in the intermediate to advanced skill markets, but manage to attract wider markets by having a minority of trails set aside for beginners / skill development / low fitness and / or deviations from the main route that present some interest and respite before returning to the challenging main route.</p> <p>The customer is looking for a seamless transition between where they stay and where they ride, this can be achieved by:</p> <ul style="list-style-type: none"> Having trails lead off from the accommodation node area; and / or Providing transport between the track and accommodation (be it a chairlift or a shuttle service) <p>Beginner and skill development trails could be designed to include rest / picnic areas for accompanying friends and family to watch and photograph them in a picturesque and slightly exciting section of the trail</p> <p>There is an opportunity to design trails to feature more of the unique features and diversity in a landscape, and integrate more character in the build and technical elements. There is also an opportunity to name trails to reflect this character and some degree of challenge within it, such</p> |

| No. | Issue | Opportunity |
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| | | as at Forrest.
The hiring of quality mountain bikes close to the trails is almost essential, and the hiring of different types of bikes and good quality equipment is moving towards being expected |
| 3.9.2 | Competitor support services
The support services and infrastructure (carparks, accommodation, food and beverage, hire and retail) is typically based close to the trail starting / finishing point, making the experience simple, convenient and efficient. In addition, the scale of these facilities make it easy for an event to service most if not all of the participants 'on site' and close together – further enhancing the atmosphere and effectiveness | Determine an optimum and maximum scale for the destination to operate at, at any one time, and design supporting infrastructure and services to this.
See Opportunity 3.5.2 |
| 3.9.3 | Competitor marketing
Marketing done by alpine resort mountain bike destination managers is significantly superior to most of the non-commercial sites, evidenced by superior imagery of the experience and landscape, use of well produced videos and effective use of social media and events to drive brand awareness.

Logistical information was also superior, such as the way trails were presented on interactive maps and the way they were profiled so the customer could easily compare and choose. These destinations do not shirk from pricing and present comprehensive pricing information with competition profiled and opportunities for discounts available. | Positioning the mountain biking destination should: <ul style="list-style-type: none"> • lead with its signature trails and place supporting product behind this, in less detail; • include accommodation and food and beverage that matches the needs of the target market supported by web based information • promote the genuine country ambience of the Forrest township experience as a destination value add for riders and visitors <p>Within a network of trails there should be a lead experience that brands the overall destination and is used to generate the main awareness and motivation to visit.</p> |
| 3.10 | Marketing | |
| 3.10.1 | Target markets
Most destinations are targeting intermediate to advanced riders under 35 years of age, almost always with pictures of male riders. Only one destination offered anything to target women (skill improvement clinics). Skill development trails are under marketed. | See Opportunity 3.5.4

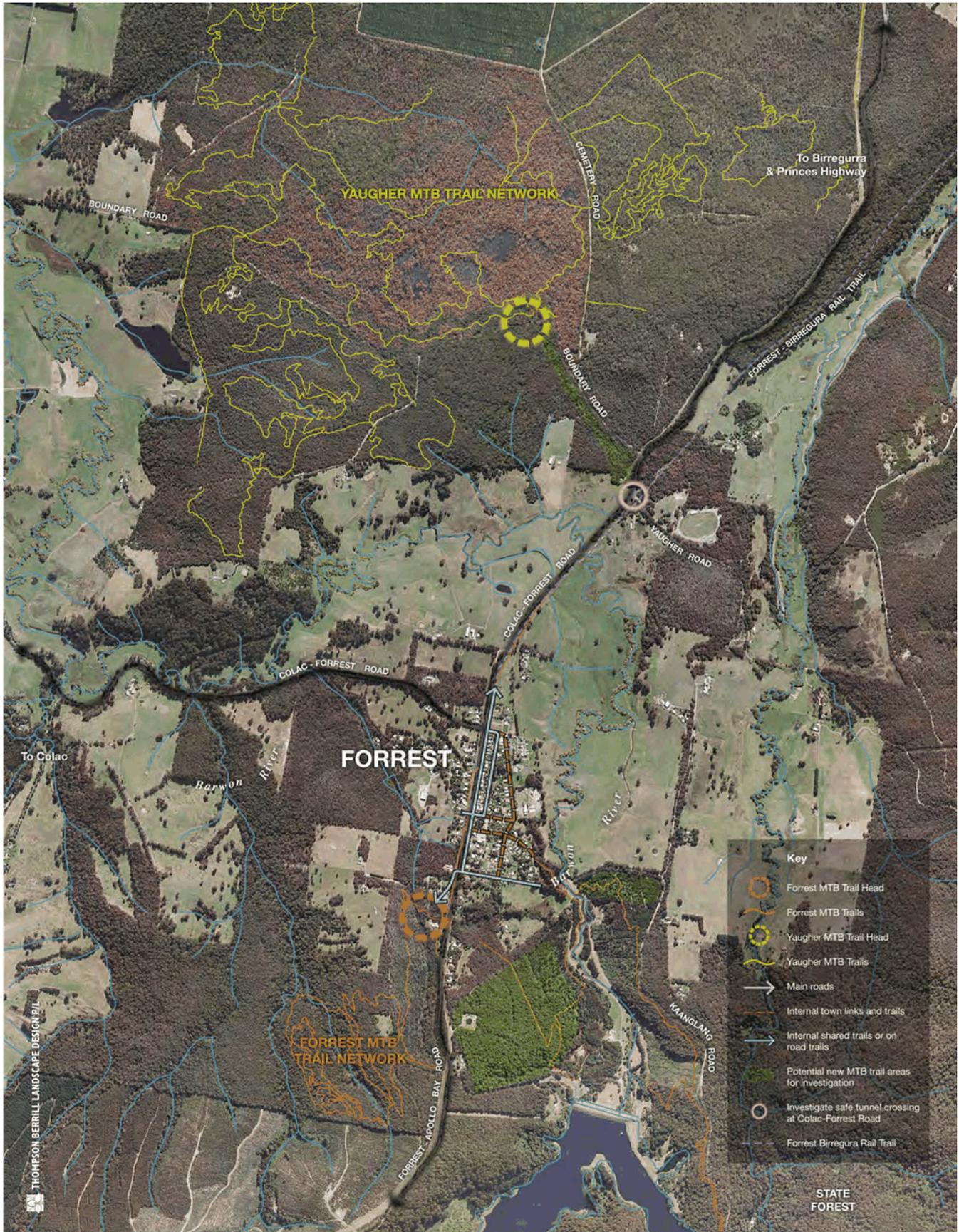
There is an opportunity to grow visitation by designing trails and marketing to target women (via skill improvement clinics) and general skill development via skill development trails |
| 3.10.2 | Positioning
The mountain bike destination is positioned as challenging – the majority of trails are moderate to advanced, with only 3 out of 16 trail designated as easy. | See Opportunity 3.5.3

After reinvigorating the mountain bike experience develop a brand for the destination and clarify and strengthen the way it is positioned. |

| No. | Issue | Opportunity |
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| | <p>There are few examples of destinations describing their trails beyond the logistical elements (distance, time, gradient and difficulty).</p> | <p>There is an opportunity to design trails to feature more of the features and diversity in a landscape, and more character in the build and technical elements. There is also an opportunity to name trails to reflect this character and some degree of challenge within it, such as there is at Forrest.</p> |
| 3.10.3 | <p>Promotion
Marketing of Forrest as a mountain biking destination is largely done through the website (http://www.rideforrest.com.au) that blends the township of Forrest with the bike riding opportunities. The website includes some You Tube videos showing intermediate bikers on some sections of the trail doing small jumps and fast turns, a short a description for each route and its level of difficulty, whether the trails are open or closed, and a map of the region trails that can be downloaded
There is no social media used on the main website</p> | <p>After reinvigorating the mountain bike experience:</p> <ul style="list-style-type: none"> • generate a set of quality images and videos with soundtracks for the most competitive mountain bike experiences, and allocate responsibility to someone to regularly refresh them • establish a Facebook site Twitter feed, generate with regular stories and allocate responsibility to someone to establish viral campaigning to drive use • introduce a series of challenges that attracts visitors to do all the trails or come back several times within a given period, positioned as a competition with a prize <p>The Colac Otway Shire to:</p> <ul style="list-style-type: none"> • Define and promote Forrest as a MTB tourism destination • Recognise and promote MTB as an important activity - social and health benefits • Promote MTB as an all ages and multi skill activity for visitors • Support and encourage sustainable/manageable MTB events in Forrest |
| 3.10.4 | <p>Customer feedback
TripAdvisor.com.au has two reviews of Excellent and Very Good rating, but feedback suggests the trails are looking tired and need reinvigoration</p> | <p>After reinvigorating the mountain bike experience campaign for more reviews on TripAdvisor to lift the overall amount of positive feedback</p> |
| 3.11 | <p>Future Growth and Development of Township as a MTB Destination</p> | |
| 3.11.1 | <ul style="list-style-type: none"> • Understanding who the MTB visitor is • Understanding what the MTB product is and where it fits in the market | <ul style="list-style-type: none"> • Define and promote Forrest as a MTB tourism destination • Providing a greater experience for |

| No. | Issue | Opportunity |
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| | <ul style="list-style-type: none"> Define and maintain the 'point of difference' of Forrest MTB | <p>MTB visitors</p> <ul style="list-style-type: none"> Support and encourage sustainable/manageable MTB events in Forrest Identify investment opportunities that support compatible future direction for Forrest Recognise and promote MTB as an important activity - social and health benefits Promote MTB as an all ages and multi skill activity for visitors Promote tourism dispersal from Forrest into the region (economic regional growth) Identify appropriate higher level marketing opportunities Attract mid to high income tourists |
| 3.12 | Environmental, Historic & Cultural Values | |
| 3.12.1 | <p>Environmental Values</p> <p>The West Barwon River flows through the Forrest township and it is near the West Barwon reservoir, which services Geelong.</p> | <p>As the West Barwon Reservoir is a service reservoir for Geelong, public interaction with the reservoir is prohibited, therefore currently limiting recreational access on the reservoir. In the future if the reservoir ceased to service Geelong, recreational activities on the water body could potentially be allowed. Subject to Barwon Water approval.</p> |
| 3.12.2 | <p>Historic and Cultural Values</p> <p>The visual aesthetic of the township and local community of Forrest have preserved their historic and cultural values from the timber industry.</p> <p>The railway to Forrest was opened in 1891 and closed in 1957, as a branch line from Birregurra. It serviced the townships of Deans Marsh, Maroon, Barwon Downs, Yaugher and Forrest. At Forrest, numerous tramways ran off into the nearby bush. These lines were used to bring the freshly cut timber to the rail head, and the associated sawmills of which there were 4 in Forrest and nearby Yaugher. All of these mills are now closed.</p> <p>Originally called Yaugher, the name was changed to Forrest when the township was established.</p> | <p>Identify heritage and cultural values to protect and integrate into future town planning and design.</p> <p>Undertake a Forrest Master Plan to identify and guide compatible development direction with the MTB industry.</p> |

| No. | Issue | Opportunity |
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| 3.13 | Land Ownership, Land Management and Maintenance | |
| 3.13.1 | <p>Land Ownership</p> <p>The Forrest Mountain Bike Trails are located on Crown land to the south and north of the township. DEPI maintain over 3 land tenures (DEPI, BW, PV).</p> | Noted. |
| 3.13.2 | <p>Maintenance</p> <p>DEPI has responsibility for maintenance, signage, and risk management of approximately 65kms of MTB trails, with 10 field staff at the Forrest depot.</p> <p>DEPI convenes the Forrest Mountain Bike Trails Committee, a stakeholder committee to coordinate operational matters in respect to maintenance, erosion control, alignment and possible new trails. It works closely with the Forrest Mountain Bike and Cycling Club and organises the Forrest Mountain Bike Club Volunteer work program.</p> <p>During the summer period all DEPI staff is on fire watch, resulting in limited to no DEPI resources or trail maintenance during this period.</p> <p>DEPI manage the trails to a reasonably high standard, giving the lack of resources and funding for MTB trail works.</p> <p>A trails audit was undertaken in 2010 to identify future upgrade and works required to the MTB trails. This audit document has been DEPI's works program.</p> | <p>During the consultation phase, the opportunity to develop an Interagency Management Plan was identified and discussed.</p> <p>It was noted that the regional Council's and township's have sparse budgets with little financial resources. Colac Otway Shire could not fund a specific trail officer to manage and maintain the trails.</p> <p>A potential 'Advisory' committee is to be further investigated to develop an interagency management plan at a strategic level from government agencies.</p> |
| 3.14 | Resources and Funding | |
| 3.14.1 | <p>Resources and Funding</p> <p>Limited resources for maintenance and management of MTB trails.</p> | <p>Consider financial contributions from event organisers to assist in the maintenance and management of the MTB trails.</p> <p>Investigate potential opportunity for corporate sponsors</p> <p>Investigate potential opportunity for creating funding streams, e.g. donations from trail users to contribute to the maintenance of the trails and upgrade of MTB visitor infrastructure at trail heads.</p> |



ISSUES AND OPPORTUNITIES PLAN FORREST MTB STRATEGIC TRAILS PLAN

DWG No. FMTB-02

FEB 2014

SCALE: tbc



16 trails showcasing the natural beauty of the Otway Ranges



Forrest Mountain Bike Trails

TRAIL SIGN KEY

- Right turn
- ← Left turn
- ↑ Straight ahead
- ↓ Straight ahead
- ↕ Two way
- ⚠ Caution
- ⚠ Extreme Caution
- ✘ Wrong way
- 11 Bridge ahead

www.rideforrest.com.au




1 Forrest loop

RATING: Easy
DISTANCE: 3Km

The Forrest Town Loop is a dual use trail for walkers and cyclists of all abilities. It has long flowing corners that descend to the West Barwon valley and then climbs back to Forrest where it joins the Forrest Heritage Trail.

2 Barlidjaru

RATING: More difficult
DISTANCE: 6Km

The local indigenous word for Platypus is Barlidjaru, (*bar-lid-ja-ru*). This trail links Forrest to Lake Elizabeth and climbs most of the way to the Lake Elizabeth carpark. You can leave your bike here and enjoy a walk to the Lake and maybe catch a glimpse of one of the lake's resident platypus.

3 Red Carpet

RATING: More difficult
DISTANCE: 4.5Km

The Red Carpet is a single track return trail from Lake Elizabeth. This trail flows through tall Eucalypt forests featuring fun, challenging descents and tight technical climbs for an exciting return option to Forrest.

4 Roller Coaster

RATING: Easy
DISTANCE: 2Km

Connecting to the main Forrest Mountain Bike Trails trailhead, Roller Coaster takes you on a wide, smooth and free flowing trail. With no obstacles it is ideal for beginners or a quick warm up lap before heading out on a more extended ride.

5 Third time lucky

RATING: More difficult
DISTANCE: 2Km

Third Time Lucky has plenty of trail features including bermed corners and jumps. This is an excellent trail to learn the different skills needed for mountain biking while enjoying the thrilling sensation of riding through the bush.

6 Follow the dog

RATING: More difficult
DISTANCE: 4.5Km

This free flowing trail through tall open eucalypt forest and dense fern gullies offers extended riding with loads of trail features along the way. Long climbs and fast flowing descents characterise this exciting trail.

7 Yaugher Super Loop

RATING: More difficult
DISTANCE: 11.5Km

The Yaugher Super Loop provides access to the entire Yaugher trail network. It is a mixture of single track and fire trails with a full range of trail features all with A and B lines.

8 Grasstrees

RATING: Very difficult
DISTANCE: 1.5Km

Grasstrees packs enough technical features and variety to challenge all riders. Extensive trail features include log rides, rollovers, step ups, drop offs, big 4X berms and jumps.

9 Marriners Run

RATING: Very difficult
DISTANCE: 4.5Km

With long climbs and fast flowing descents, this trail will test your limits. The middle section has enough technical features to pack a punch for the most experienced rider.

10 Foxtail

RATING: More difficult
DISTANCE: 4.5Km

Long fast straights and open corners provide a fun flowing ride with well spaced technical features. This trail takes you from the Yaugher trailhead to Tabletop and on to join the Yaugher Super Loop.

11 J2

RATING: More difficult
DISTANCE: 4Km

Another long flowing descent trail from the Yaugher trailhead. This trail has plenty of features along the way and joins the Yaugher Super Loop for a quick lap.

12 Yo Yo

RATING: Very difficult
DISTANCE: 3Km

A tight and technical trail that as the name suggests goes up and down to test your legs, lungs and skills.

13 Casper Black

RATING: Very difficult
DISTANCE: 5.5Km

Twisting technical climbs and descents with walls of tea tree characterise this tough and challenging trail.

14 Barre Warre

RATING: Easy
DISTANCE: 2Km

The Barwon River takes its name from the local indigenous word Barre Warre (*Bar-rah War-rah*) meaning from hills to sea. This high vantage point looks down on the Barwon River valley taking in the view back over the Forrest Township. A good warm up or warm down ride.

15 Vista

RATING: More difficult
DISTANCE: 5.5Km

This long flowing trail has some great fire trail descents and climbs with rewarding valley views. The southern loop has some tough technical climbing thrown in for good measure.

16 Tiger Loop

RATING: More difficult
DISTANCE: 3Km

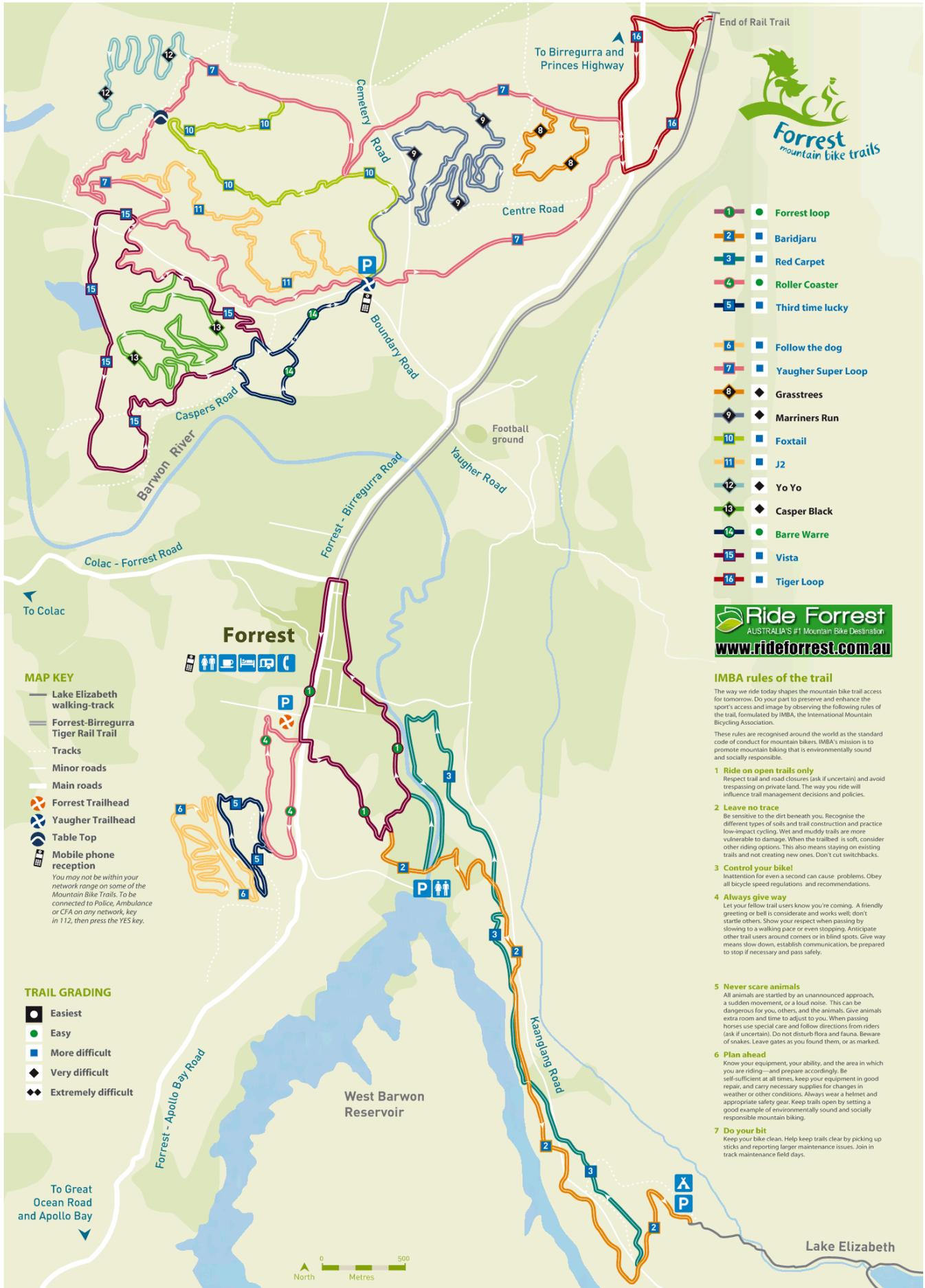
Provides a link from the Forrest-Birregurra Tiger Rail Trail to the Yaugher Trails network.

Designed around the village of Forrest, these trails showcase the natural beauty of the Otway Ranges, passing through tall eucalypt forests, dry heathy scrub and dense fern gullies.

The trails have been built with real mountain bikers in mind and offer experiences for beginners and the most hard-core dirt fanatics, choose a trail that suits you.



You may not be within your network range on some of the Mountain Bike Trails. To be connected to Police, Ambulance or CFA on any network, key in 112, then press the YES key.

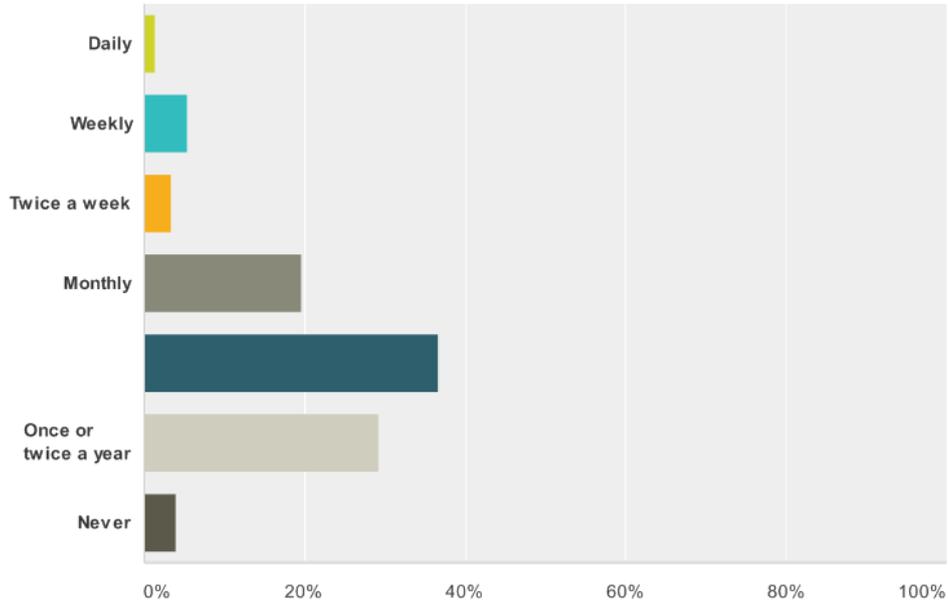


Attachment 2.
Forrest Online Survey Summary Report

Forrest MTB Strategic Plan Survey

Q1 How frequently do you visit Forrest and ride the MTB trails? (Please tick appropriate box)

Answered: 147 Skipped: 2

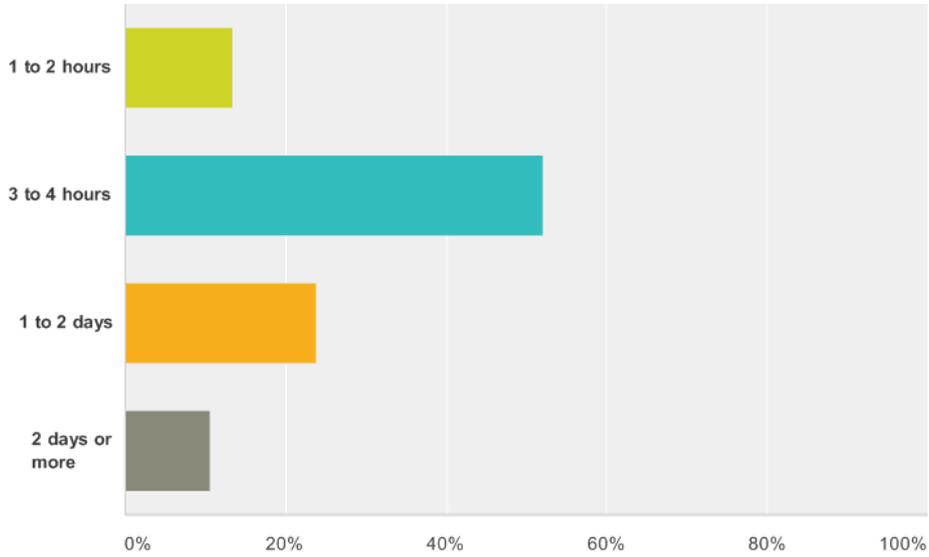


| Answer Choices | Responses |
|----------------------------|------------|
| Daily | 1.36% 2 |
| Weekly | 5.44% 8 |
| Twice a week | 3.40% 5 |
| Monthly | 19.73% 29 |
| Three or four times a year | 36.73% 54 |
| Once or twice a year | 29.25% 43 |
| Never | 4.08% 6 |
| Total | 147 |

Forrest MTB Strategic Plan Survey

Q2 How long do you typically spend out on the Forrest MTB trails? (Please tick appropriate box)

Answered: 142 Skipped: 7

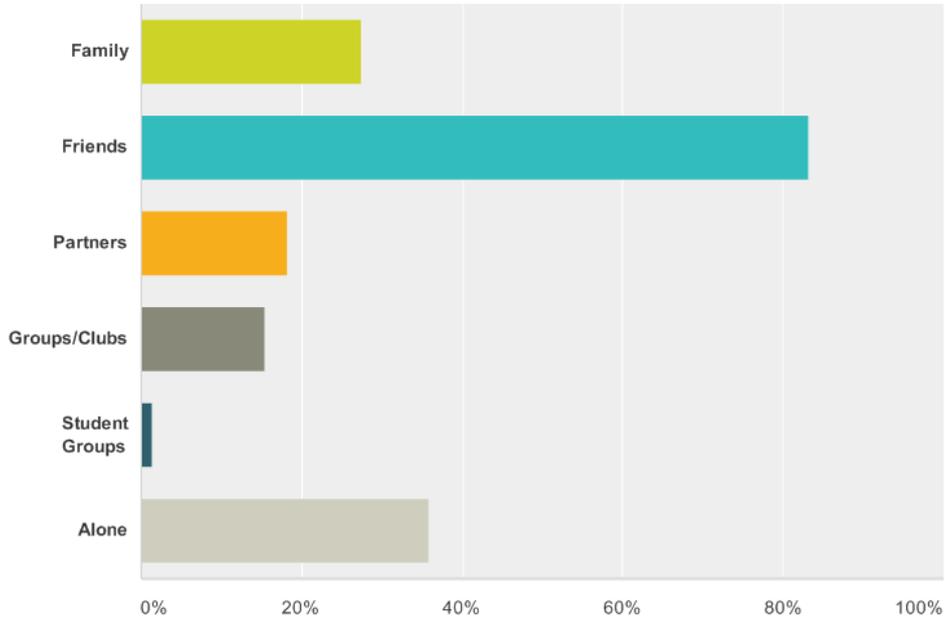


| Answer Choices | Responses | |
|----------------|-----------|------------|
| 1 to 2 hours | 13.38% | 19 |
| 3 to 4 hours | 52.11% | 74 |
| 1 to 2 days | 23.94% | 34 |
| 2 days or more | 10.56% | 15 |
| Total | | 142 |

Forrest MTB Strategic Plan Survey

Q3 Who do you ride the Forrest MTB trails with? (Please tick appropriate boxes - you can tick more than one box)

Answered: 142 Skipped: 7

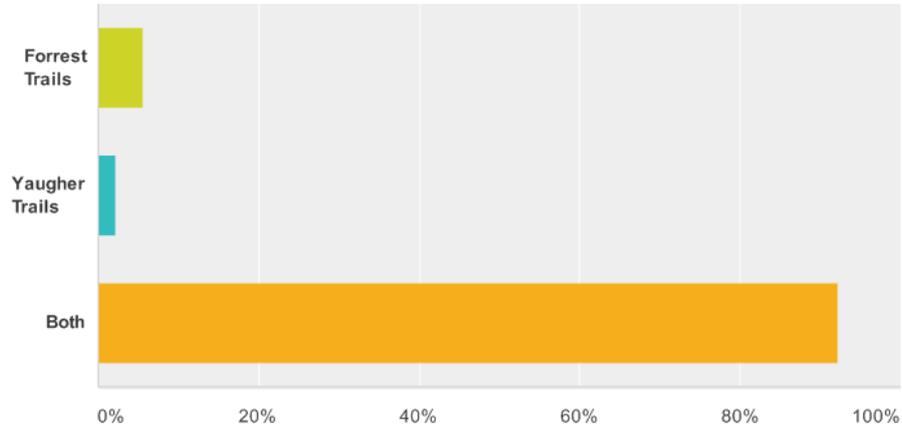


| Answer Choices | Responses | Count |
|-------------------------------|-----------|-------|
| Family | 27.46% | 39 |
| Friends | 83.10% | 118 |
| Partners | 18.31% | 26 |
| Groups/Clubs | 15.49% | 22 |
| Student Groups | 1.41% | 2 |
| Alone | 35.92% | 51 |
| Total Respondents: 142 | | |

Forrest MTB Strategic Plan Survey

Q4 Do you like the Forrest MTB trails (near Town), the Yaugher MTB trails (2km north of Town) or both? (Please tick one box)

Answered: 142 Skipped: 7

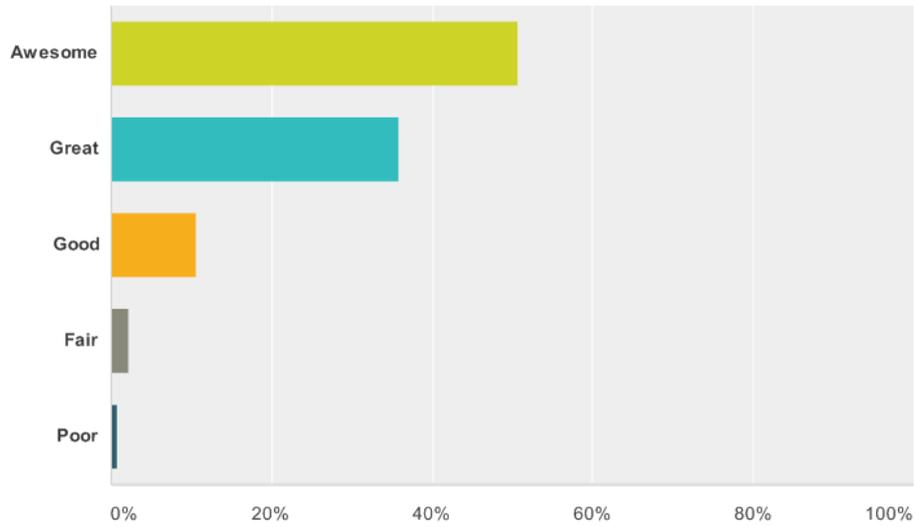


| Answer Choices | Responses |
|----------------|------------|
| Forrest Trails | 5.63% 8 |
| Yaugher Trails | 2.11% 3 |
| Both | 92.25% 131 |
| Total | 142 |

Forrest MTB Strategic Plan Survey

Q5 Please rate your experience of the Forrest MTB Trails. (Please tick one box)

Answered: 142 Skipped: 7

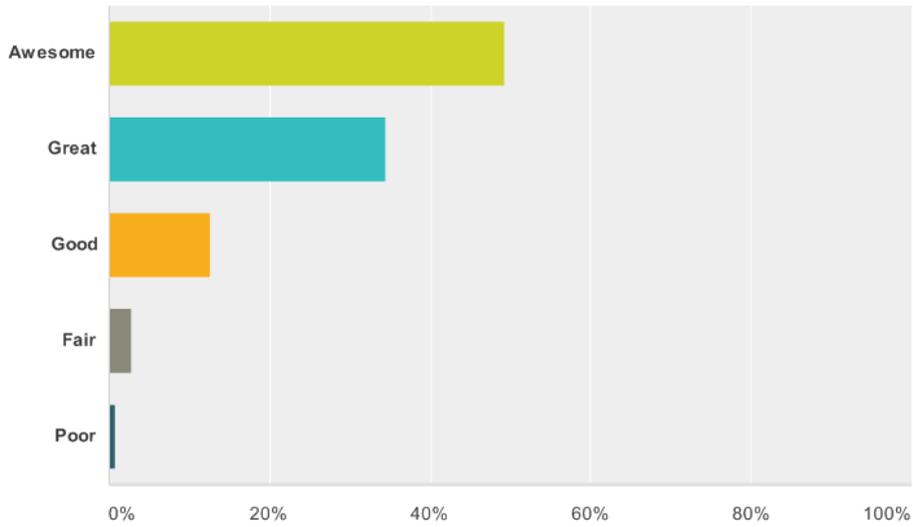


| Answer Choices | Responses | Count |
|----------------|-----------|------------|
| Awesome | 50.70% | 72 |
| Great | 35.92% | 51 |
| Good | 10.56% | 15 |
| Fair | 2.11% | 3 |
| Poor | 0.70% | 1 |
| Total | | 142 |

Forrest MTB Strategic Plan Survey

Q6 Please rate your experience of the Yaugher MTB trails. (Please tick one box)

Answered: 142 Skipped: 7

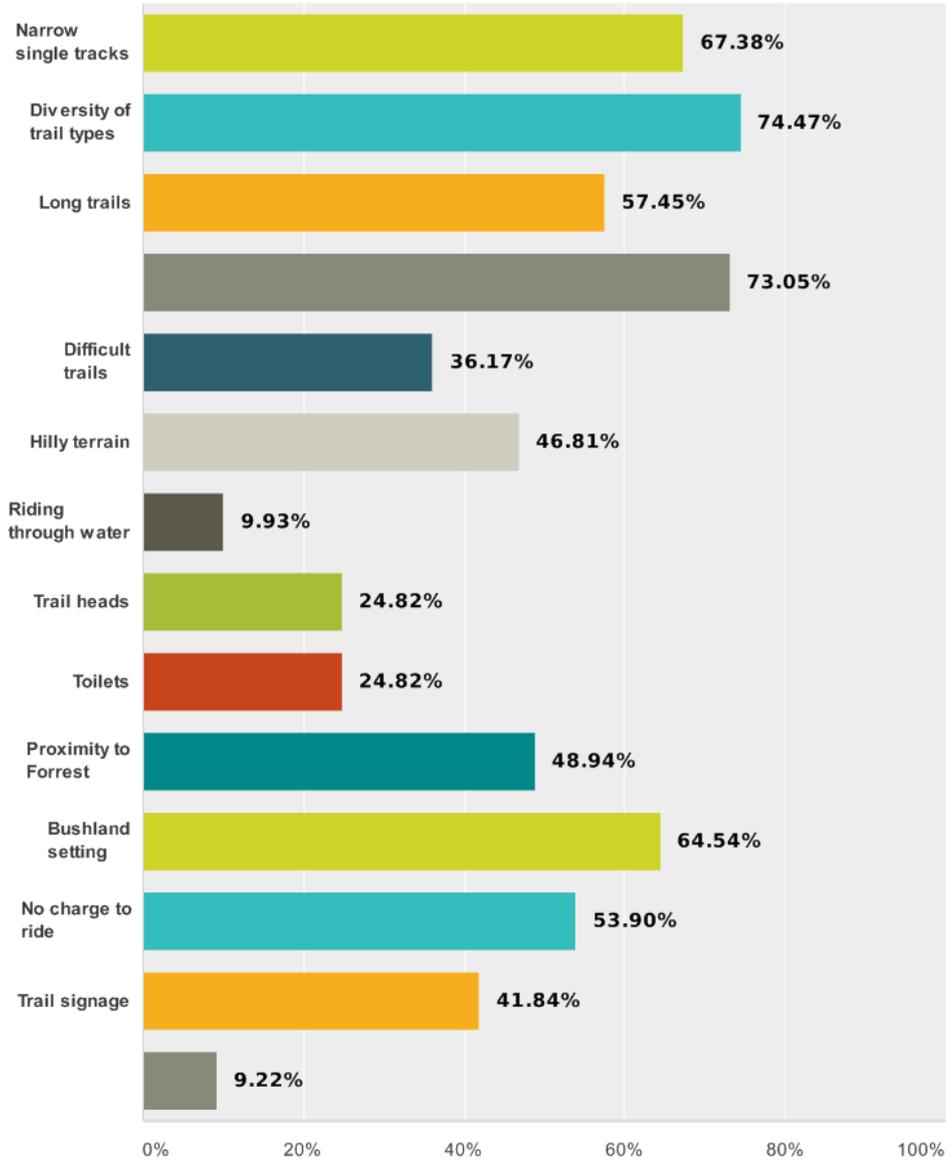


| Answer Choices | Responses | Count |
|----------------|-----------|------------|
| Awesome | 49.30% | 70 |
| Great | 34.51% | 49 |
| Good | 12.68% | 18 |
| Fair | 2.82% | 4 |
| Poor | 0.70% | 1 |
| Total | | 142 |

Forrest MTB Strategic Plan Survey

Q7 What do you like about the Forrest and Yaugher MTB trail network? (Please tick appropriate boxes - you can tick more than one box)

Answered: 141 Skipped: 8



| Answer Choices | Responses | Count |
|-------------------------------|-----------|-------|
| Narrow single tracks | 67.38% | 95 |
| Diversity of trail types | 74.47% | 105 |
| Long trails | 57.45% | 81 |
| Technical trails (bems/woops) | 73.05% | 103 |

Forrest MTB Strategic Plan Survey

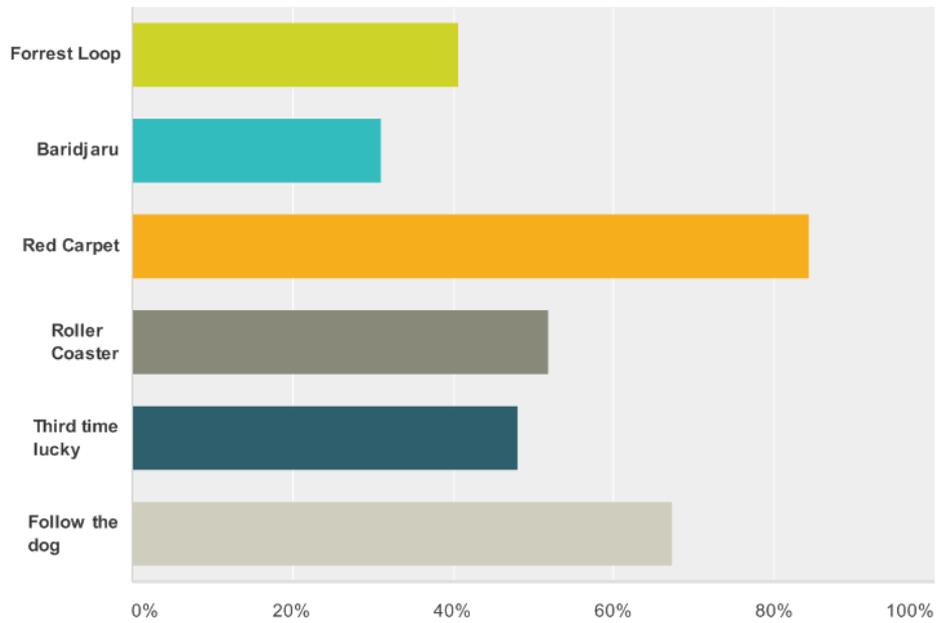
| | | |
|-------------------------------|--------|----|
| Difficult trails | 36.17% | 51 |
| Hilly terrain | 46.81% | 66 |
| Riding through water | 9.93% | 14 |
| Trail heads | 24.82% | 35 |
| Toilets | 24.82% | 35 |
| Proximity to Forrest | 48.94% | 69 |
| Bushland setting | 64.54% | 91 |
| No charge to ride | 53.90% | 76 |
| Trail signage | 41.84% | 59 |
| Other (please describe) | 9.22% | 13 |
| Total Respondents: 141 | | |

| # | Other (please describe) | Date |
|----|--|---------------------|
| 1 | Great XC tracks | 12/19/2013 6:43 PM |
| 2 | We love that it is safe to ride with children. Don't need to worry about cars on most of it. | 12/14/2013 6:41 AM |
| 3 | Directional | 12/5/2013 8:35 PM |
| 4 | Extended trail network never seems crowded. | 12/5/2013 12:32 PM |
| 5 | A & B lines | 12/5/2013 10:04 AM |
| 6 | love early morning rides with the local fauna, forrest has so much potential I can only see it getting better. | 11/25/2013 11:29 PM |
| 7 | Never been there before... | 11/25/2013 12:50 PM |
| 8 | Can ride for a long time without seeing other people. Not too crowded | 11/24/2013 10:15 PM |
| 9 | Great village and hospitality of the local people plus scenery. Awesome! And less than 1hr from Geelong! | 11/24/2013 8:38 PM |
| 10 | Everything is awesome | 11/24/2013 7:49 PM |
| 11 | Fast and flowing | 11/23/2013 12:15 AM |
| 12 | It's a mtb destination not just track or trail network | 11/22/2013 5:00 PM |
| 13 | Flow flow flow | 11/22/2013 4:06 PM |

Forrest MTB Strategic Plan Survey

Q8 Which trails do you like riding in Forrest? (Please tick appropriate boxes - you can tick more than one box)

Answered: 135 Skipped: 14

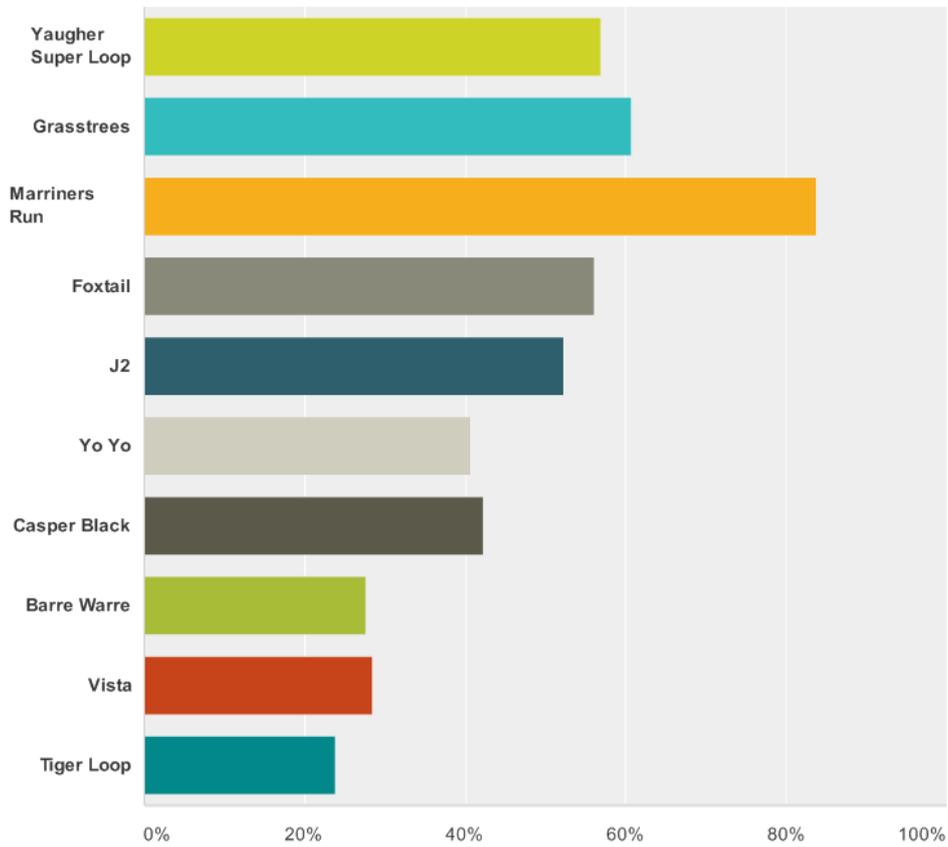


| Answer Choices | Responses | Count |
|-------------------------------|-----------|-------|
| Forrest Loop | 40.74% | 55 |
| Baridjaru | 31.11% | 42 |
| Red Carpet | 84.44% | 114 |
| Roller Coaster | 51.85% | 70 |
| Third time lucky | 48.15% | 65 |
| Follow the dog | 67.41% | 91 |
| Total Respondents: 135 | | |

Forrest MTB Strategic Plan Survey

Q9 Which trails do you like riding in Yaugher? (Please tick appropriate boxes - you can tick more than one box)

Answered: 130 Skipped: 19

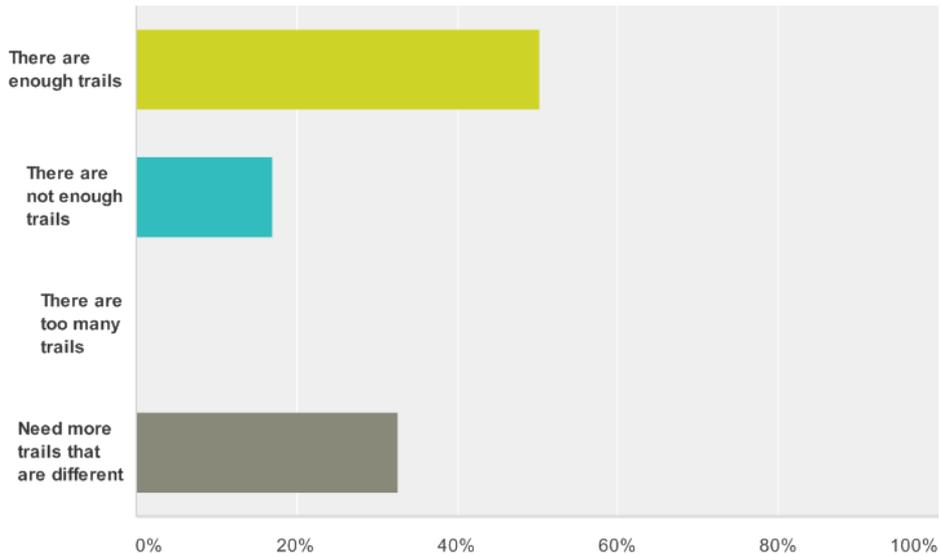


| Answer Choices | Responses |
|-------------------------------|------------|
| Yaugher Super Loop | 56.92% 74 |
| Grasstrees | 60.77% 79 |
| Marriners Run | 83.85% 109 |
| Foxtail | 56.15% 73 |
| J2 | 52.31% 68 |
| Yo Yo | 40.77% 53 |
| Casper Black | 42.31% 55 |
| Barre Warre | 27.69% 36 |
| Vista | 28.46% 37 |
| Tiger Loop | 23.85% 31 |
| Total Respondents: 130 | |

Forrest MTB Strategic Plan Survey

Q10 What is your view on the number of MTB trails available? (Please tick one box)

Answered: 135 Skipped: 14



| Answer Choices | Responses |
|-------------------------------------|------------|
| There are enough trails | 50.37% 68 |
| There are not enough trails | 17.04% 23 |
| There are too many trails | 0% 0 |
| Need more trails that are different | 32.59% 44 |
| Total | 135 |

| # | Other (please describe) | Date |
|----|---|--------------------|
| 1 | Some more trails for kids so the whole family can enjoy | 12/6/2013 1:59 PM |
| 2 | The more trails the better. | 12/5/2013 9:32 PM |
| 3 | May require an expansion of trails with a bit of difference due to the popularity of Forrest now as an mtb destination. Or just some upgrade improvement of existing system | 12/5/2013 4:48 PM |
| 4 | Great network, great place, keep building trails, more technical | 12/5/2013 3:38 PM |
| 5 | enough for 2 days riding, for a longer stay would need more. | 12/5/2013 3:02 PM |
| 6 | Could provide an off road link to Barwon Downs and/or Roadknight Creek road to facilitate XC riding on dirt roads. | 12/5/2013 12:32 PM |
| 7 | but we'd always love more ...! | 12/5/2013 11:37 AM |
| 8 | More technical trails please | 12/5/2013 10:09 AM |
| 9 | Not sure of the trail names...I just know them by number. | 12/5/2013 10:04 AM |
| 10 | But could always have more! | 12/5/2013 10:01 AM |
| 11 | Quality is more important than quantity. | 12/5/2013 9:53 AM |
| 12 | More trails would be great if possible | 11/28/2013 2:13 PM |
| 13 | Need \$\$ for maintenance of the trails & improved walk / bike trail maps | 11/26/2013 1:41 PM |

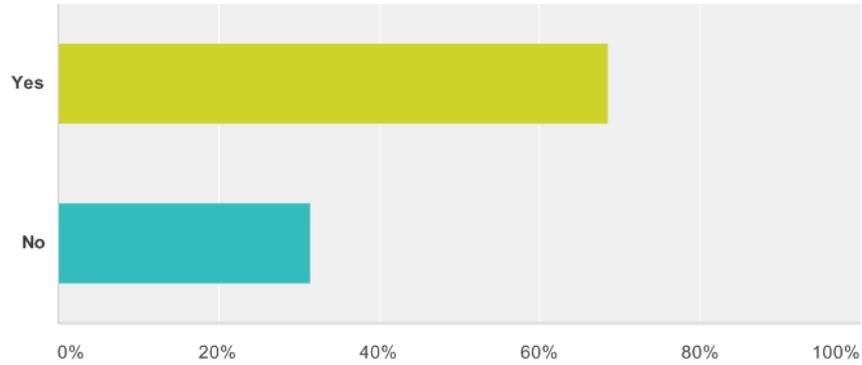
Forrest MTB Strategic Plan Survey

| | | |
|----|--|---------------------|
| 14 | There are enough, lets focus in improving the existing ones, that said i would like to something like a hiking riding trail created that woul take you along the coast to wamambool with free campsites starting in Forrest. | 11/25/2013 11:29 PM |
| 15 | There is never enough trails! although it would be good to have another long flowing downhill type trail like Red Carpet | 11/25/2013 9:57 PM |
| 16 | A few more trails with tech features like jumps, burms, rock gardens, drop offs etc would be great. | 11/25/2013 8:28 PM |
| 17 | Better quality trails are required. It's a bit dinky down there. | 11/25/2013 3:37 PM |
| 18 | currently good diversity in trails, growth of trail network will encourage more riders and keep the regulars coming back. | 11/25/2013 2:53 PM |
| 19 | there can always be more trails. more trails =more riders | 11/25/2013 1:38 PM |
| 20 | Within the super loop there is opportunity to link up more trails to provide a more dense network. Might need way markers or recommended ways to ride. More technical but natural obstacles as an A line and B line would be good. | 11/24/2013 8:38 PM |
| 21 | Perfect | 11/24/2013 7:49 PM |
| 22 | Can't have too many trails | 11/22/2013 5:00 PM |
| 23 | Need more and more variety... more trails closer to town | 11/22/2013 4:06 PM |
| 24 | Some longer trails would be great | 11/22/2013 4:01 PM |
| 25 | Great mix of trails | 11/22/2013 3:56 PM |
| 26 | Need more multi use trails for walkers and horse riders as well | 11/21/2013 7:34 PM |

Forrest MTB Strategic Plan Survey

Q11 Could improvements be made to the Forrest and Yaugher MTB trails? (Please tick appropriate box)

Answered: 124 Skipped: 25



| Answer Choices | Responses | |
|----------------|-----------|------------|
| Yes | 68.55% | 85 |
| No | 31.45% | 39 |
| Total | | 124 |

| # | Please describe why | Date |
|----|---|---------------------|
| 1 | The trail conditions are always great | 12/19/2013 6:44 PM |
| 2 | Stricter limits of competitors in organised events. The impact on the trails of number of people, greatly effect the condition of the trails. Events are great. But the number and impact is and will be seen. Single track is being widened by this impact very quickly making it not single trail. Corners and bends are being widened and rutted by inexperienced riders. The cost of maintaining true single trails and safe trails is very much impacted by numbers of riders in large events. | 12/16/2013 9:31 AM |
| 3 | We are pretty happy with what trails are there now but we think there should be plans in place to open more trails in the future. | 12/14/2013 6:43 AM |
| 4 | Could cut back the grass trees a lot more | 12/13/2013 11:51 AM |
| 5 | The issue of the die back of grass trees is a problem and also the fungus that is being transferred and killing native vegetation | 12/11/2013 4:08 PM |
| 6 | more track features - sea-saws, log rides, timber bums etc | 12/11/2013 3:45 PM |
| 7 | closure during winter/wet periods to prevent trail damage | 12/6/2013 2:20 PM |
| 8 | reduce blackberries | 12/5/2013 7:53 PM |
| 9 | > Further development of the riding areas (more trails) to add diversity > More trails like Mariners, Follow Dog, Red Carpet that are at the more technical end without being dangerous. | 12/5/2013 3:40 PM |
| 10 | Mostly around trail maintenance but this would mostly fall back to volunteers, so they can only do so much. Hard to balance sustainable trails and growth in riders and difficult weather conditions but overall trails are pretty good. | 12/5/2013 3:12 PM |
| 11 | More trails | 12/5/2013 1:12 PM |
| 12 | I think it is a very good park at the moment. | 12/5/2013 12:56 PM |
| 13 | I am scared of snakes, especially tiger snakes so I tend to come in the cooler months! | 12/5/2013 12:45 PM |
| 14 | Existing trail networks could be expanded. MTB trail links could be provided to Barwon Downs and/or Roadknight Creek Road to provide access to dirt roads for extended XC riding. | 12/5/2013 12:34 PM |

Forrest MTB Strategic Plan Survey

| | | |
|----|---|---------------------|
| 15 | Could include some more challenging jumps. Perhaps something fast and crazy like Cressy Descent at You Yangs. Could put in a downhill course. | 12/5/2013 12:14 PM |
| 16 | By developing select "all weather" trails for better enjoyment without trashing the trails during winter, and perhaps closing others for longer to protect them for the main riding in the dryer times. By installing drinking water sources at the trail heads - very frustrating not being able to refill during rides. | 12/5/2013 12:12 PM |
| 17 | Bigger range in technical difficulty. Current trails are beginner and intermediate only. | 12/5/2013 12:12 PM |
| 18 | more jumps/drops of higher technical difficulty, more with steeper gradients e.g. yoyo | 12/5/2013 11:39 AM |
| 19 | Always maintenance | 12/5/2013 11:20 AM |
| 20 | More signage | 12/5/2013 10:59 AM |
| 21 | Put in more trails, it will = more return visits for people that have to travel to the site. | 12/5/2013 10:56 AM |
| 22 | A technical linking trail from Forest to Yaugher. More technical trails is always a plus. Not necessarily slow rock gardens technical. I love the flow of trails 4,5 & 6 as a loop. | 12/5/2013 10:17 AM |
| 23 | It is pretty much perfect, if it ain't broke don't fix it. Any one who says they need more or better is living in an odd fantasy, I bet they can't cover them all at race speed in a day anyway. | 12/5/2013 10:17 AM |
| 24 | Anything can be improved. These trails are very good though. | 12/5/2013 10:10 AM |
| 25 | More advanced obstacles as an option. | 12/5/2013 10:10 AM |
| 26 | Maybe little bridges over muddy gutters. | 12/5/2013 10:03 AM |
| 27 | An extreme/harder/skill building trail, jumps, berms, etc. | 12/5/2013 9:58 AM |
| 28 | More diversity | 12/5/2013 9:58 AM |
| 29 | More trails | 11/29/2013 9:49 PM |
| 30 | Yes but i'm being picky, they are great now. | 11/28/2013 2:15 PM |
| 31 | A greater network of trails. More maintenance of trails - some trails are starting to erode and show signs of over use. | 11/27/2013 7:44 PM |
| 32 | i don't ride the trails that much so in reality I think they are fine, however if i were to ride them often I would understand how you could become bored with them. In this instance it would be good to have an ongoing program that facilitates upgrades or new trails on a regular basis. | 11/27/2013 3:28 PM |
| 33 | Yo Yo should be rideable, the ascents you have to do on foot diminish the experience. | 11/27/2013 2:30 PM |
| 34 | More diverse trail types for other riding styles not just hardcore XC. Trails that have a destination too. Riding circles in a forest is getting dull but a route to the dam and back would be good etc | 11/26/2013 9:28 PM |
| 35 | Maintenance ongoing, Walk/Bike paths for young families. | 11/26/2013 1:42 PM |
| 36 | more of them | 11/26/2013 10:41 AM |
| 37 | If there was a way to link the Southern & Yauger Trails through single / 4wd tracks around the back of town, This would be great | 11/25/2013 9:59 PM |
| 38 | clearing vegetation | 11/25/2013 8:54 PM |
| 39 | Same as last answer, a few trails with more technical features and or downhill oriented | 11/25/2013 8:34 PM |
| 40 | XC is well and truly covered. This may be a chance to develop some enduro type trails as that aspect grows, not dh, not xc but a flowy mix. | 11/25/2013 8:21 PM |
| 41 | A long all mountain enduro track and downhill track please!! | 11/25/2013 6:59 PM |
| 42 | Some boggy areas could use better sustainable approaches and maintenance. | 11/25/2013 5:54 PM |
| 43 | If you have ridden high quality trails elsewhere, you will recognise that the Forrest trails do not offer the kind of quality of machine built trails that is available in places like Mt Buller, VIC or Atherton, QLD. | 11/25/2013 3:39 PM |
| 44 | seasonal closure of wetter trails to prevent damage. | 11/25/2013 2:55 PM |
| 45 | More maintenance. Also, trail networks need to change and evolve over time or people become bored. | 11/25/2013 2:52 PM |
| 46 | Never changes apart from the wear and tear level. Got to be something new to get me to travel there again as it has not changed in the 5 years I've gone there and a bit over it now. | 11/25/2013 1:47 PM |

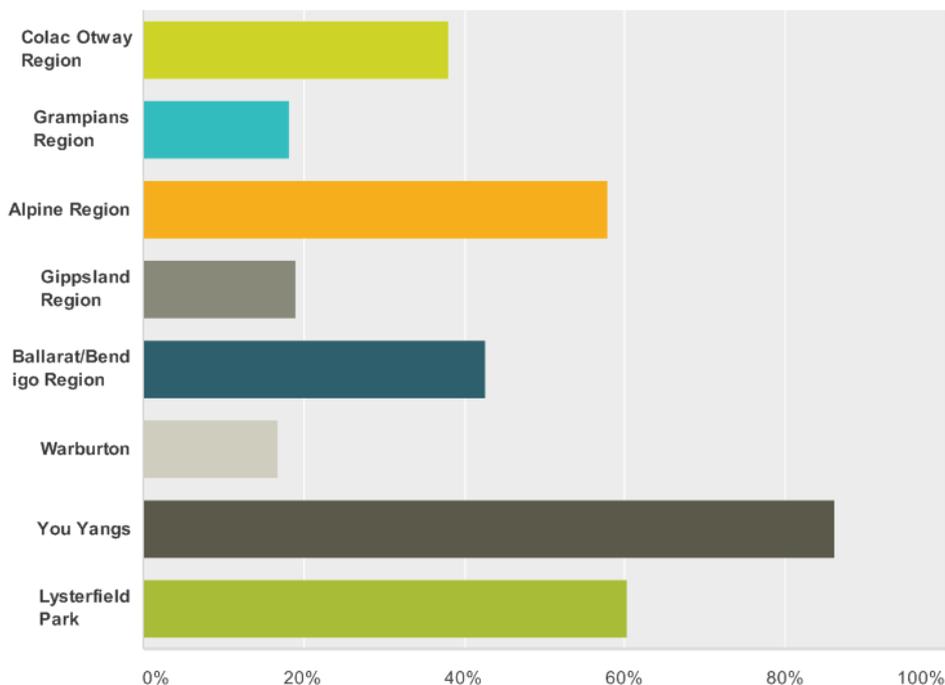
Forrest MTB Strategic Plan Survey

| | | |
|----|--|---------------------|
| 47 | Could always do with more trails. Would encourage people to stay longer (we usually do a big day trip from Melbourne and ride everything) | 11/25/2013 1:37 PM |
| 48 | Better water drainage - they do not hold rain well - and it rains a lot. | 11/25/2013 12:32 PM |
| 49 | Drainage, stonework for all round weatherproofing and added technical elements. | 11/25/2013 11:36 AM |
| 50 | Yaughter could of with more trail head facilities. Toilet is great but cover, tables chairs would help | 11/24/2013 10:18 PM |
| 51 | Maintenance of berms on mariners | 11/24/2013 9:51 PM |
| 52 | In parts be more technical eg stones and more challenging water crossings or gulleys. | 11/24/2013 8:41 PM |
| 53 | Current trail network is great, maybe some more technical trails | 11/24/2013 8:33 PM |
| 54 | All weather ability | 11/24/2013 8:08 PM |
| 55 | more diversity eg flow trail, skill park,pump track | 11/24/2013 7:54 PM |
| 56 | A bit more trail drainage and maintenance | 11/24/2013 7:49 PM |
| 57 | Ensure trails are maintained and open | 11/24/2013 7:47 PM |
| 58 | Increase technicality of some trails | 11/23/2013 6:54 PM |
| 59 | Someone with better knowledge of MTB trail maintenance should be working with DSE full time to keep on top of trail damage. | 11/23/2013 2:20 PM |
| 60 | A few more small jumps and trick areas that have ride arounds for those that want to | 11/23/2013 1:44 PM |
| 61 | Better signs, including shortcuts to trailhead (e.g. for when bike breaks down and you have to walk). | 11/23/2013 10:31 AM |
| 62 | Better wall rides, rocky terian | 11/23/2013 6:29 AM |
| 63 | Improved drainage and water maintenance would be good. | 11/23/2013 6:13 AM |
| 64 | Maybe using a different soil for rehab works, other than the 'ball bearing' type.... | 11/22/2013 9:51 PM |
| 65 | Better and more regular maintenance and ongoing development | 11/22/2013 8:37 PM |
| 66 | Signage | 11/22/2013 7:18 PM |
| 67 | Amazing experience as they are | 11/22/2013 7:07 PM |
| 68 | As a only sometimes visitor there is plenty of variety for me | 11/22/2013 5:22 PM |
| 69 | Not required but yes. Any improvement positive | 11/22/2013 5:07 PM |
| 70 | Continuing maintenance and development of existing trails rather than new. | 11/22/2013 4:47 PM |
| 71 | That yellow rock is super slippery!! | 11/22/2013 4:02 PM |
| 72 | Perhaps seasonal closure of some trails that don't cope with the winter weather as well | 11/22/2013 3:57 PM |
| 73 | Anything can always be improved on, but I'm hard pressed to come up with anything concrete... | 11/22/2013 3:36 PM |
| 74 | Better Maintenance of trails wrt drainage and wear and tear Inclusion of more features with optional "B" lines A good linking trail between Forrest town and Yaughter would be very beneficial | 11/22/2013 1:29 PM |
| 75 | More vegetation clearance/maintenance. More diversity of trail features | 11/22/2013 8:17 AM |
| 76 | Maintenance and facilities | 11/21/2013 7:36 PM |

Forrest MTB Strategic Plan Survey

Q12 What other Victorian destinations have you visited to go Mountain Biking? (Please tick appropriate boxes - you can tick more than one box)

Answered: 131 Skipped: 18



| Answer Choices | Responses |
|-------------------------------|------------|
| Colac Otway Region | 38.17% 50 |
| Grampians Region | 18.32% 24 |
| Alpine Region | 58.02% 76 |
| Gippsland Region | 19.08% 25 |
| Ballarat/Bendigo Region | 42.75% 56 |
| Warburton | 16.79% 22 |
| You Yangs | 86.26% 113 |
| Lysterfield Park | 60.31% 79 |
| Total Respondents: 131 | |

| # | Interstate/other (please name) | Date |
|---|---|-------------------|
| 1 | Colorado USA continental divide trails | 12/5/2013 7:53 PM |
| 2 | ACT Stromlo and Bruce Ridge, WA Perth and south west mtb trails, mt remarkable SA | 12/5/2013 4:53 PM |
| 3 | Anglesea, Canberra (Stromlo etc) | 12/5/2013 3:40 PM |
| 4 | New Zealand / Tasmania west coast / | 12/5/2013 3:04 PM |
| 5 | Southern highlands/Margaret river | 12/5/2013 1:12 PM |

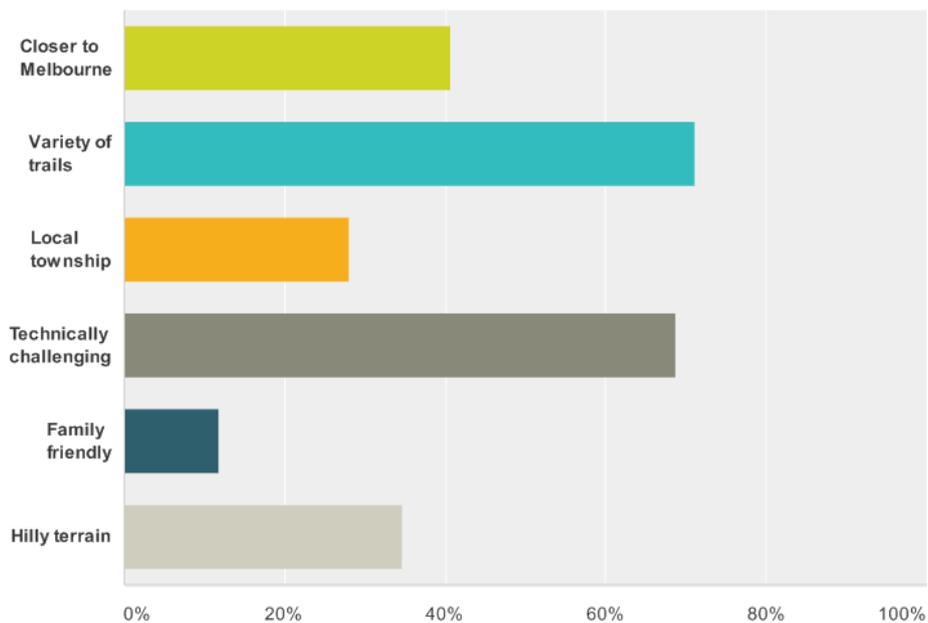
Forrest MTB Strategic Plan Survey

| | | |
|----|--|---------------------|
| 6 | Anglesea | 12/5/2013 12:57 PM |
| 7 | Nerang Forest | 12/5/2013 12:45 PM |
| 8 | Darwin | 12/5/2013 12:12 PM |
| 9 | Thunder point Wamambool, Mt Clay Portland | 12/5/2013 11:39 AM |
| 10 | Wombat forest | 12/5/2013 10:28 AM |
| 11 | wombat | 12/5/2013 10:24 AM |
| 12 | I get to the States twice a year and Ride Fruita, Moab, Sedona, Denver front range. Canberra and Melrose also rate highly. | 12/5/2013 10:17 AM |
| 13 | Yarra Trails/Candlebark; Smiths Gully; King Lake/Dandenongs; Mt Stromlo; Atherton trails | 12/5/2013 10:10 AM |
| 14 | Alice Springs, ACT, Margaret River, NSW south coast, West coast of Tasmania, Melrose, Red Hill. | 12/5/2013 10:10 AM |
| 15 | French/Swiss alps. Utah USA. Ontario and Quebec Canada. | 12/5/2013 9:58 AM |
| 16 | Anglesea | 11/28/2013 5:20 PM |
| 17 | Anglesea | 11/27/2013 2:30 PM |
| 18 | Too many to write here | 11/26/2013 9:28 PM |
| 19 | anglesea, officer, buxton | 11/26/2013 10:41 AM |
| 20 | Stromlo, Kowen Forest, Majura Pines, Awaba, Yarramundi | 11/26/2013 7:25 AM |
| 21 | perth hills | 11/25/2013 8:54 PM |
| 22 | Redhill | 11/25/2013 6:59 PM |
| 23 | Buxton / buller / castlemaine | 11/25/2013 6:07 PM |
| 24 | Many destinations all over the country. | 11/25/2013 3:39 PM |
| 25 | Anglesea, yarra valley | 11/25/2013 1:40 PM |
| 26 | Blackwood, MTBO destination | 11/25/2013 12:51 PM |
| 27 | Anglesea | 11/24/2013 8:53 PM |
| 28 | Tathra annually. Anglesea Vic | 11/24/2013 8:33 PM |
| 29 | Alice Springs | 11/24/2013 7:47 PM |
| 30 | too many to list South Oz, Tassie, Brisbane, Noosa Nsw perth | 11/24/2013 7:43 PM |
| 31 | Mt Stromlo, Atherton, Brisbane, Hobart | 11/22/2013 8:37 PM |
| 32 | Silvan and MtDandenong | 11/22/2013 3:36 PM |
| 33 | Alice Springs | 11/22/2013 1:29 PM |

Forrest MTB Strategic Plan Survey

**Q13 What is it about the other MTB destination that attracts you to ride there?
(Please tick appropriate boxes - you can tick more than one box)**

Answered: 118 Skipped: 31



| Answer Choices | Responses |
|-------------------------------|-----------|
| Closer to Melbourne | 40.68% 48 |
| Variety of trails | 71.19% 84 |
| Local township | 27.97% 33 |
| Technically challenging | 68.64% 81 |
| Family friendly | 11.86% 14 |
| Hilly terrain | 34.75% 41 |
| Total Respondents: 118 | |

| # | Other (please describe) | Date |
|---|---|--------------------|
| 1 | Moved to the region due to the mountain bike trail. | 12/16/2013 9:31 AM |
| 2 | Live in Mount beauty and enjoy exploring other trail networks for holidays. | 12/5/2013 4:53 PM |
| 3 | LIke in Warrnambool so have to travel to get a decent MTB trail, so variety is good | 12/5/2013 3:12 PM |
| 4 | Closet to SA | 12/5/2013 1:15 PM |
| 5 | Different locatipns | 12/5/2013 1:12 PM |
| 6 | Convenience | 12/5/2013 12:45 PM |
| 7 | Accessibility and riding with friends. | 12/5/2013 12:34 PM |
| 8 | I live in Geelong, so the You Yangs are close. | 12/5/2013 12:14 PM |
| 9 | differant riding | 12/5/2013 9:54 AM |

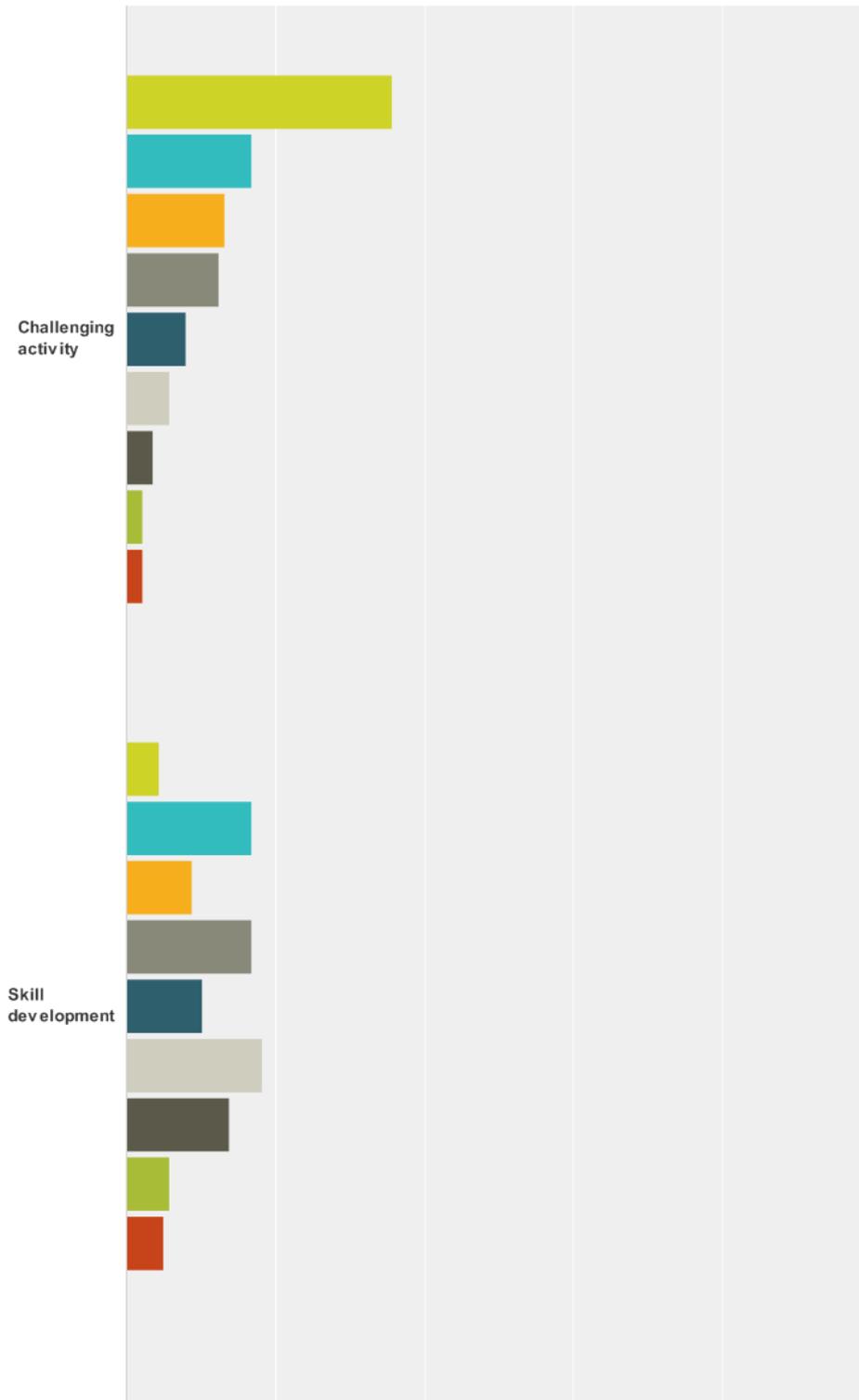
Forrest MTB Strategic Plan Survey

| | | |
|----|---|---------------------|
| 10 | Happen to be in the area - other attractions in the areas | 12/3/2013 7:24 PM |
| 11 | Closer to the ACT | 11/30/2013 8:54 PM |
| 12 | Closer to home | 11/29/2013 9:49 PM |
| 13 | CLOSER TO GEELONG | 11/28/2013 2:58 PM |
| 14 | Always looking for new challenges | 11/27/2013 7:44 PM |
| 15 | Close to where I live, Geelong | 11/27/2013 3:28 PM |
| 16 | connects townships | 11/25/2013 8:54 PM |
| 17 | Close to Geelong | 11/25/2013 8:34 PM |
| 18 | Road trips | 11/25/2013 6:07 PM |
| 19 | Professionally built trails. | 11/25/2013 3:39 PM |
| 20 | quality of trails | 11/25/2013 2:29 PM |
| 21 | Destination for a road trip. | 11/25/2013 1:47 PM |
| 22 | MTBO related | 11/25/2013 12:51 PM |
| 23 | Close to Apollo Bay | 11/25/2013 9:01 AM |
| 24 | Closer to home. More events | 11/24/2013 10:18 PM |
| 25 | Closer to home | 11/24/2013 9:22 PM |
| 26 | Closer to Geelong | 11/24/2013 7:47 PM |
| 27 | Just love travelling with bike | 11/22/2013 5:07 PM |
| 28 | Invitations from friends | 11/22/2013 8:17 AM |

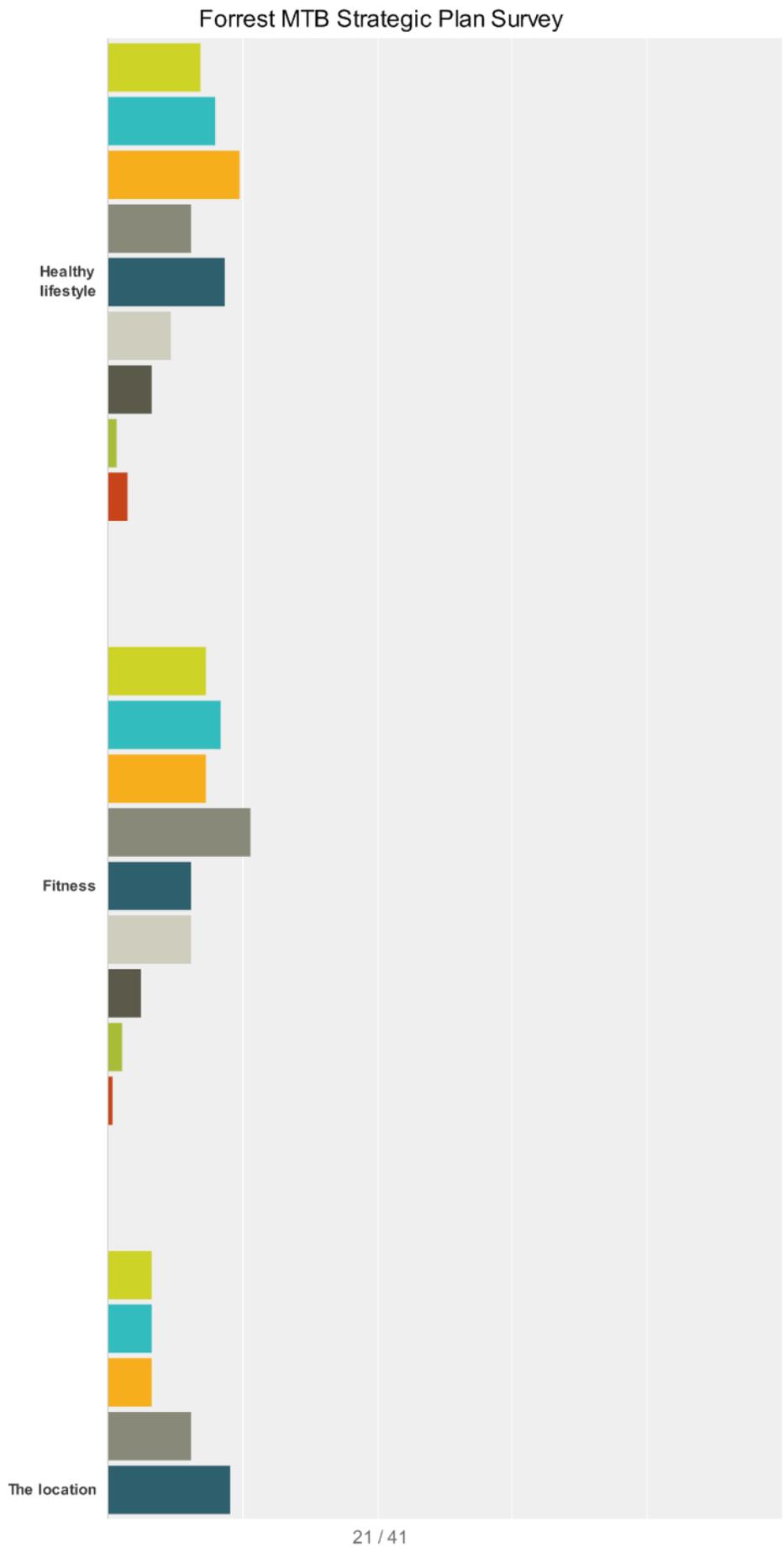
Forrest MTB Strategic Plan Survey

Q14 Please rank from #1 onwards what you most like about Mountain Biking? (Please number appropriate boxes - you don't need to number what is not important to you)

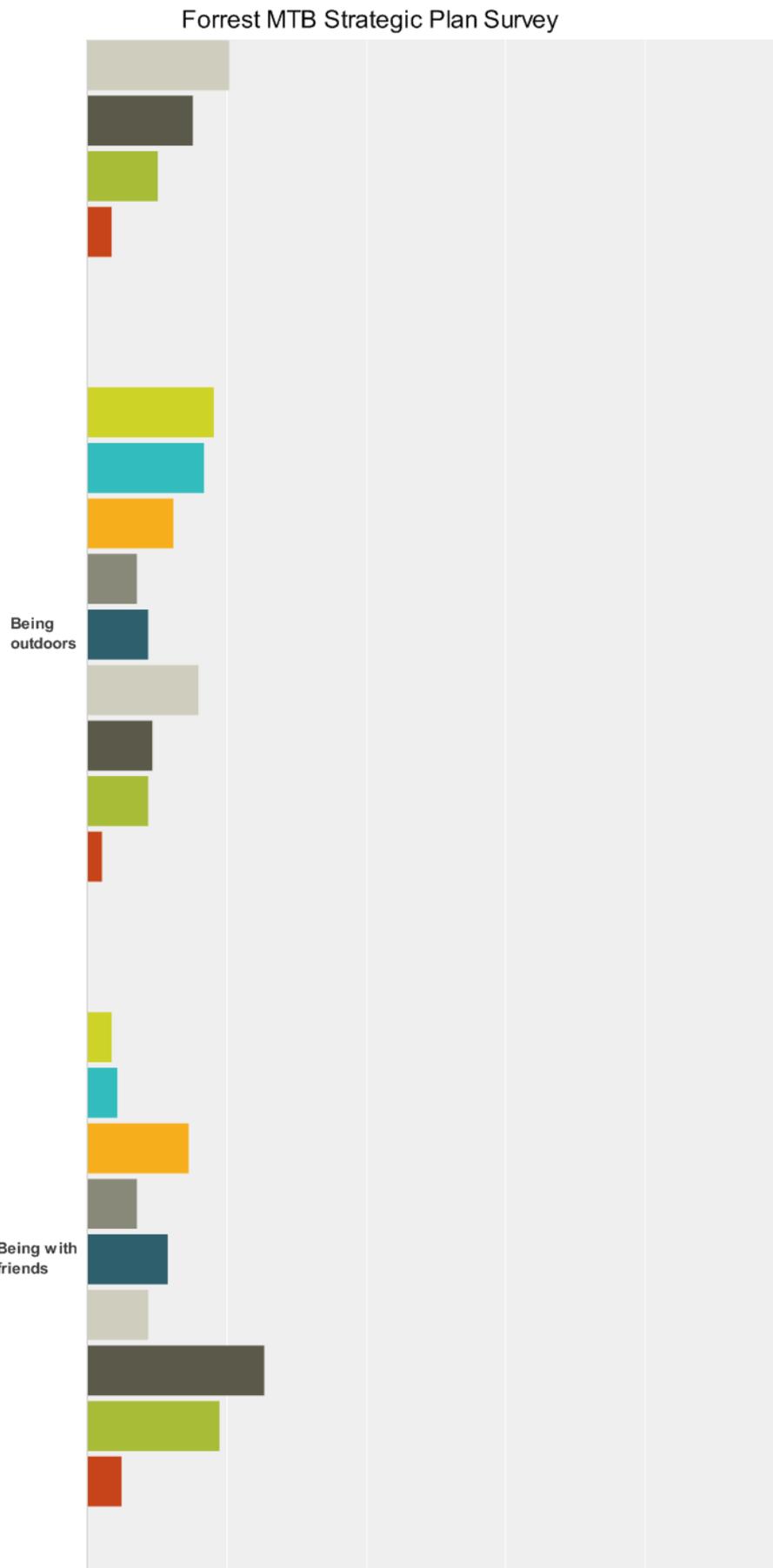
Answered: 137 Skipped: 12



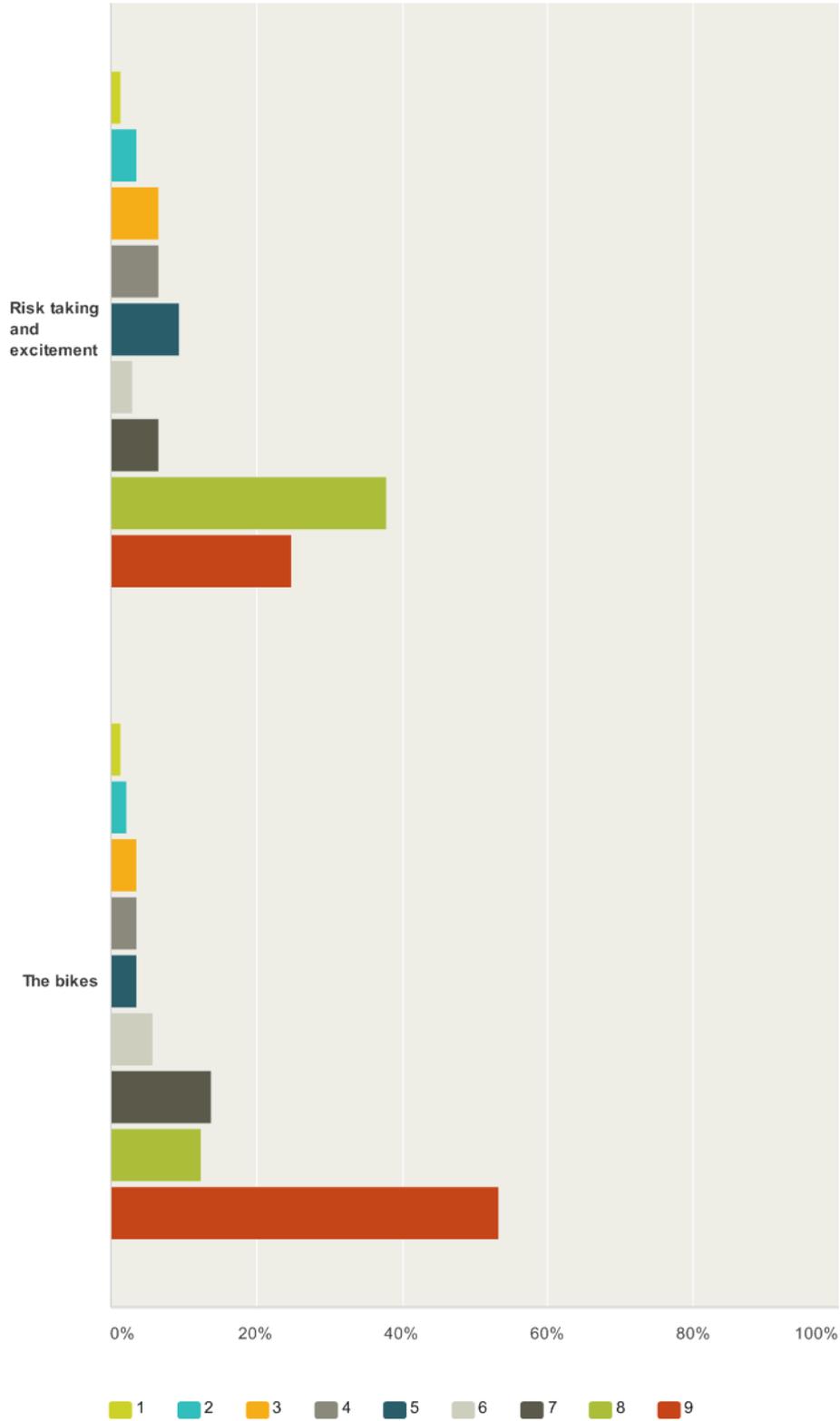
20 / 41



21 / 41



Forrest MTB Strategic Plan Survey



| | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | Total | Average Ranking |
|----------------------|--------------|--------------|--------------|--------------|-------------|------------|------------|------------|------------|-------|-----------------|
| Challenging activity | 35.77%
49 | 16.79%
23 | 13.14%
18 | 12.41%
17 | 8.03%
11 | 5.84%
8 | 3.65%
5 | 2.19%
3 | 2.19%
3 | 137 | 7.04 |

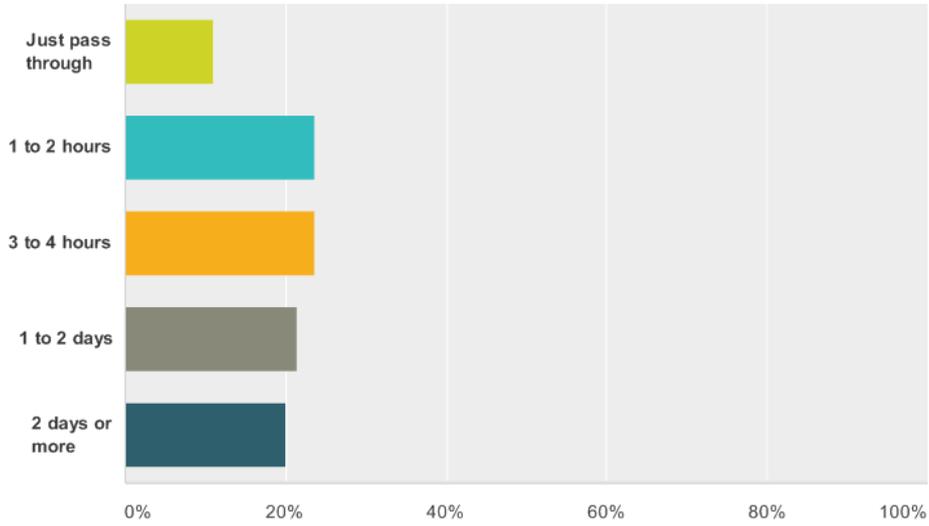
Forrest MTB Strategic Plan Survey

| | | | | | | | | | | | |
|----------------------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|--------------|-----|------|
| Skill development | 4.38%
6 | 16.79%
23 | 8.76%
12 | 16.79%
23 | 10.22%
14 | 18.25%
25 | 13.87%
19 | 5.84%
8 | 5.11%
7 | 137 | 5.18 |
| Healthy lifestyle | 13.87%
19 | 16.06%
22 | 19.71%
27 | 12.41%
17 | 17.52%
24 | 9.49%
13 | 6.57%
9 | 1.46%
2 | 2.92%
4 | 137 | 6.17 |
| Fitness | 14.60%
20 | 16.79%
23 | 14.60%
20 | 21.17%
29 | 12.41%
17 | 12.41%
17 | 5.11%
7 | 2.19%
3 | 0.73%
1 | 137 | 6.27 |
| The location | 6.57%
9 | 6.57%
9 | 6.57%
9 | 12.41%
17 | 18.25%
25 | 20.44%
28 | 15.33%
21 | 10.22%
14 | 3.65%
5 | 137 | 4.75 |
| Being outdoors | 18.25%
25 | 16.79%
23 | 12.41%
17 | 7.30%
10 | 8.76%
12 | 16.06%
22 | 9.49%
13 | 8.76%
12 | 2.19%
3 | 137 | 5.85 |
| Being with friends | 3.65%
5 | 4.38%
6 | 14.60%
20 | 7.30%
10 | 11.68%
16 | 8.76%
12 | 25.55%
35 | 18.98%
26 | 5.11%
7 | 137 | 4.27 |
| Risk taking and excitement | 1.46%
2 | 3.65%
5 | 6.57%
9 | 6.57%
9 | 9.49%
13 | 2.92%
4 | 6.57%
9 | 37.96%
52 | 24.82%
34 | 137 | 3.07 |
| The bikes | 1.46%
2 | 2.19%
3 | 3.65%
5 | 3.65%
5 | 3.65%
5 | 5.84%
8 | 13.87%
19 | 12.41%
17 | 53.28%
73 | 137 | 2.39 |

Forrest MTB Strategic Plan Survey

Q15 How long do you typically spend in the Forrest Township? (Please tick appropriate box)

Answered: 135 Skipped: 14

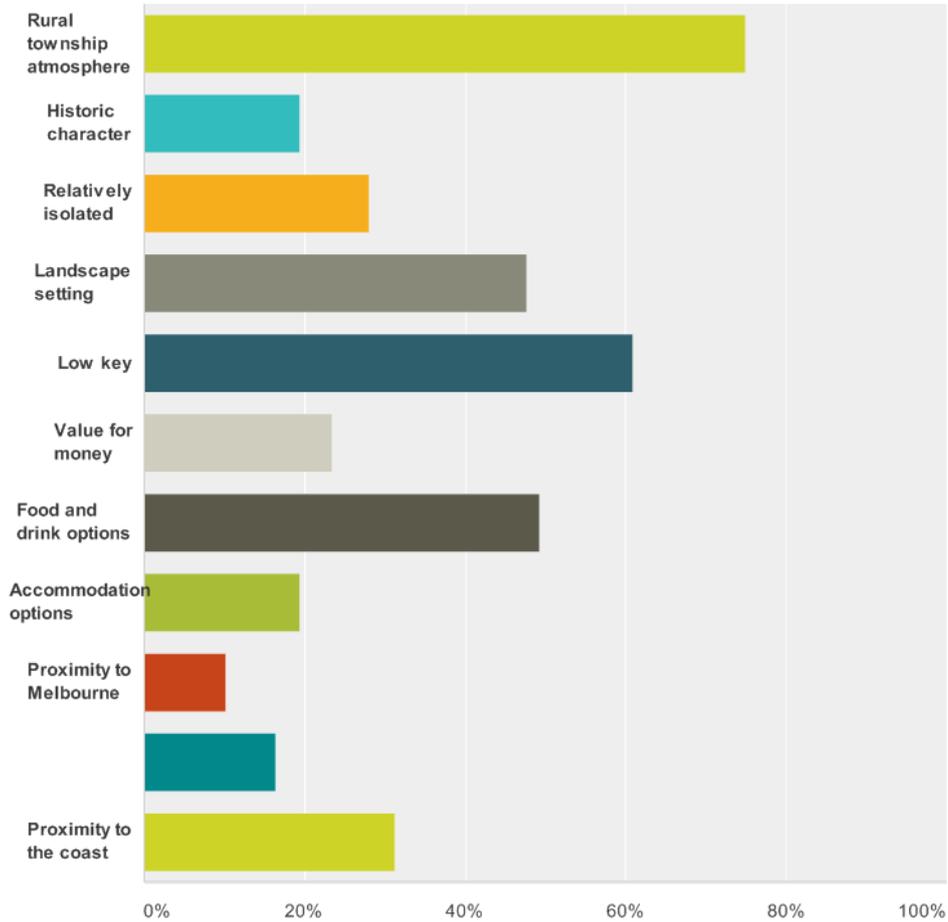


| Answer Choices | Responses | |
|-------------------|-----------|------------|
| Just pass through | 11.11% | 15 |
| 1 to 2 hours | 23.70% | 32 |
| 3 to 4 hours | 23.70% | 32 |
| 1 to 2 days | 21.48% | 29 |
| 2 days or more | 20% | 27 |
| Total | | 135 |

Forrest MTB Strategic Plan Survey

Q16 What do you like about the Forrest Township? (Please tick appropriate boxes - you can tick more than one box)

Answered: 128 Skipped: 21



| Answer Choices | Responses |
|---------------------------------|-----------|
| Rural township atmosphere | 75% 96 |
| Historic character | 19.53% 25 |
| Relatively isolated | 28.13% 36 |
| Landscape setting | 47.66% 61 |
| Low key | 60.94% 78 |
| Value for money | 23.44% 30 |
| Food and drink options | 49.22% 63 |
| Accommodation options | 19.53% 25 |
| Proximity to Melbourne | 10.16% 13 |
| Proximity to other destinations | 16.41% 21 |

Forrest MTB Strategic Plan Survey

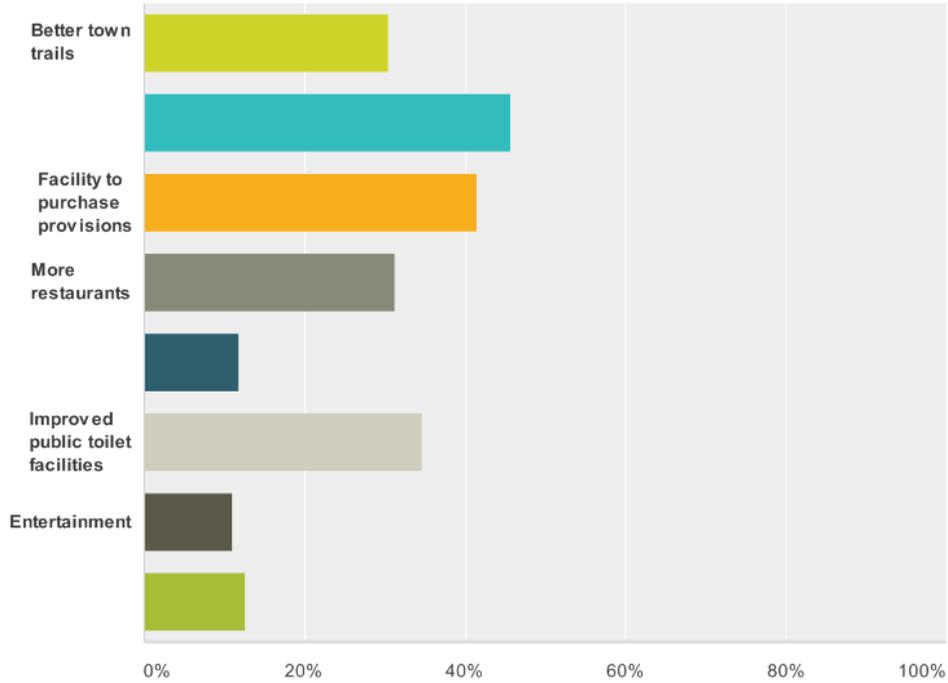
| | | |
|-------------------------------|--------|----|
| Proximity to the coast | 31.25% | 40 |
| Total Respondents: 128 | | |

| # | Other (please specify) | Date |
|----|---|---------------------|
| 1 | Live in forrest | 12/14/2013 6:46 AM |
| 2 | The lovely fresh air and the brewery and guest house which make delicious lunches | 12/11/2013 4:11 PM |
| 3 | Norm and Jess's cafe/bike shop | 12/5/2013 12:18 PM |
| 4 | Dont really spend any time in the town | 12/5/2013 10:59 AM |
| 5 | Nothing in particular. I've normally stayed in Apollo Bay or just pass through. | 12/5/2013 10:14 AM |
| 6 | Great Beer!! | 12/5/2013 10:03 AM |
| 7 | Bike and coffee shop | 12/3/2013 7:33 PM |
| 8 | Proximity to Apollo Bay | 11/27/2013 7:52 PM |
| 9 | Tracks for riding | 11/25/2013 6:11 PM |
| 10 | Caravan park | 11/25/2013 5:57 PM |
| 11 | It's nice enough, but I couldn't say that I 'love' any of the above aspects enough to tick the box... | 11/25/2013 3:44 PM |
| 12 | passing through there | 11/25/2013 2:31 PM |
| 13 | Brewery! | 11/25/2013 1:40 PM |
| 14 | Usually camp or take a cabin. | 11/24/2013 8:49 PM |
| 15 | Always have a beer at the brewery and grab snacks at the shop | 11/24/2013 8:38 PM |
| 16 | proximity to geelong | 11/24/2013 7:39 PM |
| 17 | I live here | 11/22/2013 8:45 PM |
| 18 | Mtb trails. Without trails I wouldn't go there. | 11/22/2013 6:23 PM |
| 19 | Forrest lacks character but is in a great spot | 11/21/2013 10:29 PM |

Forrest MTB Strategic Plan Survey

Q17 What could be improved within the Forrest Township to encourage you to stay longer? (Please tick appropriate boxes - you can tick more than one box)

Answered: 118 Skipped: 31



| Answer Choices | Responses |
|---|-----------|
| Better town trails | 30.51% 36 |
| MTB washdown facility | 45.76% 54 |
| Facility to purchase provisions | 41.53% 49 |
| More restaurants | 31.36% 37 |
| More accommodation (please specify below) | 11.86% 14 |
| Improved public toilet facilities | 34.75% 41 |
| Entertainment | 11.02% 13 |
| Additional nearby adventure/recreational activities | 12.71% 15 |
| Total Respondents: 118 | |

| # | Other (please specify) | Date |
|---|--|--------------------|
| 1 | more places to eat | 12/19/2013 6:48 PM |
| 2 | PUBLIC SHOWER FACILITY | 12/13/2013 9:49 PM |
| 3 | Since the Barwon Downs store closed it's hard to get good provisions | 12/11/2013 4:11 PM |
| 4 | more trails | 12/5/2013 3:07 PM |
| 5 | Better access to dirt roads out of Barwon Downs and off Roadknight Creek Road for XC riding. | 12/5/2013 12:39 PM |

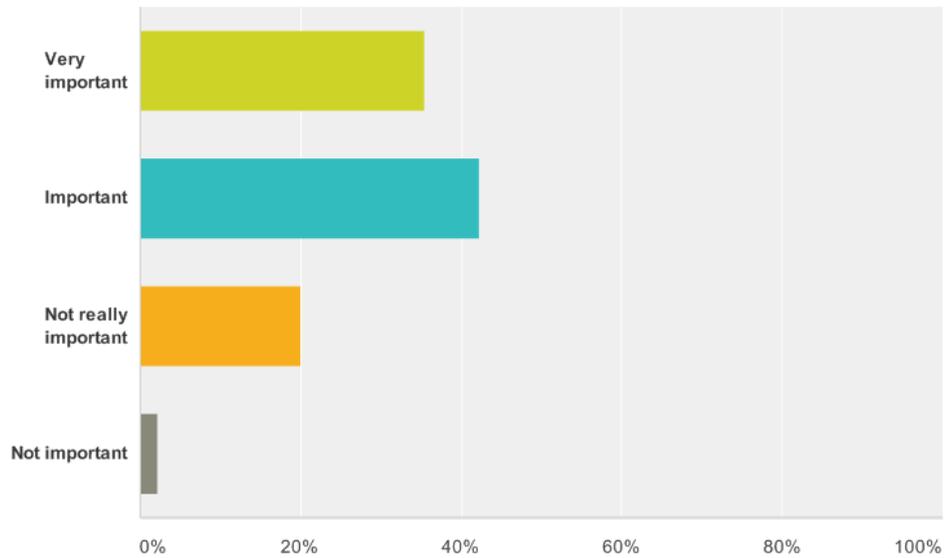
Forrest MTB Strategic Plan Survey

| | | |
|----|--|---------------------|
| 6 | Its great, leave it alone ! | 12/5/2013 12:20 PM |
| 7 | Other things for the whole family to do and MTB events would make me stay | 12/5/2013 12:18 PM |
| 8 | As per earlier - more access to drinking water for refills at trail heads | 12/5/2013 12:14 PM |
| 9 | More kid friendly (7yrs and younger) | 12/5/2013 11:05 AM |
| 10 | I dont intend on staying there, we drive in and drive out as I live in Geelong | 12/5/2013 10:59 AM |
| 11 | Hut style accomadation for small groups overnight, for weekends etc. | 12/5/2013 10:23 AM |
| 12 | More cabins in caravan park | 12/5/2013 10:05 AM |
| 13 | After 2 months in European alps this year, the Gites/Refuges were a real highlight. Cheap, clean, good food, good atmosphere. | 12/5/2013 10:03 AM |
| 14 | Longer weekends! And becoming independently wealthy so I don't have to work | 12/3/2013 7:33 PM |
| 15 | More Dog Friendly accommodation | 11/28/2013 5:23 PM |
| 16 | better general store - owners are rude, range is poor. other forrest businesses fantastic | 11/26/2013 3:42 PM |
| 17 | Public Transport with my bike from Melbourne or Coast | 11/26/2013 1:45 PM |
| 18 | safer access to lake elizabeth ie sealed roads | 11/25/2013 11:36 PM |
| 19 | more complementary activities within 20mins - things to keep rest of family entertained | 11/25/2013 8:57 PM |
| 20 | Some locals think they own the trails. The Forrest MTB Club sometime post militant style messages about riders and use of the trails. Not cool. Is a turn off to riding there. | 11/25/2013 12:37 PM |
| 21 | More family friendly trails. | 11/25/2013 11:40 AM |
| 22 | Like it how it is | 11/24/2013 8:38 PM |
| 23 | more budget accomodation, another caravan park maybe | 11/24/2013 7:39 PM |
| 24 | Better bus link to train station | 11/23/2013 10:34 AM |
| 25 | More food options for mountain bikers at night | 11/22/2013 9:33 PM |
| 26 | Longer weekends | 11/22/2013 6:23 PM |
| 27 | More self catering options. Houses/apartments but with cheaper prices. | 11/22/2013 4:05 PM |
| 28 | Pretty nice as is | 11/22/2013 3:42 PM |
| 29 | Forrest needs food options open all week, not just weekends. Fuel availability would also help | 11/21/2013 10:29 PM |

Forrest MTB Strategic Plan Survey

Q18 How important is the Forrest Township to your MTB experience? (Please tick appropriate box)

Answered: 135 Skipped: 14



| Answer Choices | Responses | |
|----------------------|-----------|------------|
| Very important | 35.56% | 48 |
| Important | 42.22% | 57 |
| Not really important | 20% | 27 |
| Not important | 2.22% | 3 |
| Total | | 135 |

Forrest MTB Strategic Plan Survey

**Q19 How is the Forrest Township important
to your MTB experience?**

Answered: 81 Skipped: 68

| # | Responses | Date |
|----|--|--------------------|
| 1 | AS A BASE FOR ACTIVITIES | 12/13/2013 9:49 PM |
| 2 | For rest and recreation after riding and before and also accommodation | 12/11/2013 4:11 PM |
| 3 | live in Apollo Bay - to far to go to other areas | 12/11/2013 3:47 PM |
| 4 | Activity hub | 12/6/2013 2:21 PM |
| 5 | Live in Colac so not as important but nice to make the day a real outing lunch etc. could have better play ground for the kids to play in to enhance young family experience | 12/6/2013 2:04 PM |
| 6 | I enjoy stopping for a coffee or a meal after a good ride on the trails. Choices are limited but still enjoyable. | 12/5/2013 9:38 PM |
| 7 | The towns interest in mtb. Friendly service and helped with getting bike going again. | 12/5/2013 8:41 PM |
| 8 | Its a small town that requires my attendance | 12/5/2013 7:54 PM |
| 9 | Enjoy the low key atmosphere and the natural surrounding. Existing facilities suit me fine. | 12/5/2013 5:00 PM |
| 10 | Has food and beer for post ride debrief | 12/5/2013 4:15 PM |
| 11 | > Always have lunch in town and often buy provisions from the store. Family /Friends often meet after ride for lunch > Lake Elizabeth / Reservoir good place for the kids to go walk/picnic while I ride > Trailhead facilities good and generally clean > Occasionally camp | 12/5/2013 3:43 PM |
| 12 | Its great to relax after a ride with family and friends and enjoy a meal/drink and the relaxing surroundings | 12/5/2013 3:16 PM |
| 13 | just need a few basic facilities, it' fine like it is... | 12/5/2013 3:07 PM |
| 14 | The atmosphere and quaint nature of the town gives the whole MTB feel. | 12/5/2013 1:17 PM |
| 15 | Great place to stay friendly people | 12/5/2013 1:02 PM |
| 16 | A good place to grab a drink and something to eat. | 12/5/2013 12:58 PM |
| 17 | need for infrastrucutre to support activity and entertain family | 12/5/2013 12:48 PM |
| 18 | The town is the starting and finishing point and provides the style or feel for each visit. A welcoming atmosphere makes me feel part of the town. | 12/5/2013 12:39 PM |
| 19 | Its a great little town which you can stay in, eat in and ride (not in the car !) to all of the trails. Its awesome. Leave it alone ! | 12/5/2013 12:20 PM |
| 20 | It is great to have accomodation (camping or house), the cafe, brewery to make the stay comfortable. | 12/5/2013 12:18 PM |
| 21 | place to have an after ride beer with mates. | 12/5/2013 11:40 AM |
| 22 | Great place to get a drink & something to eat after riding. | 12/5/2013 11:22 AM |
| 23 | great location and it is mTB friendly | 12/5/2013 11:05 AM |
| 24 | Gives opportunity to have a beer and food after riding and also options to stay overnight for longer mtb ride weekend trips | 12/5/2013 10:32 AM |
| 25 | Food and beers | 12/5/2013 10:26 AM |
| 26 | I love that everyone in the town knows that I come to ride there, no discrimination, quite the opposite, they love that we are there to ride and welcome us with open arms. I ride a lot of road bike races also, and the difference in being in a bike sport where there is a whole community who have built a place to do the sport and are so friendly is Heaven on Earth. Best MTB spot on the Planet. | 12/5/2013 10:23 AM |
| 27 | A trail centre like the ones in Wales, UK that has café, toilets and showers (pay for), bike shop/mechanics. | 12/5/2013 10:22 AM |

Forrest MTB Strategic Plan Survey

| | | |
|----|---|---------------------|
| 28 | Most of us spend as much time sitting around talking about how good we are as we do riding. It's nice to have good places to do this. | 12/5/2013 10:19 AM |
| 29 | Stay longer if more food options, esp a reasonable shop for supplies | 12/5/2013 10:05 AM |
| 30 | Food, Beer, safe trailhead for parking. | 12/5/2013 10:03 AM |
| 31 | Because we live far away and biking and good accom and food and beer makes for a whole weekend. | 12/3/2013 7:33 PM |
| 32 | Central location to set up base and be surrounded by an awesome network of trails. | 11/30/2013 8:59 PM |
| 33 | Having good accommodation, food and drink so close to the trail network makes the whole trip alot more laid back, relaxed and stress free | 11/28/2013 6:26 PM |
| 34 | Good place for the Family | 11/28/2013 5:23 PM |
| 35 | Great little town and great people, hope it stays that way ! | 11/28/2013 2:19 PM |
| 36 | Because I live in close enough proximity to Forrest I do not need to rely on Forrest township, though I choose to stop at the Brewery and Wonky Donkey occasionally. | 11/27/2013 7:52 PM |
| 37 | it's good to have somewhere to go and meet beforehand and then to sit down after the ride and "Tell war Stories" so to speak. | 11/27/2013 3:31 PM |
| 38 | Needs to be a good base | 11/26/2013 9:31 PM |
| 39 | lovely town, great after-ride options, friendly (apart from general store) | 11/26/2013 3:42 PM |
| 40 | Unique nature based experience for all outdoor activities | 11/26/2013 1:45 PM |
| 41 | strange question - Forrest without the trails vs. the trails without Forrest. Can't imagine one without the other | 11/26/2013 10:45 AM |
| 42 | Stop in for a coffee or lunch after a day of riding, visit the damn. | 11/25/2013 11:36 PM |
| 43 | Having a small town with a vibrant economy where provisions can be purchased without significant travel is very important | 11/25/2013 10:04 PM |
| 44 | place to relax after a ride to talk it over with mates. | 11/25/2013 8:57 PM |
| 45 | Its important in terms of the overall experience.. The more enjoyable the town is to stay in the more time i spend there so the more time i'll spend ridind the trails. | 11/25/2013 8:42 PM |
| 46 | not really important to start with, but these things do take time. With some accomodation, restaurant/pub, mtb skills/tours and increased patronage mountain bikers themselves will help drive the experience. Good luck and thankyou, i'll be back again soon! | 11/25/2013 8:28 PM |
| 47 | Love the town and the food coffee and trails | 11/25/2013 7:03 PM |
| 48 | Meeting point with somewhere to eat and drink after ride | 11/25/2013 6:11 PM |
| 49 | It's a place for gathering at the end of the day. Sitting around the campfire, relaxing at camp between rides, chatting with locals, etc. | 11/25/2013 5:57 PM |
| 50 | Mainly just to know that I can get something to eat and drink. I live within an hour so it's not essential for much more than that. | 11/25/2013 3:44 PM |
| 51 | Great base to start finish rides. option to have a meal / drink post ride. Accommodation options for extended stays. Friendly laid back atmosphere, and a good base for family and less enthusiastic riders to relax and enjoy the stay. | 11/25/2013 3:00 PM |
| 52 | I generally park at Forrest trailhead. I usually visit the Comer Store or Brewery. | 11/25/2013 2:54 PM |
| 53 | Great community, it makes an otherwise rural destination a social activity | 11/25/2013 1:42 PM |
| 54 | Nice to have good food, coffee, and beer available so close to the trailhead. | 11/25/2013 1:40 PM |
| 55 | It's not. It's more about the trails. | 11/25/2013 12:37 PM |
| 56 | Very, It allows for a family friendly activity. | 11/25/2013 11:40 AM |
| 57 | Great to meet up with other people riding the trails. Some good food options becoming available. | 11/24/2013 10:21 PM |
| 58 | I regularly ride the you yangs. While they are good and close it is only two hills. Forrest offers a variety of skilled trails from green to black with enough hills to keep you fit. I look forward to riding at Forrest for the all-day riding challenge. I should visit more often since it is only 30mins extra travel than to the you yangs. | 11/24/2013 8:49 PM |
| 59 | Love the brewery | 11/24/2013 8:38 PM |

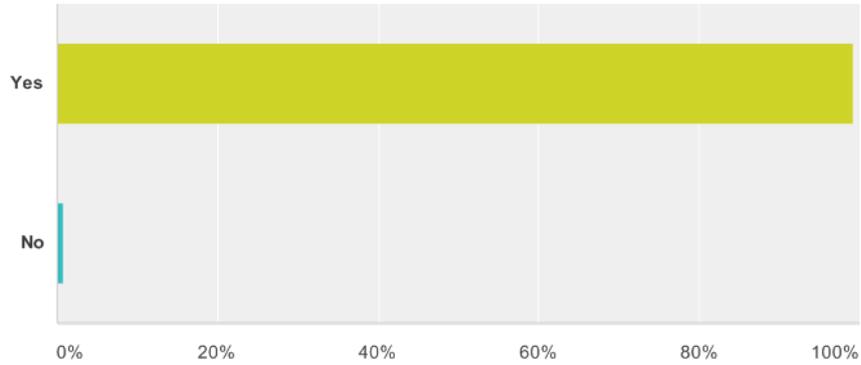
Forrest MTB Strategic Plan Survey

| | | |
|----|---|---------------------|
| 60 | Base to return to after a ride | 11/24/2013 8:10 PM |
| 61 | we love relaxing and getting some food and a beer after a long ride through the forest trails. | 11/24/2013 7:56 PM |
| 62 | Great to get a refreshing ale or warming coffee and good tucker after a ride | 11/24/2013 7:50 PM |
| 63 | Provides hub for me to base my mtb trip around | 11/23/2013 6:56 PM |
| 64 | It is the heart and soul of MTB. | 11/23/2013 4:35 PM |
| 65 | The rural setting in the outways give the trails their identity | 11/23/2013 2:54 PM |
| 66 | Why I go the town | 11/23/2013 6:31 AM |
| 67 | Just adds to the whole atmosphere, it's the base the whole thing is built on. | 11/23/2013 6:16 AM |
| 68 | Nice to have a friendly community to hang out with when not on the bike :) | 11/22/2013 9:56 PM |
| 69 | Without the township there is no Forrest | 11/22/2013 8:45 PM |
| 70 | Very very important. So good to live near the coast but have this on our doorstep. | 11/22/2013 7:22 PM |
| 71 | As much as I'd like to spend the whole time riding, it is also good to spend time relaxing & enjoy the town. | 11/22/2013 7:10 PM |
| 72 | Food, drinks accommodation is important but most important is that it's a mtb destination, therefore full of like minded people creating a great social experience. | 11/22/2013 6:23 PM |
| 73 | Helps link two trail heads together and provide a spot to either rest between trails or cool down after with beers and a bite at either pub or brewery which helps make into complete day trip. | 11/22/2013 4:53 PM |
| 74 | Great to have a town so nearby for food/drinks/toilets | 11/22/2013 4:20 PM |
| 75 | Love that when you roll into town there is always other riders hanging around, bikes on cars & the town is very accepting of MTBers | 11/22/2013 4:11 PM |
| 76 | Makes for a lovely holiday. The brewery is tops! | 11/22/2013 4:05 PM |
| 77 | Provides a great platform to enjoy MTB and show all riders all varying abilities the trails. | 11/22/2013 3:59 PM |
| 78 | Nice to chill with a good meal at the brewery after a ride. Also the Corner shop is a must visit for a pie, coffee or just a chat. | 11/22/2013 3:42 PM |
| 79 | It's not really, but it is a nice small town. If it had a few more visitor friendly cafes then stays would be extended | 11/22/2013 1:32 PM |
| 80 | ? | 11/22/2013 8:24 AM |
| 81 | Forrest is a convenient place to stay, and when open, the Brewery is a good place to eat after biking. | 11/21/2013 10:29 PM |

Forrest MTB Strategic Plan Survey

Q20 Would you encourage your MTB friends to visit Forrest and ride the MTB trails?

Answered: 136 Skipped: 13



| Answer Choices | Responses | |
|----------------|-----------|------------|
| Yes | 99.26% | 135 |
| No | 0.74% | 1 |
| Total | | 136 |

| # | Please describe why | Date |
|----|---|---------------------|
| 1 | Great place to ride | 12/13/2013 11:56 AM |
| 2 | It's a great place to ride and not too crowded and near the beach for a swim after | 12/11/2013 4:12 PM |
| 3 | beautiful part of the country, awesome riding, getting 'out there' | 12/6/2013 2:22 PM |
| 4 | Great technical trails that are a lot of fun to ride. They are also a lot less intimidating than some of the you yangs trails as they are not as rocky. | 12/5/2013 9:40 PM |
| 5 | Great trails | 12/5/2013 8:42 PM |
| 6 | Because its fantastic. A must for friends from O/S and other parts of this state. | 12/5/2013 7:57 PM |
| 7 | I think the trail network is very good and offers diversity and challenges for all abilities. Is well signposted and easy to navigate. | 12/5/2013 5:02 PM |
| 8 | Great setting and awesome trails | 12/5/2013 4:15 PM |
| 9 | Great riding, great environment, nice township (improving) | 12/5/2013 3:44 PM |
| 10 | Fun trails suitable with variety of trails for different skill levels | 12/5/2013 3:17 PM |
| 11 | greta trials | 12/5/2013 3:08 PM |
| 12 | Absolutely, it is a wonderful MTB Park. | 12/5/2013 12:59 PM |
| 13 | Great trails. | 12/5/2013 12:51 PM |
| 14 | good fun, value, reward | 12/5/2013 12:49 PM |
| 15 | It provides excellent trails for relaxed or competitive riding in a peaceful environment. | 12/5/2013 12:40 PM |
| 16 | Its a great, quiet little town. To be clear, if you start thinking of putting fun parks or large apartment complexes in, it will no longer be awesome. Leave it alone ! | 12/5/2013 12:21 PM |
| 17 | Super trails, beautiful forests lots of trails with lots of variety. | 12/5/2013 12:19 PM |
| 18 | Diversity of trails, quality of riders on trails, relaxed township | 12/5/2013 12:14 PM |
| 19 | Beautiful location, great trails. & can stay/eat/drink right there. | 12/5/2013 11:23 AM |

Forrest MTB Strategic Plan Survey

| | | |
|----|---|---------------------|
| 20 | the experince, the place and the joy! | 12/5/2013 11:06 AM |
| 21 | And I do, everyone needs to ride Marriners! | 12/5/2013 11:00 AM |
| 22 | I love riding there and I know others will too | 12/5/2013 10:33 AM |
| 23 | Great location and trails. Only problem is distance from Melbourne | 12/5/2013 10:27 AM |
| 24 | Good range of trails in a beautiful setting. | 12/5/2013 10:23 AM |
| 25 | See above | 12/5/2013 10:21 AM |
| 26 | Great place to go riding. | 12/5/2013 10:07 AM |
| 27 | Great trails and many kilometres of them, so don't have to repeat same ones all the time. | 12/5/2013 10:06 AM |
| 28 | This survey is getting too long! | 12/5/2013 10:03 AM |
| 29 | The trails are awesome and there's plenty of them. Great bush setting too. | 11/30/2013 9:00 PM |
| 30 | Variety of trails, there is something for everyone to ride | 11/28/2013 6:27 PM |
| 31 | For all the previous listed great things about Forrest | 11/28/2013 3:00 PM |
| 32 | Because they are the best ive ridden, but Nambour is a close second | 11/28/2013 2:21 PM |
| 33 | The trails at Forrest are as good, if not better than others I have ridden. | 11/27/2013 7:53 PM |
| 34 | it's a fun activity, and the trails are world class | 11/27/2013 3:32 PM |
| 35 | great experience | 11/26/2013 3:43 PM |
| 36 | Because I think its a great place to ride with a lot of diversity especially good for the beginner, its where I began. | 11/25/2013 11:38 PM |
| 37 | The challenging flowy trails, nothing like it in Victoria | 11/25/2013 10:04 PM |
| 38 | great day activity - nice brewery | 11/25/2013 8:58 PM |
| 39 | You can ride all day and not cover the same trail. Plenty of different trails and views to take in. | 11/25/2013 8:29 PM |
| 40 | Yes, but to be clear, I would encourage them to MTB anywhere. I do not believe that the Forrest trails are the best quality trails around. They need to have professionals build their trails if they are going to be successful. | 11/25/2013 3:45 PM |
| 41 | Because it's a great destination. | 11/25/2013 2:55 PM |
| 42 | I plan to go in 2014 | 11/25/2013 2:20 PM |
| 43 | Good to go to, but needs some more variety to keep people returning. | 11/25/2013 1:54 PM |
| 44 | Fun trails, great community | 11/25/2013 1:43 PM |
| 45 | Great trails | 11/25/2013 1:40 PM |
| 46 | It is a great place to ride. | 11/24/2013 10:22 PM |
| 47 | Beautiful town ,great bush setting and a brewery what more can you ask for? | 11/24/2013 9:32 PM |
| 48 | Variety of trails, good value accommodation, escapism, hospitality, brewery and good food for social relaxing. | 11/24/2013 8:53 PM |
| 49 | Great trails, great brewery | 11/24/2013 8:39 PM |
| 50 | Variety | 11/24/2013 8:10 PM |
| 51 | great fun trails, amazing bush and scenery. | 11/24/2013 7:56 PM |
| 52 | The trails are fun. The people are friendly. | 11/23/2013 6:57 PM |
| 53 | Great variety of trails in beautiful location. | 11/22/2013 9:57 PM |
| 54 | Nice quiet town, great setting in the otways. Great trails | 11/22/2013 9:34 PM |
| 55 | There is no other experience like it | 11/22/2013 8:45 PM |
| 56 | Absolutely. Good tracks for one and all. | 11/22/2013 7:23 PM |
| 57 | Because it's a must. Great trails, food, drink accom. Why would you not?? | 11/22/2013 6:25 PM |
| 58 | Some of the best trails in Vic | 11/22/2013 5:25 PM |
| 59 | Because there are a good variety of fun trails that provides something different to the usual you yangs or lysterfield and can hang out in township afterwards. | 11/22/2013 4:54 PM |

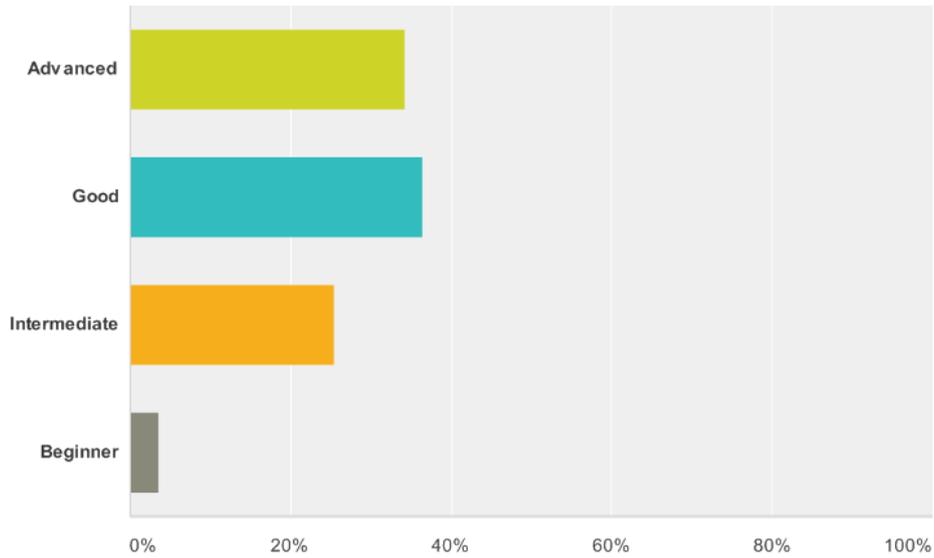
Forrest MTB Strategic Plan Survey

| | | |
|----|--|---------------------|
| 60 | It's awesome! | 11/22/2013 3:56 PM |
| 61 | Good trail network with good variety of terrain | 11/22/2013 1:33 PM |
| 62 | I am not a biker, but lots of friends are and most who come to the Otways love it. Strangely, the Otways are not that well known even in Melbourne. Most who come come again. MTB trails need to be promoted more in Melbourne and Interstate. | 11/21/2013 10:32 PM |

Forrest MTB Strategic Plan Survey

Q21 Please describe your level of MTB skill? (Please tick appropriate box)

Answered: 137 Skipped: 12

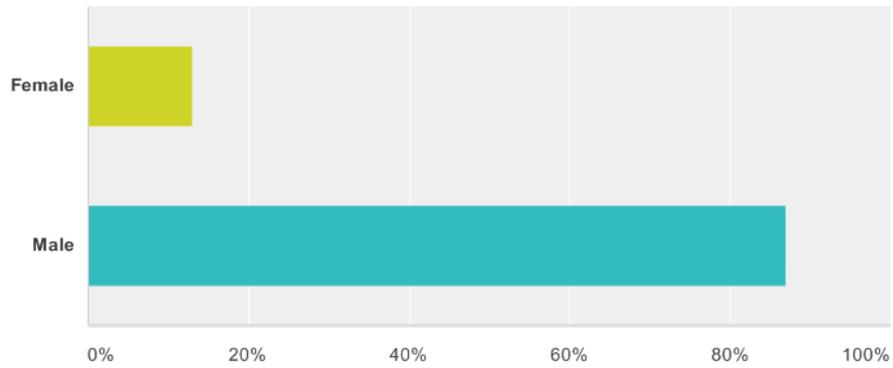


| Answer Choices | Responses | |
|----------------|-----------|------------|
| Advanced | 34.31% | 47 |
| Good | 36.50% | 50 |
| Intermediate | 25.55% | 35 |
| Beginner | 3.65% | 5 |
| Total | | 137 |

Forrest MTB Strategic Plan Survey

Q22 Gender

Answered: 139 Skipped: 10

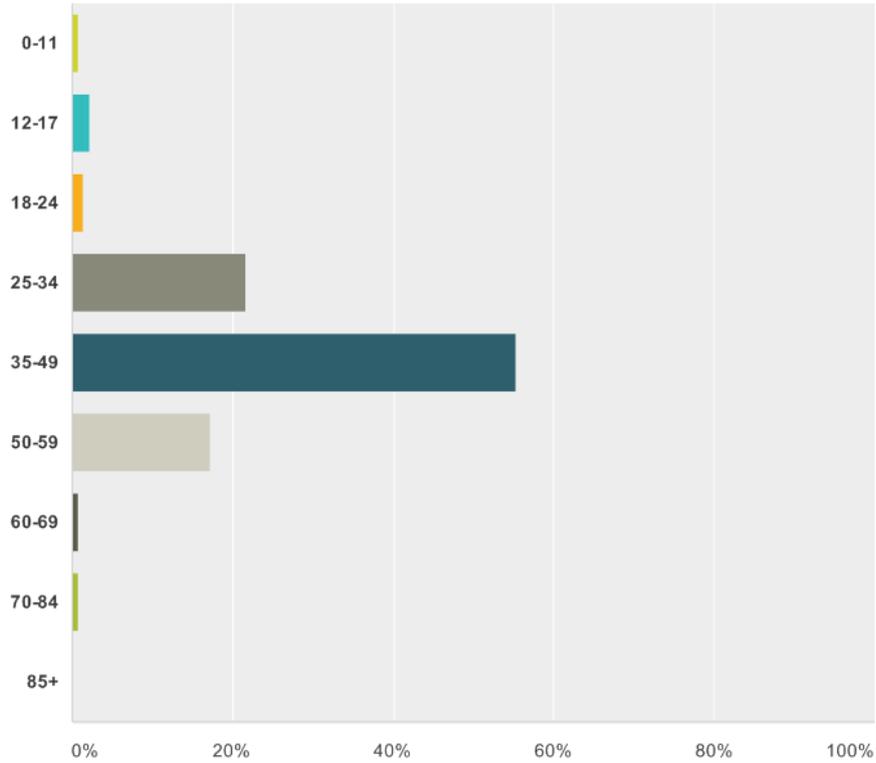


| Answer Choices | Responses | |
|----------------|-----------|------------|
| Female | 12.95% | 18 |
| Male | 87.05% | 121 |
| Total | | 139 |

Forrest MTB Strategic Plan Survey

Q23 Age Range

Answered: 139 Skipped: 10

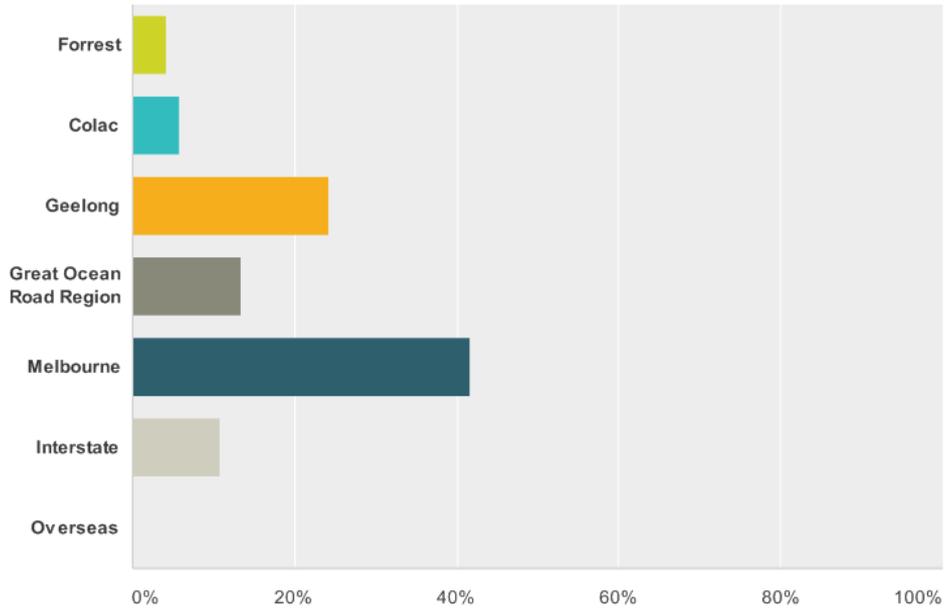


| Answer Choices | Responses | |
|----------------|-----------|------------|
| 0-11 | 0.72% | 1 |
| 12-17 | 2.16% | 3 |
| 18-24 | 1.44% | 2 |
| 25-34 | 21.58% | 30 |
| 35-49 | 55.40% | 77 |
| 50-59 | 17.27% | 24 |
| 60-69 | 0.72% | 1 |
| 70-84 | 0.72% | 1 |
| 85+ | 0% | 0 |
| Total | | 139 |

Forrest MTB Strategic Plan Survey

Q24 Where do you live? (Please tick one box)

Answered: 120 Skipped: 29



| Answer Choices | Responses | |
|-------------------------|-----------|------------|
| Forrest | 4.17% | 5 |
| Colac | 5.83% | 7 |
| Geelong | 24.17% | 29 |
| Great Ocean Road Region | 13.33% | 16 |
| Melbourne | 41.67% | 50 |
| Interstate | 10.83% | 13 |
| Overseas | 0% | 0 |
| Total | | 120 |

| # | Other (please specify) | Date |
|----|--|--------------------|
| 1 | I ride in Forrest whenever i'm in Apollo Bay | 12/19/2013 6:50 PM |
| 2 | Gerangamete | 12/14/2013 6:47 AM |
| 3 | We have a weekender just outside of Forrest and we're there every second weekend | 12/11/2013 4:12 PM |
| 4 | Ocean Grove | 12/5/2013 7:57 PM |
| 5 | Mount Beauty Victoria | 12/5/2013 5:02 PM |
| 6 | Warrnambool | 12/5/2013 3:17 PM |
| 7 | Frankston | 12/5/2013 1:14 PM |
| 8 | South East Melbourne | 12/5/2013 12:59 PM |
| 9 | Momington Peninsula | 12/5/2013 12:49 PM |
| 10 | Hamilton | 12/5/2013 10:00 AM |

Forrest MTB Strategic Plan Survey

| | | |
|----|--|---------------------|
| 11 | Natimuk 3409 | 12/3/2013 7:33 PM |
| 12 | Melbourne | 11/28/2013 6:27 PM |
| 13 | Anglesea | 11/27/2013 2:32 PM |
| 14 | i move between colac geelong and borregurra | 11/25/2013 11:38 PM |
| 15 | birregurra | 11/25/2013 8:58 PM |
| 16 | 8 months a year in Geelong, 4 in France | 11/25/2013 8:43 PM |
| 17 | Ballarat | 11/25/2013 8:29 PM |
| 18 | Bendigo | 11/25/2013 6:12 PM |
| 19 | Gippsland | 11/25/2013 1:54 PM |
| 20 | anglesea | 11/25/2013 1:43 PM |
| 21 | Mulgrave VIC3170 | 11/25/2013 12:54 PM |
| 22 | Apollo Bay | 11/25/2013 9:56 AM |
| 23 | Apollo Bay. Funny that it isn't an option here. Typical COS! | 11/25/2013 9:03 AM |
| 24 | Work in Geelong, live In Torquay. So the plan this summer is to ride the You yangs during the week, Forrest every other weekend. | 11/24/2013 8:53 PM |
| 25 | Warrnambool | 11/24/2013 7:56 PM |
| 26 | Halls Gap | 11/22/2013 9:57 PM |
| 27 | warmy | 11/22/2013 7:17 PM |
| 28 | Central Coast, NSW | 11/22/2013 7:10 PM |
| 29 | Warrnambool | 11/22/2013 6:25 PM |
| 30 | Brisbane | 11/22/2013 4:40 PM |
| 31 | Ballarat | 11/22/2013 4:11 PM |
| 32 | We own MTB accommodation near Forrest and stay there when we can. | 11/21/2013 10:32 PM |

Attachment 3.
Community Letter



Forrest MTB Strategic Plan

Dear local resident,

Colac Otway Shire has commenced a Strategic Plan for the mountain bike trails in Forrest and we welcome your participation. The Plan will consider key issues, opportunities and consult with key stakeholders, agencies and the local community, to develop a future direction for Forrest's MTB trails.

Forrest Mountain Bike trails consist of a network of 16 Trails of approximately 66km in length. There are two trail heads with car parks, signs, toilets and shelter. The trails link to the recently completed Tiger Rail Trail Stage 1 that will eventually link Forrest to the township of Birregurra.

Previous consultation and other planning projects by the Shire identified exciting tourism and economic opportunities for Forrest, building on the proven strengths and growing Forrest Mountain Bike experience.

Council aims to develop a 'vision' for the MTB strategy via engaging with the local community, mountain bike and township stakeholders. This will inform how Council balances future development of the MTB facility with tourism services and destination products in Forrest.

As part of the community consultation process, the Colac Otway Shire invites you and your family to attend an Informal 'Drop In' Comment and Feedback Session to provide local resident and community input to the development of the Forrest MTB Strategic Plan.

The purpose of the 'Drop In' session is to discuss issues and opportunities that will inform the development and vision of the Strategic Plan. Please drop in and tell us your stories and experiences.

The details of the community 'Drop In' session are as follows:

**25th September 2013
5:00pm - 7:00pm
Forrest Hall**

If you are unable to attend, your feedback would be appreciated. Please provide feedback to the project manager:

Mike Barrow
Manager Economic Development
Phone: (03) 5232 9450
Email: mike.barrow@colacotway.vic.gov.au



Attachment 4.
Background Documents Review

SIMONMCARTHUR
TOURISM & BUSINESS DEVELOPMENT
AND ASSOCIATES

Background document review supporting
Forrest Mountain Bike Trails Strategic Plan

FEBRUARY 2014

Simon McArthur and Associates | 112 Cleary St, Hamilton NSW 2303 | 0427 240 664 | simon@smcarthur.com | www.smcarthur.com

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 AND ASSOCIATES

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EXECUTIVE SUMMARY



Executive Summary

This Report provides an interpretation of salient recommendations from plans and reports relating to tourism within Forrest and immediate surrounds.

The two most relevant reports were:

- Tourism Plan for Public Land in the Otways Hinterland, which provided a comprehensive analysis of tourism across public land and strategic yet detailed recommendations to grow nature tourism

- Forrest Structure Plan, which provided Forrest specific recommendations for mountain bike facilities and services

The most immediate finding was the number of recommendations directly focusing on improving and developing more mountain bike routes and associated experiences, facilities, services, events and promotion across the Region. Potential routes proposed included:

1. A collection of half-day mountain bike routes along existing unsealed roads, designed to reflect varying levels of difficulty and remoteness
2. A much more challenging and specialised mountain biking experience

3. A two-day road tour circuit out of Colac that utilises the Old Beechy Line Rail Trail
4. Improvements to trails around Stevensons Falls and Lake Elizabeth that include mountain biking options
5. A new Rail Trail between Forrest and Birregurra

It is unlikely that every proposed new route / experience will be implemented. The development of mountain biking within the Forrest area should be done within the context of regional mountain bike development initiatives identified above. This means checking to determine the status and likely implementation of these initiatives, followed by consideration of the Forrest initiative in the context of those implemented or likely to be implemented. The goal should be to create differentiation and avoid unnecessary and disadvantageous duplication.

A number of recommendations have been made to improve mountain bike facilities within Forrest, including:

1. Town based equipment hire and tour groups to support tourist recreational needs
2. A bicycle sales, hire and repair store

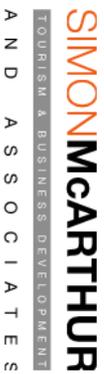
A number of recommendations have been made to improve mountain bike services within Forrest, including:

1. Bicycle racks near commercial businesses and lockable storage space at the mountain bike trail heads
2. Public showers and lockers in association with existing public toilet and picnic area at the mountain bike trail heads

One target market for Forrest has been set by the Otways Public Lands Tourism Plan using the Roy Morgan Values segment known as Socially Aware, and this should be taken into account for this MTB Project

A regional brand / positioning has been set for the region, and this should be taken into account when setting mountain bike marketing initiatives for this MTB Project.

DOCUMENT REVIEW



1. Tourism Plan for Public Land in the Otways Hinterland

Report prepared by Missing Link Tourism Consultants (Simon McArthur) and adopted by Victorian Department of Sustainability and Environment 2005. The implementation status is unknown.

Key relevant points

- The report provides a comprehensive analysis of tourism across public land and strategic yet detailed recommendations to grow nature tourism. The following eight strategies are the main result:
 1. Establish a tourism zoning system for the Hinterland, classifying the Hinterland into an independent and an assisted zone, catering for the needs of independent and guided visitors respectively
 2. Concentrate tourism development and marketing within three tourism nodes, including a major nature and rural node within the Assisted zone based around
 - Forest, West Barwon Reservoir and Lake Elizabeth.
 - New events linking Lake Elizabeth, the West Barwon Reservoir and Forrest; and
 - Mountain biking along the less developed and less travelled routes.
 3. Split the existing touring Route 22 into two different driving experiences, and promote and maintain one four wheel drive route within the region.
 4. Deliver additional capacity building to further develop skills in the areas of product development, customer service, interpretation, guiding and local environment knowledge.
 5. Develop additional soft adventure and ecotour experiences, and support the development of the Forrest/West Barwon Dam/Lake Elizabeth Node with:
 - A guided half day walk in the Lake Elizabeth area with a local character;
 - Dining and accommodation in Forrest, such as the Forrest Guesthouse, self contained cottages and bed and breakfast;
 - Cultural functions and events, such as the Music Mayhem and cinema in the local hall;
 6. Provide assistance for the consolidation of existing events and establishment of new nature-based events near townships
 7. Develop a significant nature-based accommodation experience that acts as a hub for related activities – the two optimal sites identified were Victree/Triplet Falls and The Cumberland Station
 8. Refocus regional marketing emphasis to more accurately depict the brand and reduce tourism promotion of certain sites and experiences
 - The Strategy recommended the development of three forms of mountain bike experiences:
 1. A two-day road tour circuit out of Colac that utilises the Old Beechy Line Rail Trail and accommodates visitors at Gellibrand,

DOCUMENT REVIEW

Beech Forrest or Forrest – an operator should be assisted to provide services such as the hire of mountain bikes, supporting gear, food and beverages and matching accommodation. The road tour experience would involve starting from Colac and travelling south down the Old Beechy Line Rail Trail. Depending on the time of the day, cyclists could then stop overnight at Gellibrand or Beech Forrest, and return to Colac via Lardner Track, Sayers Track to Stevensons Falls, Upper Gellibrand Road, Roadnight Creek Road to Forrest than via Barongarook State Forrest to Colac. The experience should be promoted as requiring minimal planning or equipment. It should package bike hire, day ration packs, accommodation, and even transportation of personal belongings. Reinstate most (if not all) of the Old Beechy Line as a multi-use trail between Colac and Beech Forrest. The Old Beechy Line is one of the few historic sites that can be actively used rather than passively viewed. If sensitively redeveloped, the experience would give visitors the chance to place themselves in the mindset of the historic railway use. If creatively interpreted, the development will add richness to the physical experience of riding or walking, by helping visitors to imagine the function and people behind the historic use of the Olways. The initiative will add a valuable experience that would consolidate the role of the Forrest and Beech Forrest nodes. To maximise economic impact, the product should offer a pick up and drop off service from at least one of the nodes, and could be tailored into a guided experience offering interpretation and lunch.

2. A collection of half-day mountain bike routes along existing unsealed roads. There are several existing unsealed roads that could form half-day mountain biking routes for people staying within the region. The rides should be designed to reflect varying levels of difficulty and remoteness. Relatively easy rides could be: Olway Fly/Tripolet Falls; Phillips Track (presently overgrown); Halls Ridge Road; Aire Crossing Track; Youngs Creek Track; and Browntown Track. More challenging rides in a semi remote area could be Carlisle State Park. A challenging ride in a remote area could be the Curtis Track/Kennet River Road loop.

3. A much more challenging and specialised mountain biking experience. The half to full day experience would include twisting Forrest paths and narrow challenging single track sections and down hill runs.

Implications for Project¹

- The development of mountain biking within the Forrest area should consider the context of regional mountain bike development initiatives addressed above. This means checking to determine the status and likely implementation of these initiatives, followed

¹ The recommendations in this report pertaining to development of on road MTB activities may be now less relevant than the current emphasis on further development of existing mountain bike trails systems in Forrest which are well serviced by recent improvements in MTB services, MTB hire, support facilities, diverse accommodation options and F&B within the township environs

DOCUMENT REVIEW



by the consideration of the Forrest Initiative in the context of those implemented or likely to be implemented. The goal should be to create differentiation and avoid unnecessary duplication.

- The Forrest MTB project may fit the third strategic initiative as a more challenging and specialised experience, with the development of more challenging rides.

- The target market for Forrest is the Roy Morgan Values segment known as Socially Aware, and this should be taken into account for the Forrest mountain biking development initiatives.

- Marketing the Forrest mountain bike initiatives should be done in context with the regional brand presented in the Report.

2. Feasibility Study into increased economic activity in Forrest

Report prepared by Regional Innovation and adopted by Colac Otway Shire Council 2005. The implementation status is unknown.

Key relevant points

- The Report draws heavily on the analysis and recommendations of the Tourism Plan for Public Land within the Otway Hinterland (profiled above).

- The Report undertook consultation to identify the following attributes that local stakeholders liked about Forrest:

- Coast and the bush, close to Apollo Bay and Lorne, Colac and Geelong
- The natural environment, abundant bush and wildlife, native birds, West Barwon Dam
- Peace and quiet of town and natural beauty and small village atmosphere
- Friendly and safe nature of the place and people

- Community spirit, good & friendly people
- Not overdeveloped and availability of affordable housing
- The lack of tourists
- The way it was
- History of saw-milling industry, logging past
- Opportunity to develop hinterland hub

- The Report addresses a mix of public sector and private sector funded projects and initiatives, including:

- Establish Forrest as the northern Gateway and service centre to the Otway Ranges National Park and Otway Forrest Park
- Improve entrance signage to Forrest with a view to removing poor signage and introducing more attractive signage likely to get visitors to stop.
- Develop a Visitor Information / Interpretation Centre at Forrest for the Otway Ranges National Park and Otway Forrest Park, interpreting the heritage of the Region and acting as a collection point for walks, rides and tours

DOCUMENT REVIEW

- Redevelop the Old Mill site for a mixed use that acts as a catalyst for further local development (in progress)
 - Establish new public toilets and improvement of the landscape / streetscape in Forrest
 - Increase the range of visitor accommodation (particularly caravan park cabins and an ecolodge within the Forrest region, linked with the Otway Ranges NP and Otway Forrest Park. (in progress)
 - Increase recreation activities at West Barwon Reservoir (such as fishing and canoeing²)
 - Seal Turtons Track to form part of the Otways Tourist Route for all weather travel (completed)
 - Establish a touring route that incorporates the coast and the Hinterland (particularly Forrest)
 - Develop a website promoting Forrest (completed)
- The Report supported mountain biking recommendations within the Tourism Plan for Public Land within the Otway Hinterland and also recommended the development of two additional of mountain bike initiatives:
1. Trails around Stevensons Falls and Lake Elizabeth that include mountain biking options; and
 2. Rail Trail between Forrest and Birregurra
- Implications for Project**
- The Report extensively supports strategies from the Tourism Plan for Public Land within the Otway Hinterland, including recommendations for the development of mountain bike experiences – so the same implications apply for this Report.
 - The development of a VIC in Forrest needs a full feasibility Study, as many similar ventures have proved costly to develop and unfeasible to operate. The location needs to offer a safe access route for mountain bike riders and easy for vehicles to access
- The redevelopment of the Old Mill Site in Forrest is also a strategic initiative to be considered by this Project (in progress)
3. **Forrest Structure Plan**
 Report prepared by CPG for Colac Otway Shire 2011. The implementation status is unknown.
- Key relevant points**
- Report reviewed existing planning controls for township and adjoining lands to provide a framework for settlement and integrated development across land use, movement networks and public spaces. In addition to identifying opportunities for town expansion, town centre revitalisation, the Report identifies opportunities for ecotourism and considers how Forrest will accommodate additional tourism infrastructure, such as:
 - Explore opportunities to develop gateway treatments to the north and south of Forrest at the entrances to Grant Street

² This recommendation is invalid because this is a water reservoir, and so no primary contact is allowed

DOCUMENT REVIEW

- Encourage the provision of adequate car parking facilities in proximity to tourist and recreation destinations
 - Encourage the development of tourist infrastructure including bed and breakfast, group accommodation, farmers markets, rural produce, café, restaurants eateries and the like
 - Investigate opportunities for group accommodation to locate on the old Timber Mill site (in progress)
 - Encourage niche business to locate in town in the form of cellar door, rural produce stores and the like
 - Facilitate opportunities to locate recreational events in Forrest (completed)
 - The Report also provides recommendations specific to mountain biking:
 - Encourage the development of town based equipment hire and tour groups to support tourist recreational needs (completed)
 - Encourage the development of a bicycle sales, hire and repair store (completed)
 - Explore opportunities to develop public showers and lockers in association with existing public toilet and picnic area at the mountain bike trail head locations in the north of town
 - Encourage an expansion of the mountain bike facilities at the mountain bike trail head
 - Encourage the provision of bicycle facilities, including bicycle racks near commercial businesses and lockable storage space
- Implications for Project**
- The recommendations listed above are still relevant and several are directly relevant to this MTB Project, and so their level of support and implementation status should be further investigated.
 - Though the above recommendations are given priority, it is uncertain what may actually eventuate in implementation as the Report uses non-committal language such as Encourage, Investigate and Explore to caveat all recommendations.
- 4. Kona Odyssey 2012 Event Report**
- Report prepared by Rapid Ascent (event coordinator) for sponsors, event website used to present event description. The implementation status is unknown.
- Key relevant points**
- Annual event based in Forrest in late April featuring bike expo, food stands and live entertainment and three races:
 - 100km out and back to Forrest via flowing single track featuring a solid mix of climbing and descending, designed for experienced and fit riders
 - 50km out and back to Forrest via mix of dirt roads, double track and single tracks, designed for novices and newcomers
 - 15km loop of 4WD tracks, swooping single track, rainForrest, roads and country scenery, designed for kids and new comers

DOCUMENT REVIEW



- Attracted 6,446 competitors and spectators to Forrest over the weekend
 - 1,176 competitors, 90% male dominated, 56% from Melbourne, 19% from interstate, none from international
 - Each competitor spent on average \$391 over the race weekend (\$156 accommodation, \$72 petrol, \$99 meals, \$24 sponsor products, \$19 food at expo, \$21 tourist activity)
 - \$3,864,090 direct economic impact and \$2,997,888 indirect impact
 - Lots of social media marketing and MTB magazine editorial and promotion, good stats on marketing activity
 - 62% of competitors said it was in the top three events
- Implications for Project**
- No research done to determine return rate to Forrest from competitors or spectators
 - No research done to identify demand for additional accommodation in Forrest (given there is insufficient supply that spills over to Apollo Bay)
- 20% of competitors would like the 100km race to start and finish in Forrest, but 65% want no change
 - 56% of competitors said the tracks flowed without congestion but 23% found sections of the course too difficult
 - The Event website (<http://www.rapidascend.com.au>) does not include a page or link to assist MTB users to return to the area and ride outside the event weekend
- 5. Tourism Economic Impact Analysis Colac Otway Shire**
- Report prepared by Compelling Economics for the Colac Otway Shire and Geelong Otway Tourism 2012.
- Key relevant points**
- This report examines the contribution made to the Colac Otway Shire by the various industry sectors that comprise the tourism industry and highlights the importance of the tourism industry to the Colac Otway Shire.
 - Tourism represents 7.4% of the total employment in the Shire (643 jobs), compared to 5.6% for the Great Ocean Road Region.
 - The major tourism related activities in Colac Otway include accommodation & food Services (455 jobs), retail trade (86 jobs) and arts & recreation Services (35 jobs).
 - For every dollar of direct expenditure by visitors to Colac Otway Shire, the broader local economy is estimated to benefit by a further \$0.65 once flow-on industrial and consumption effects are taken into consideration. This can also be expressed as a tourism output multiplier of 1.65.
 - Applying the tourism industry multiplier of 1.65 to the total output generated by Colac Otway Shire's Tourism sector of \$104.7 Million, the total value (direct + industrial + consumption) of tourism to the municipality's economy is estimated at up to \$172.7 Million. This economic output is estimated to support an additional 937 jobs in the region.

Background document review supporting Forrest Mountain Bike Trails Strategic Plan 9

DOCUMENT REVIEW

- Peak population for the Shire was calculated at 46,560 – Forrest is just 957 (2%), made up of:
 - 324 permanent population, 473 holiday home population, 100 caravan park population, and 60 hotel/motel/B&B population.

Implications for Project

- This Report has limited implications for the MTB Project.

6. Tourism Economic Analysis The Great Ocean Road 2011

Report prepared by Compelling Economics for Regional Development Australia as part of the Great Ocean Road Tourism Destination Management Plan 2011.

This Report presents visitor and economic data at regional levels and is not considered locally specific enough to be useful to the Forrest MTB Project.

Attachment 5.
Benchmarking Analysis

SIMONMCARTHUR
TOURISM & BUSINESS DEVELOPMENT
AND ASSOCIATES

Benchmarking Analysis and summary of
existing Mountain Bike Destinations

FEBRUARY 2014

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EXECUTIVE SUMMARY

I. Executive Summary

I.1. Background

This report provides the results of a website-based benchmarking of primarily mountain biking trails within Australia and to a limited extent, internationally. The objective of the benchmarking was to profile each destination:

- Apparent target market (based on what the website said, or second best, what it inferred through trail type and imagery)
- Positioning (the essence of the mountain biking destination, if it was refined and branded)
- Product (the trails on offer (length, difficulty, character and landscape), any guided tours, camps, events, bike rental service, lifts up mountains and ancillary services (food and beverage, accommodation etc))
- Promotion (how the destination is promoted and what is promoted)

- Cost (whether the experience is based on a use fee, fees for access or no fees at all)
- Other information (imminent plans for expansion, partnerships in operation and management etc)

In some cases, there is more information for some destinations because more information was available within their promotional material. The benchmarked destinations were then separated into:

1. Commercially run mountain bike destinations (destinations where a charge for access to the trails was entirely or largely present within promotion)
2. Non-commercial mountain bike destinations (destinations where there was no entrance fee or use fee promoted)

Three commercially run mountain bike and eight non-commercial destinations were benchmarked, which included Forrest:

The order of the benchmarked sites is roughly equivalent to our interpretation of their competitive strength in the market place. In our view, Forrest therefore sits approximately in the middle of the total collection of benchmarked destinations.

I.1.1. The most competitive mountain bike destinations

The Alpine mountain resorts have several strategic advantages over the other benchmarked destinations:

1. They have significant infrastructure, services and businesses established for the winter ski season that can be harnessed to service mountain bikers
2. Their geography (as a mountain) sometimes gives them control over access to the top of the mountain, from which downhill riding starts from – so they can charge for providing this access and use the fee to support development and maintenance of the mountain bike product

EXECUTIVE SUMMARY



3. They have established, significantly scaled commercial businesses to help plan, develop, operate and promote mountain biking at their destinations

Consequently, these destinations have built some of the best product that we benchmarked. There is significant diversity in product (including grades, technical skills and landscape). These destinations don't just provide lots of trails, they provide lots of differentiated trails. There is also much value adding of the base trail product, including guided tours, camps, events and ancillary services – even bike hire offers different types of bikes.

The support services and infrastructure (carparks, accommodation, food and beverage and retail) is typically based close to the trail starting / finishing point, making the experience simple, convenient and efficient. In addition, the scale of these facilities make it easy for an event to services most if not all of the participants 'on site' and close together – further enhancing the atmosphere.

Marketing done by these destination managers is also significantly superior to most of the non-commercial sites, evidenced by superior imagery of the experience and landscape, use of well produced videos and effective use of social media and events to drive brand awareness. Logistical information was also superior, such as the way trails were presented on interactive maps and the way they were profiled so the customer could easily compare and choose. These destinations do not shirk from pricing and present comprehensive pricing information with competition profiled and opportunities for discounts available.

1.III. Product development opportunities for Forrest

Single tracks that function as a (integrated set of cascading and connected) circuit(s) present more powerfully than (disparate) networks, and they present more powerfully than one way routes requiring the rider to turn around and ride the same route back to the start back to the start.

Successful destinations offer the majority of their product in the intermediate to advanced skill markets, but manage to attract wider markets by having a minority of trails set aside for beginners / low fitness and / or deviations from the main route that present some respite before returning to the challenging main route.

Within a network of trails there should be a lead experience that brands the overall destination and is used to generate the main awareness and motivation to visit (like Mount Everest is to Nepal).

The customer is looking for a seamless transition between where they stay and where they ride, this can be achieved by:

- Having trails lead off from the accommodation node area; and / or
- Providing transport between the track and accommodation (be it a chairlift or a shuttle service)

Beginner and skill development trails could be designed to include rest / picnic areas for

EXECUTIVE SUMMARY

accompanying friends and family to watch and photograph them in a picturesque and slightly exciting section of the trail

There are few examples of destinations describing their trails beyond the logistical elements (distance, time, gradient and difficulty).

There is an opportunity to design trails to feature more of the features and diversity in a landscape, and more character in the build and technical elements. There is also an opportunity to name trails to reflect this character and some degree of challenge within it, such as the at Forrest.

The hiring of quality mountain bikes close to the trails is almost essential, and the hiring of different types of bikes and good quality equipment is moving towards being expected. Clear and totally consistent sign posting is a given not a value add. Events are a very effective way to raise brand awareness. Specific opportunities for Forrest are:

- To be designed to highlight the brand character of the destination to demonstrate its strengths;

- to go beyond competition racing to add complimentary experiences, stand out from the mix; and
- to be integrated with an overall marketing plan that uses the event to trigger return visitation to ride – particularly from the spectators.

1.1.V. Marketing opportunities for Forrest

Positioning the mountain biking destination should:

- lead with its signature trails and place supporting product behind this, in less detail;
- include accommodation and food and beverage that matches the needs of the target market supported by web based information

- promote the genuine country ambience of the Forrest township experience as a destination value add for riders and visitors

Most destinations are targeting intermediate to advanced riders under 35 years of age, almost always with pictures of male riders. Only one destination offered anything to target women (skill improvement clinics), so this segment as a group is an opportunity for growth.

A specific website for the destination allows richer and more comprehensive marketing to occur. Destinations only profited by land managers are unlikely to be differentiated and made competitive in a crowded market place.

Videos are essential to portray the mountain bike experience and the lead trails. These need to be constantly refreshed and should be loaded onto a website and used with viral (?) campaigning. Introduce a challenge that attracts visitors to do all the trails or come back several times within a given period, positioned as a competition with a prize.

EXECUTIVE SUMMARY

Clubs have been an effective way to generate repeat use, and resources for trail maintenance, improvement and management. They can also be used to provide an active backdrop (like an event) and to host new visitors.

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Benchmarking Analysis and summary of existing Mountain Bike Destinations

IV

BENCHMARKING ANALYSIS

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A N D A S S O C I A T E S

1. Commercially run mountain bike destinations

- Thredbo river), Pipeline Trail (2 return km flat trail) or the Cascade Hut Trail (22km return moderate, difficult ride along the Alpine Way).
- There is a mandatory two hour safety induction tour for either the Thredbo MTB Downhill or Kosciuszko Flow Trail.
- Bikers can hire all biking equipment or bring their own equipment.

1.1 Thredbo MTB Trail (Australia)

Market

- The design of the website suggests young riders under 35 years of age, from intermediate to experienced bikers

Position

- Easy valley riding and exciting downhill trails through mountainous terrain

Product

- Mountain biking is offered in summer (November to May) and there are five key experiences on offer:
 - Valley Trails is a total of 41.5km of return trails with more easier or more technical routes such as the Thredbo Valley Track (7 return kilometers of gentle gradient riding along the
- Kosciuszko Flow is a 5.8km trail for intermediate bikers. It is a gravity fed flow trail that goes from the top of the Kosciuszko Express to the Village. It is filled with rollovers and smooth berms, taking a less direct route down the mountain, using the chairlift system
- The Thredbo MTB Downhill trail is the main experience – a 3.8km trail for advanced bikers filled with jumps, Rock drops and fast single track. It is the longest lift accessed downhill mountain biking run in Australia.
- Personal guided mountain bike tours are offered for groups
- Private mountain biking lessons for all ability levels
- Bikers can hire all biking equipment or bring their own equipment.
- All riding attracts a use fee.
- Lift only access (included Leisure Centre access):
 - 1 day: Adult \$69 – Child \$49
 - 1pm start: Adult \$49 – Child \$29
 - 2 day: Adult \$119 – Child \$84
 - 3 day: Adult \$159 – Child \$112
- Sample MTB Access – 1 Ride (for those wanting to give MTB a try. This is an upgradable product to a full day MTB Access Product)
 - 1 Ride: Adult \$33 – Child \$18
 - MTB Season Pass: Adult \$455 – Child \$275

Benchmarking Analysis and summary of existing Mountain Bike Destinations

BENCHMARKING ANALYSIS



- Lift & Downhill rental Package (including pass, bike and equipment):
 - 1 day: Adult \$201 – Child \$171
 - 2 day: Adult \$302 – Child \$241
 - 3 day: Adult \$393 – Child \$312
- Lift & Flow rental Package (including pass, bike and equipment):
 - 1 day: Adult \$181 – Child \$150
 - 2 day: Adult \$282 – Child \$221
 - 3 day: Adult \$373 – Child \$292
- Promotion
 - The main promotion is through pages within the Thredbo Alpine Village website (<http://www.thredbo.com.au/activities/summer-activities/biking/>), featuring pricing, but also including an overall route map, and information on the experiences and events. A mountain biking brochure can be downloaded from the website.
 - The operators have generated significant social media activity. Facebook (Thredbo Alpine Village) has 61,259 Likes and Twitter (Thredbo Alpine Village) has 4,534 Followers
 - Events such as Australian MTB Interschools (MTB Championship) and NSW/ACT State Series Event Rocky Trail Downhill/Roller Coaster (State Race) are used to drive brand awareness and trigger special visits.
- **1.2 Whistler Mountain Bike Park (Canada)**
 - Product
 - 124km north from Vancouver
 - There are two types of experiences on offer:
 - 1,507 vertical meters of lift-serviced descending mountain bike trails, featuring the Peak zone trail: expanding the Park's vertical by over 1,000 feet for a total of almost 5,000 feet of riding; and the Garbanzo Zone trail: 2200 vertical feet of brand new single track trails
 - Within the park there is an Air Dome: an 8,400 square foot covered indoor mountain bike training facility with a huge foam pit, ramps, and a quarter pipe and half pipe.
 - Market
 - The design of the website suggests young riders under 35 years of age, mainly advanced and expert bikers wearing full MTB equipment (helmet, gloves etc.)
 - Position
 - Gentle, banked cruising, tight and twisty single track, steep rock faces, gnarly, root strawn lines, drop offs
 - Trails only open in summer (from May to October)
 - Bikes can be rented

Benchmarking Analysis and summary of existing Mountain Bike Destinations 8

BENCHMARKING ANALYSIS

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| <ul style="list-style-type: none"> A number of events are offered such as the Crankworx Freeride MB Festival The Park provides amenities, pub, restaurant, bike rent shop, bike sale shop and repairs and 2 skill centers, a jump park, drop off park, and the Boneyard Slopesyle Park. | <p>Promotion</p> <ul style="list-style-type: none"> A destination specific website (http://bike.whistlerblackcomb.com) presents a comprehensive and attractive profile of opportunities and logistical support information, dramatic images of professional bikers doing challenging jumps and bike tricks, and YouTube video showing expert bikers doing challenging jumps and fast turns on the trails | <p>1.3 Mount Buller</p> <p>Market</p> <ul style="list-style-type: none"> The steep and varied terrain and the design of the website suggest intermediate-advanced bikers <p>Position</p> <ul style="list-style-type: none"> Designed and built by a World renown designer to deliver technical challenges blended into natural alpine environment <p>Product</p> <ul style="list-style-type: none"> Mt Buller is 240km northeast from Melbourne and operates as an alpine destination with skiing in winter and a range of well-developed adventures in summer. There are over 100km of trails (including 40km of single track), with 14 cross country trails and eight downhill trails well profiled on the Mount Buller website: The cross country MTB trails are: |
| <p>Cost</p> <ul style="list-style-type: none"> All riding attracts a use fee. Air Dome season pass Can \$240 Lift Tickets (Late season) <ul style="list-style-type: none"> - 1 day: Adult \$53 – Child \$31 - 2 day: Adult \$106 – Child \$62 - 3 day: Adult \$153 – Child \$89 Lift Tickets (Regular season) <ul style="list-style-type: none"> - 1 day: Adult \$61 – Child \$35 - 2 day: Adult \$122 – Child \$71 - 3 day: Adult \$152 – Child \$90 Sampler Ticket Early Season: Adult \$28 – Child \$15 Sampler Ticket Regular Season: Adult \$31 – Child \$17 | <ul style="list-style-type: none"> A Trail brochure can be downloaded from the website (maps, trail description, difficulty level etc) The website also drives social media: Facebook with 44380 Likes, and Twitter with 16700 followers. The Whistler Mountain Bike Park sponsors 5 professional mountain bike athletes, who are used to drive brand awareness of the Mountain Bike opportunities | |

Benchmarking Analysis and summary of existing Mountain Bike Destinations 9

BENCHMARKING ANALYSIS

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TOURISM & BUSINESS DEVELOPMENT
 A N D A S S O C I A T E S

- | | | |
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| <p>1. Split Rock (760m moderate to steep climbing through a number of switch backs and boardwalks, medium fitness)</p> | <p>endurance, medium technical skill)</p> | <p>to high level of fitness and technical skill)</p> |
| <p>2. Gang Gangs 3km, Narrow single track gently descending then returning to the Village on the Split Rock return single track, intermediate)</p> | <p>6. Picnic Trail (2.3km, Moderate descent on fire trail onto contouring single track and moderate ascent on wide fire trail, Medium endurance, medium technical skill)</p> | <p>11. Box Corner Link (600m, mostly flat, low fitness)</p> |
| <p>3. Village Family Trail (3km, Gentle upwards and downwards slopes low endurance, low technical skill)</p> | <p>7. Easy skills Trail (100m, mostly flat, low fitness)</p> | <p>12. Stonefly (10km, loop trail that climbs for 6km, then descends for 4km. Generally, the gradients are moderate, but there _are some short steep sections, high fitness</p> |
| <p>4. Delatite River Trail (9km, Moderately challenging wide descending trail with river crossings. Fitness: Medium endurance, medium technical skill, intermediate)</p> | <p>8. Intermediate Skills Trail (150m, mostly flat, low fitness)</p> | <p>13. River spur trail (2.5km, moderately challenging upward slope, medium endurance, medium technical skill)</p> |
| <p>5. Silk Lane (4km, Moderate downhill and uphill sections on fire trail and single track, Medium</p> | <p>9. Misty Twist (4.1km, moderate downhill and uphill riding on premium single track, medium endurance, medium technical skill)</p> | <p>14. Snowgums (420m, moderate climbing through a number of switch backs and boardwalks, medium fitness)</p> |
| | <p>10. Corn Hill (2.5km, Wide fire trail and narrow single track featuring technical steep ascents and technical fast descents), medium</p> | <p>• The eight Downhill trails are:
 1. Abom/Village Link (1.5km, undulating single track 1m wide with switchbacks)</p> |

BENCHMARKING ANALYSIS

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TOURISM & BUSINESS DEVELOPMENT
 A N D A S S O C I A T E S

- 2. Home Trail (2.5km moderate undulating via a 3m wide 4WD track)
 - In summer, the Mount Buller premium snow runs provide a downhill mountain bike course using the berms, ramps and jumps
 - Two operators rent downhill bikes, cross country dual suspension bikes and cross country front suspension bikes for half and full days and prices from both operators are listed on the website
 - A local operator offers guided tours and clinics to help learn to ride, improve riding skills or just enjoy the stunning scenery. Complete beginners can learn basic skills, while intermediate and advanced riders can enjoy riding with the local experts and learning new techniques to improve their riding
 - There are also several camps for men and women, and clinics just women, designed to improve mountain biking skills, packaged with accommodation, meals, skills development, support on the trails
 - Between November and March Mount Buller runs nine mountain bike events. The events range from high country challenge, endurance rides and uphill challenges. Several events combine racing with film festivals.
 - Mount Buller has promotes bike-friendly accommodation and there is even one property called Andy's Fat Tyre Lodge
- 3. Abom Downhill (2.2km moderate single track)
 - Cost
 - There is no charge for entry or use of the trails. However, use of access lifts up the mountain significantly increase the amount of downhill flow riding in a given day. Costs to access lifts up the mountain are:
 - Unlimited Northside Express chair lift day pass: \$60 per person
 - Two-day value pass: \$106
 - Summer Season Pass: \$368 per person.
 - Prices for guided tours are:
 - \$65 Mountain bike skills 101 (1.5hrs)
- 4. Yellow Post (1km moderate single track with drop offs to 0.5m)
 -
- 5. International (2km steep single track with drop offs to 2m, bridges at min width of 250mm and beams to a max angle of 75 degrees)
 -
- 6. Kingsporn Bridle Trail (8.65km difficult steep single track with lots of technical sections and open fire trail at the end)
 -
- 7. Outlaw Express (1km intermediate moderate to undulating track)
 -
- 8. Copperhead (3km beginner / intermediate flow down trail)
 -

Benchmarking Analysis and summary of existing Mountain Bike Destinations 11

BENCHMARKING ANALYSIS

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TOURISM & BUSINESS DEVELOPMENT
 A N D A S S O C I A T E S

- \$77 Immediate advanced skills (2hrs)
 - \$120 Downhill 101 (2hrs)
 - \$120 Downhill rush (3hrs)
 - \$145 Stonemly experience (4-5hrs), discounted with increased group size
- Promotion**
- Mount Buller has its own website (<http://www.mtbuller.com.au/Summer/>) which has a major menu for mountain biking that then clicks through to a purpose built site (<http://bike.mtbuller.com.au>). The purpose built site:
 - profiles the 14 trails with comprehensive and comparable information
 - features an interactive map for the trail categorising trails as: downhill, x-country, easy, intermediate and advanced, and also includes a map of the village;
 - provides an update MTB Trail Report page indicating current temperature,
 - wind direction and strength, trail status (open or closed), Bike Shuttle Status, current news and information and any trail work;
 - profiles the guided tour product and pricing;
 - includes a few videos showing intermediate and expert riders on a range of middle to challenging terrains; and
 - shows links to Facebook and Twitter Mt. Buller profiles, and mountain biking is a major part of the Facebook profile, despite the site having to cover all activities offered in the area
 - The seven special events are used to drive brand awareness and visitation to the destination and accommodation operators.
 - Mt. Buller sells merchandise promoting the mountain biking brand, such as T-Shirts, caps – hoodies and stubby holders.
- More information**
- Mount Buller is actively expanding and enhancing the product range. In summer 2014 they are planning to open the first Australia IMBA EPIC trail called the Mt. Buller's EPIC trail. Works began in December 2013. More info on <http://www.mtbuller.com.au/Summer/Bike-Buller/New-Trails-Infrastructure>
 - The destination also uses competitions to drive brand awareness. Bikers who complete the 7 peaks Alpine Ascent Challenge (mountain roads) within 5 months time get the chance to a prize. Every time a biker completes a summit he is awarded a stamp on his 7 Peaks Passport. This is valid from October 13 2013 to March 31 2014.

Benchmarking Analysis and summary of existing Mountain Bike Destinations 12

BENCHMARKING ANALYSIS

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A N D A S S O C I A T E S

2. Non-commercial mountain bike destinations

2.1 Ride Rotorua (New Zealand)

| | | |
|--|--|--|
| <p>Market</p> <ul style="list-style-type: none"> The design of the website suggests intermediate to experienced young bikers under 35 years of age <p>Position</p> <p>An adventurous and technical ride through the Forrest within close proximity to the city centre of Rotorua</p> <p>Product</p> <ul style="list-style-type: none"> There are several experiences on offer a range of trails from easy to difficult: <ul style="list-style-type: none"> The feature is a 2.5 hour ride (130km) without any repeated sections, without | <p>getting further than 2km in a straight line from the trailhead. The trail network is a combination of different trails within a Forrest.</p> <ul style="list-style-type: none"> MTB courses (full day or 2 day mountain biking workshops and introduction to mountain biking for school groups guided tours for mountain bikers with a good level of fitness adventure packages including kayaking, rafting, horse riding and indoor climbing A shuttle service is offered to transport bikers and their bikes to the top of a hill to access 30 km of down hill trails Amenities include car park, visitor centre, accommodation close to the trail, bike hire and repairs shop. <p>Promotion</p> <ul style="list-style-type: none"> The destination and facility has its own specific website (http://www.riderotorua.com/trails2.php?catid=1) which presents a comprehensive and attractive profile of opportunities and logistical support information. The key strengths of the website are: <ul style="list-style-type: none"> videos showing advanced and expert bikers doing small jumps and fast turns social media: the Facebook has 3400 Likes <p>Other info</p> <ul style="list-style-type: none"> There was no downloadable brochure on the website. There is a Rotorua MTB Club, and the trails are informally managed by Rotorua | <p>Cost</p> <p>Riding on the trails is free to all as the costs of building and maintaining MTB trails is covered by government, not for profit and philanthropists</p> |
|--|--|--|

Benchmarking Analysis and summary of existing Mountain Bike Destinations

BENCHMARKING ANALYSIS



Mountain Bike Club (Inc); Charitable Trust funding, freelancers

- The trail system is continually being extended and enhanced

Forrest reached via a rail trail link) and the Southern Trail Head (6 routes) which traverse across undulating and heavily forested land immediately adjacent to the Forrest township

- Roller Coaster, an easy 2km trail connecting the main Forrest trailhead. The route is a wide, smooth and free flowing trail ideal for beginners or as a warm up

2.2 Ride Forrest

Market

- The rideforrest website suggests more experienced and fit mountain bikers

- The **Forrest** MTB trails are:
 - Forrest Loop, a 3km dual use trail for walkers and cyclists of all

- Third Time Lucky, a 2km more difficult trail that features bermed corners and jumps

Position

Challenging – the majority of trails are moderate to advanced, with only 3 out of 16 trail designated as easy

- abilities. It has long flowing corners that descend into the West Barwon Valley and then climbs back to Forrest where it joins the Forrest Heritage Trail

- The **Yaughner** MTB trails are:
 - Follow the Dog: a more difficult 4.5km flowing trail through tall open Forrest and dense fern gullies, offering long climbs and fast flowing descents

Product description

- Challenging – 13 hard to very hard routes
- There are over 48km km of trails (with 15 cross country trails trails) well profiled on the rideforrest website in two separate areas; the Yaughner Trail Head (9 routes in a cluster 2km ride north of

- Baritjaru, a 6km more difficult trail that links Forrest to Lake Elizabeth and climbs most of the way to the Lake Elizabeth carpark
- Red Carpet, a 4.5km more difficult single track return trail from Lake Elizabeth, with challenging descents and tight technical climbs

- Yaughner Super Loop, an 11.5km more difficult trail that is a mixture of single track and fire trails, with A and B lines on all trails in the network
- Grasstrees, a 1.5km very difficult route featuring log rides, rollovers,

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- step-ups, drop offs. Big 4X berms and jumps
- Marinaers Run, a very difficult 4.5km trail with long climbs and fast flowing descents
- Foxtail, a 3.5km more difficult trail with long fast straights and open corners
- J2, a more difficult 4km long flowing descent trail from the Yaugher trailhead and joining the Yaugher Super Loop
- Yo Yo, a very difficult 3km up and down trail
- Casper Black, a very difficult 5.5km trail of twisting technical climbs and descents with walls of tea tree
- Barre Warre, a 2km easy trail offering high vantage points to view the Barwon River Valley and Forrest Township
- Vista, a more difficult 5.5km long flowing trail with fire trail descents and climbs, tough technical climbing and rewarding views
- Tiger Loop, a more difficult 3km link from the Forrest-Birregurra Trail to the Yaugher Trails network
- Mountain bike tours for small groups are available with a professional guide and mountain bike skill courses are for sale from local operators at the Forrest Corner Store
- Social rides are run by the Forrest MTB & Cycling Club (e.g. on Thursday nights is their regular night Brew Crew ride)
- Nearby in Forrest there are MTB support amenities: restaurant, coffee shop, bike hire and repairs shop, pub, and a range of MTB friendly couple and group sized accommodation, including converted houses and setup for MTB riders with
- cost effective accommodation and bike storage sheds
- The Forrest Camping Ground has low cost cabins and tent sites
- The Corner Store has a dedicated website offering a range of bikes for hire, tours, free trail maps, coffee and food, bike products, spares and merchandise promoting the Forrest mountain biking brand experience, such as T-Shirts and caps
- Directional and degree of difficulty signs are provided along the trail
- The two trail heads each provide: carparking, toilet, trail maps, shelter and seating (not water).
- The separation of the two trail areas creates some logistical separation for riders, especially access to the Yaugher trails network from Forrest due to distance and main road crossing

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Promotion

- There is a destination specific website (<http://www.rideforrest.com.au>) that blends the township of Forrest with the bike riding opportunities. The website includes some You Tube videos showing intermediate bikers on some sections of the trail doing small jumps and fast turns, a short a description for each route and its level of difficulty, whether the trails are open or closed, and a map of the region trails that can be downloaded.

Cost

- There is no charge for entry or use presented on the website

dirt roads to tight technical single track and extreme downhill tracks.

2.3 You Yangs MTB Trail

Market

- The design of the website suggests young riders under 35 years of age
- Race categories include junior, Under 15 and over 35

One area (The Stockyards was a former quarry rebuilt into a track system) offers downhill tracks overlooked by granite crags. These tracks feature natural rock ramps, rock gardens, fast runs and designed down hill runs.

Position

Downhill speed and danger, supported by an active Club. Narrow focus on high skill level and fast downhill riding

- Another area (The Kurrajong Plantation) is a 12km circuit offers flatter tracks for less experienced bikers. This circuit is also open to vehicles.
- Trails are signposted with names and difficulty ratings to prevent people getting lost or straying onto terrain that's beyond their skill level.

Product

- TripAdvisor.com.au has two reviews of Excellent and Very Good rating, but feedback suggests the trails are looking tired and need reinvigoration
- The Forrest MTB & Cycling Club runs the Olway Odyssey and Kona 24 hour MTB races based in Forrest
- 55km south west of Melbourne and 22km km north of Geelong
- 50km network of purpose built tracks of varying length and levels, from easy open
- Trails are often shared with runners, walkers and equestrians.
- Amenities and picnic tables are available on the The Kurrajong Plantation but not the Stockyards.

BENCHMARKING ANALYSIS

| | | |
|--|--|---|
| <p>Cost</p> <p>There is no charge for entry or use presented on the website</p> <p>Promotion</p> <ul style="list-style-type: none"> The destination website (http://www.youyansmblinc.com.au) focuses entirely on mountain biking activity. The site offers pictures, videos, events description, announcements etc. The destination is co-managed by the Club and Parks Victoria. There are three videos promoting the destination. One video is an amateur production for the introduction experience. The second promotes the fastest and most adventurous experience with helmet camera footage and hip younger person's soundtrack. The third starts with an interpretation of the construction of a new trail (Shepherds Track) and finishes the same fast and furious experience. | <ul style="list-style-type: none"> The Facebook page has almost 3,000 Likes. The website provides a downloadable map of the region and trail maps (covering the distance of the different trails, difficulty level, descriptions and characteristics and emergency information). The Destination also uses events to drive promotion. Events include the first De Grandi 6 hours All Mountain Enduro. Winners get prizes and or cash. There is an annual Club event (last on 22nd Sept. 2013) that offers music and shuttle. Different categories according to the level of expertise of the bikes. The event is run by volunteers and the only people paid in this event are professionals first aid. Winners get prizes and or cash. The Destination also has merchandise (including T-shirts and hoodies which display the You Yangs logo) to promote the brand. Though the website promotes | <p>merchandise it lacks a proper distribution system to order online.</p> <p>Other information</p> <ul style="list-style-type: none"> MB riding not permitted on walking tracks. There is a You Yangs Club and membership form can be downloaded from the website. The Club invites users to donate money to the You Yangs Trail Fund. The MTB club gets sponsorship by different businesses such as You Yangs Shuttle Service, getpixel, Prologica etc <p>2.4 Colac Otway Region (Old Beechy Rail Trail)</p> <p>Market</p> <ul style="list-style-type: none"> Some sections are suitable for families and beginners while some are only suitable for intermediate to advanced riders |
|--|--|---|

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| | | |
|---|--|--|
| <p>Position</p> <ul style="list-style-type: none"> Follow one of the state's former narrow gauge railways through an area of scenic Forrest, rich farmland and rolling hills – with the coast not far away | <p>Cost</p> <ul style="list-style-type: none"> There is no charge for entry or use presented on the website | <p>(http://www.colacotway.vic.gov.au/Page/faq.aspx?Page_Id=3454&h=0). It doesn't appear to be an official website about the Old Beechy Rail Trail.</p> |
| <p>Product</p> <ul style="list-style-type: none"> 150km and two hours west of Melbourne. The 45 km mountain trail can be completed in short sections and in either direction. It includes some gentle undulating slopes and some steep up and down hill sections. Directional signs are provided along the trail, but no interpretation about the former rail use was promoted. The trail surface is represented by large sections of purpose-built trail with compacted gravel and short sections of heavy gravel on dirt roads and also some sealed roads. The trail is shared between mountain bikers and walkers. | <p>Promotion</p> <ul style="list-style-type: none"> The lead promotion for this trail is a brochure that can be downloaded from The Colac Otway Shire Council website. The brochure includes information about how to get there, accessing the trail, a map of the trail including length of the trail, cross section of the rises and falls, difficulty, start and finish location and track conditions. The brochure breaks down the full route into seven shorter rides of 25-60 minutes duration, each profiled and rated in difficulty. It also presents local picnic areas, amenities, accommodation, restaurants and cafes near by the trail. | <p>2.5 Murray to Mountain</p> <p>Market</p> <ul style="list-style-type: none"> The design of the website suggests young riders under 35 years of age, mainly advanced bikers. <p>Position</p> <ul style="list-style-type: none"> A masculine focused food and wine linked experience for one route and an advanced technical challenge for the other <p>Product</p> <ul style="list-style-type: none"> Both country mountain biking and road riding trails are promoted through two trails: <ul style="list-style-type: none"> Victoria's High Country Brewery Trail: downhill MTB track extending from the rivers to the mountains. Along the Trail there are 4 local Brewery companies to |

Benchmarking Analysis and summary of existing Mountain Bike Destinations

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visit, sample beer and eat at the brewery restaurants

is a link to YouTube videos on YouTube.com showing intermediate bikers on the trails

2.6 Lysterfield Park

Market

- Dirty Dozen Mountain Bike Trail: a collection of 12 MTB trails located across North East Victoria. Includes unsealed roads through National Parks to more challenging single track, downhill and parks.
- Link to TripAdvisor.com.au: 23 reviews of which 14 show an Excellent Rating and 9 a Very Good rating.
- Brochure about the trails can be downloaded from their website. this offer a map of the trails and description.

- The website profile has limited information from which to derive the market. Local families are one target market but local riders looking for a moderate challenge may also be targeted, based on the product. There also appears capacity for group and event bookings.

Cost

- There is no charge for entry or use presented on the website

Position

- City based natural site with big trails and a Commonwealth Games Course

Promotion

- Promotion is largely driven through the breweries using the trail to visit them. A website promotes the brewery route at (<http://www.muraytomountains.com.au>). The website has limited information about the riding trail and riding experience (no distance or ride time).
- Social media used is: Facebook with 1,450 Likes and Twitter with 329 Followers. There

Product

- Located on the south-eastern edge of Melbourne.
- Offers 20 km of trails designed for mountain bike, some shared and some equestrian use. Easier trails run around the lake and parkland. More moderately challenging trails offer a range of flowing single trails through established plantations or steep

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and technical sections of trail through rocky landscapes.

- All trails are graded and sign posted with directional information.
- There is a visitor center in Trailmix, which includes amenities such as a café, bike store, information centre and recreational facilities. Mountain bikes can be hired in the Centre.

Cost

There is no charge for entry or use presented on the website

- There is no official webpage that specifically promotes the Lysterfield MB trails and experience.

- The MB product within is presented within the Parks Victoria website (<http://parkweb.vic.gov.au/explore/parks/lysterfield-park/things-to-do/mountain-biking>).

There is a small section dedicated to the

MTB experience in the park. No pictures or videos offered to Internet users.

- The Mountain Bike Riding brochure and a map of the Park can be downloaded. This includes information about how to get to the park, where bikers can cycle, expected bikers behavior and a map of the park indicating all the trails and their characteristics. the name and distance of the trails, description about the type of terrain and ride and facilities.

Other information

- The trail network is intimately linked to a local mountain bike group (Lysterfield District Trail Riders). Users and bikers are invited to become a member of the group, and do monthly working bees on the mountain bike trail network.
- The trail alignments have been designed from Parks Victoria together with the Mountain Bike Association of Australia.

- The number of groups using mountain bike trails is restricted during wet months (to maintain trail integrity). Small groups

include between 15 to 50 individuals and they have to book with the park office one week in advance to use the trails. Large groups (50+ individuals) require an event permit at all times and an application needs to be filled in six weeks in advance. Applications can be found in the Parks Victoria website.

2.7 Lliydale to Warburton Trail

Market

- Couples and families are the target market (a section of the Aqueduct Trail, 7km out of 20km) and The Rail Trail (an old railway alignment)
- Recreational riding groups of varying sizes are also acknowledged but no specific trail or promotional material supported there needs

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Position

- Part of a family outing to have a fairly easy ride to see Forrests and the Yarra Valley

is the original building from Lillydale Station and is now a museum.

using key words on Google (Warburton

Product

- Approximately 80km east of the centre of Melbourne, accessible from the suburban Lillydale Railway Station. Multiple road entry points. There are two trails.

- The most popular and easiest trail is the

Aqueduct Trail, which runs for 20km (East from Warburton) through the O'Shannassy

show a range of websites that only briefly mention mountain bike trails in Warburton and around Warburton.

- A Rail Trail runs for 40km west from Warburton (edge of Melbourne's suburbs) into rural landscape, then Forrested lower mountain landscape. Predominately a flat ride but the trail has a climb up to Mt Evelyn from both directions. It features use of former Warburton Railway line, and so includes former railway stations and associated infrastructure. Most of the original station platforms are still present with replica station signage, however many station buildings have been lost through time. The Station building at Yarra Junction

Aqueduct (built in 1914 and decommissioned in 1996).

- The top hit site (shown in the 3rd page of the Google search) was the Warburton Valley website

- The Cog Café in Warburton offers hire of mountain bikes, children's trailers, baby seats and tag-a-longs, as well as bike sales and a bike workshop. It is possible to hire one direction and pay for pick up.

- Some trails are shared-use facilities and they are also used by walkers, runners and horse-riders.

(<http://www.warburtoninfo.com/Things-to-do.html>) which provides limited information about mountain biking in the Warburton region.

- There is no charge for entry or use presented on the website

Cost

There is no charge for entry or use presented on the website

Promotion

- There is no specific website for this trail, and it was difficult to find a relevant website

A better profile of the trail was found through the Rail Trails Australia website (<https://www.railtrails.org.au/trail?view=trail&id=64>). This site includes information about trail features, description, access points, future developments, picture section, bikers comments and also a news section.

- The Warburton visitor website has a Facebook page, that is quite active in

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displaying bikers pictures, comments, and news, and has 633 Likes.

- There is a Twitter Trail Rail profile linked to the Rail Trail website with 139 followers, but it doesn't drive any promotion of brand or product.

Other information

- The Warburton Rail Trail is managed by the Shire of Yarra Ranges.
- The Yarra Glen Rail Trail has been proposed as an extension of the Warburton Rail Trail from Lilydale, which, when completed, will allow a Rail Trail experience from Yarra to Yarra, that is, from Yarra Glen to Warburton. This extension will bring the Warburton Rail Trail to more than 50 kms long.

2.8 Gippsland Region trails

Market

- The Walthalla Goldfields Rail Trail is suitable for most ages and fitness levels
- Some sections of Mount Baw Baw Trails are suitable for beginners and children, others are suitable for most ages and fitness levels, while some others require a good level of fitness.
- Some sections of the Gippsland Lakes Discovery Trail are suitable for most ages and fitness levels, while others require experience and a good level of fitness
- Some sections of Nowa Mountain Bike Park are suitable for beginners and children whilst others are for more experienced bikers with a good level of fitness
- Mount Hotham to Dinner Plain is suitable for most ages and fitness levels

Position

- The five trails profiled here are individually but quite generically positioned as riding opportunities in a natural landscape.

Product

- Some of the tracks are rail trails or similar, but there are also a couple of dedicated mountain bike parks with a great range of tracks to suit all ages and abilities
- The trails are single route return ranging from 20km to 65km and one is less than 10km. No networks or circuits were found. No pick up services were found promoted for the end of the one way routes.
- Easy trails were found to be a section of Erica Mountain Bike Park, Walthalla Goldfields Rail Trail, Mount Baw Baw Trails, Gippsland Lakes Discovery Trail, Nowa Mountain Bike Park and Mount Hotham to Dinner Plain.

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- Challenging trails (requiring prior riding experience and a degree of fitness) were found to be Tyers Junction Rail Trail, a section of Erica Mountain Bike Park, Mount Baw Baw Trails, Nowa Mountain Bike Park.
- there and some extra notes about the trail).
Maps are broad scale and not suitable for on-site use, and photos are generic and do not present any of the specific trails

Cost

- There is no charge for entry or use presented on the website

Promotion

- There are no trail websites specifically developed for any one trail – all trails within the Gippsland Region are promoted on the Visit Victoria website (<http://www.visitvictoria.com/Regions/Gippsland/Activities-and-attractions/Outdoor-activities/Cycling/Mountain-biking/Tyers-Junction-Rail-Trail>).
- The website page coverage is more logistical information than promotion. Only a brief description of each trail is provided (length of the trail, level of difficulty, type of terrain, where to start and finish, how to get

Appendix C.
Agency Technical Workshop



Forrest MTB Trails Strategic Plan

Summary Report of Agency & Technical Workshop & Site Visit held on 26 August 2014

Workshop and site visit objectives

TBLD were requested by the Project Manager to undertake a further technical workshop and site visit to specifically explore opportunities for implementation of strategic recommendations, in order to enhance and strengthen the implementation aspects of the Forrest MTB Strategic Plan project.

The three main objectives for discussion were to:

- Investigate by workshop and brief site visit specific opportunities for existing trail enhancement and or new trail development, with a focus on Forrest and to capture technical team input of opportunities that are synergistic with the objectives of the strategy. A workshop was held with key agency and stakeholder representatives to identify possible locations, types of trail enhancements or new trails, and a brief site visit was undertaken following the workshop, to visit and discuss locations where trails could be located, and what benefit they could bring to Forrest. The focus was on identifying and agreeing on priority projects that would be of most benefit to Forrest in the short to medium term
- Determine strategic budgets for identified trail enhancements and new trails, and
- Discuss with agency members options for governance, administration structure, next steps and timing for progression of the Interagency Management Group (IMG)

These three objectives were achieved, with strong agreement from the attendees as to the identified directions. The summary of the workshop and the site visit are summarised below and the budget estimate is attached.

Part A Agency & Technical Workshop Meeting

Tuesday 26 August 2014, 9.00am – 12.00pm, COPACC – Cnr Rae and Gellibrand Streets, Colac

Attendance:

| | |
|-----------------------|---|
| Michael Swanson (MS) | Business Development Officer, Colac Otway Shire |
| Craig Clifford (CC) | DEPI |
| Craig Woobridge (CW) | DEPI |
| Norm Douglas (ND) | Forrest MTB Group |
| Gwyn Hatton (GH) | Barwon Water |
| Glen Berrill (GB) | Director, Thompson Berrill Landscape Design |
| Andrea Szymanski (AS) | Senior Landscape Architect, Thompson Berrill Landscape Design |

Apologies:

| | |
|-----------------------|----------------|
| Stewart Anderson (SA) | COS |
| Michelle Anstee (MA) | Parks Victoria |

A1. Welcome and Introductions

Michael Swanson (MS) welcomed all to the meeting. Members at the meeting briefly introduced themselves as most had met at the previous Project Working Group Meeting. Attendees provided a brief summary of their role and previous involvement with the MTB trails.

A2. Brief Project overview of key new trail recommendations

- CC noted DEPI have been the key driver in delivery of MTB trails and trail heads, including ongoing maintenance with a crew based out of Forrest Depot. DEPI have 6 permanent officers with an additional 20 staff in peak season for fire prevention works.
- CC noted a few trails including Grass Trees, Marriners Run and Fox Trail have recently been improved/upgraded with funding from DEPI district funds. A. Mariner recently undertook approximately \$10K upgrade and improvement works with great success. The upgraded trails will be reopened shortly.
- CW and ND noted local volunteers (MTB group members) meet every 3 months with DEPI to discuss trail maintenance and prioritise works.
- ND noted the Forrest Cycle Club would soon cease being a 'cycle club' and become a 'trail care club'.

A3. Key findings and recommendations of the strategy

A3.1 Land ownership and management constraints/opportunities

- DEPI manage and maintain the existing trails on Crown land. They also manage trails on BW land.
- GH noted key BW issues would include BW admin, insurances, public indemnity, liability and heavy use of trails and events degrading the site.
- CC noted his discussions with David Rourke (DEPI) regarding trails on BW land, there is an informal working agreement that DEPI manage the trails on BW land, but no formal agreement (TBC).
- CC noted in the past 7 years, DEPI and BW have had no concerns raised between agencies regarding MTB trails.
- CC confirmed DEPI's risk management and trail maintenance is undertaken without BW input.
- GH noted BW has only 3 operators in the region and limited resources. BW is comfortable with the current arrangements that DEPI deals with and manages the MTB trails on BW land.
- GH noted BW are generally in agreement with the suggestion of additional trails on BW land, conditional on consideration of the following comments:
 - The MTB trails have the potential to spread weeds. CC noted DEPI would have undertaken weed control on verges of trails only.
 - Passive recreational areas, dog walking, BBQ and public toilet use, and user conflicts on walking trails.
 - Vegetation and environmental assessment of site – protection of vegetation.
 - Maintenance of track –DEPI and volunteer works group still to maintain trails.
 - Potential increased use of BW recreation facilities, BBQs, toilets etc., requiring a higher level of maintenance which BW cannot resource.
 - Risk assessment.
 - Minor vandalism of depot site, however more trails will provide improved passive surveillance at site.
 - BW public road used by MTB riders.

- User risk of water crossing points, service assets and diversion works.

Action: GH to provide MS (for supply to TBLD) BW digital data/plan of BW roads, maintenance, tracks, services etc.

A3.2 Identification of possible new trail in Forrest MTB network for more "difficult, challenging, downhill, flow, single track trails" to diversify and strengthen Forrest's appeal as key MTB destination

- GB noted the MTB strategy and consultation results clearly identified a need for more "trails that are different" in proximity to Forrest Township. The PWG reviewed the plans and contours and identified some definite areas for potential new trails and upgraded B Lines on existing trails. These were mapped and inspected on site (refer Dwg FMBT-04).
- Red Carpet is known as one of the most popular trails.
- CC noted the establishment of new trails could require a lengthy planning and approvals process. There is a huge potential for enhancement of existing trails with A and B Lines. The 2010 audit of the Forrest Trails by World Trails provides detailed assessment of the existing trails with recommendations and schematic designs for trail improvement works.
- ND noted MTB riders like to re-ride trails to improve their skills, and to have fun and challenge themselves.
- CW notes the trails need to comply with IMBA standards regarding proximity of parallel trails to reduce short cutting, goat tracks, and damage to vegetation.
- The group discussed the opportunity to establish new trails on BW land, a sloping unused area below the treatment plant – a 2-3km 'blue' trail loop with challenging "B Line" sections for more skilled riders and direct links to town, satisfying repeat runs and appropriate for users with physical ability and for general family and intermediate MTB fun.
- ND noted the opportunity to use Fern Gully track (an existing walking track). The group agrees this needs more thought to ensure all recreational users are provided for.
- GB noted Forrest needs to retain other recreational options for non-MTB users.
- ND noted there is a high risk of conflict on the Rail Trail due to the high speeds achievable by riders.
- CW noted Forrest trails are more difficult to maintain than Yaughar due to the topography, soil, etc.
- Follow the Dog – review the current classification as it is marked Blue but due to the degree of physical demands of the ride it is closer to a black trail
- Magnificent diversity of vegetation and landscape setting – a great overall experience and ride.
- ND and CC noted 'Rollercoaster', whilst identified as a green trail, is too fast for family riders and provides a negative first Forrest MTB experience; highest accident incident during events, on this track.
- Opportunity to redevelop the start of 'Rollercoaster' to cater for beginners/skill development with more diversity; utilise existing footprint.
- Sealing of the trail makes it faster; not necessarily safer.
- Opportunity to use the topography of BW land for gravity/downhill tracks.

A3.3 Identification of location for Skills Park

- It was agreed that a skills park be developed and located adjacent to the Forrest Trail Head to the southwest of car park amongst established trees, where it can provide maximum benefit and ease of access for MTB riders.
- The skills park is to cater for riders from beginner to intermediate experience levels

- The skills park will provide a controlled environment allowing supervision from the adjacent trail head and offer a range of skill challenges within an easy to follow circuit with optional more difficult lines and skill development features
- The skills park will provide for increased enjoyment, improvement of technical skills and promote the healthy lifestyle of the sport of mountain biking

A3.4 Potential consolidation of existing trails

- It was agreed that trail upgrade, enhancement works to existing trails and identification of opportunities for new trails will redefine and enhance the Forrest MTB offer as follows:
 - Provide diversification of trail types
 - Provide a skills park
 - Provide enhanced MTB opportunities for beginner and intermediate riders close to Forest township and within easy riding from the Forrest Trail Head
 - Realignments and enhancements to existing trails.
 - Identification of new and exciting single-track flow trails in close proximity to town linking into loop trail networks
 - Consolidation of some under utilised trails in Yaugher for use by others e.g. equestrian trails (refer Dwg FMBT- 05)
- Provide better clarity for equestrian use and better user signage.
- Improve the trail (ride experience) and the sustainability of the trail.

Action: *DEPI to discuss with cycle club opportunities for trails as discussed in the meeting with club regarding equestrian use. DEPI to inform of outcomes in local club meeting.*

A3.5 Development costs/funding/grants

- DEPI are committed to working with the community.
- DEPI senior planning and management will support the direction of the strategy.

A4. Interagency Management Group

MS provided a summary of options for the development of the Interagency Management Group (IMG).

A4.1 Discussion of agency members and options for governance, administration structure, steps and timing for progression of IMG

Structure of IMG:

MS discussed the possible admin structure for the IMG and how this group would be formed. It is important to note that Council are key drivers of the IMG to ensure the recommendations and priority actions for the strategy are implemented.

MS clarified for the group the number of options the IMG can be structured as. They are:

1. Section 86 Committee – advice on behalf of Council, can make decisions and spend funds.
2. Advisory Group – to implement a strategy and look for funding options.
3. Reference Group – similar to IMGs.

MS advised that as Council is not a land owner or land manager the structure of the group would be best suited to that of an Advisory Group.

A4.1.1 Governance

- Key agencies – COS, BW, DEPI, PV and potential representatives from local clubs (MTB and equestrian).
- Who are the IMG reporting to?

In discussions with DEPI it is agreed that the initial set up of the IMG would be an Advisory Group structure. The Advisory Group would need to explore a more suitable governance structure once the group achieved an ongoing maintenance funding stream for the sustainability of the trails.

It is suggested that this be a tiered structure with the Advisory Group to continue implementing the recommendations and priority actions and the next tier being a more formal governance structured group.

There is also the possibility that over time the two groups decide to merge and become the overall IMG for both the implementation of the strategy and the handling of the funds for the ongoing maintenance of the trails.

A4.1.2 IMG

- What advice is this group going to be giving?
- Funding holder?
- COS want to continue to progress the opportunities identified in the strategy.
- Group clearly needs to be developed and see the strategy through.
- Develop land use agreement between DEPI and BW.

After discussions with DEPI they are aware of their responsibilities of trail maintenance and would be a key lead agency along with Council, Barwon Water and Parks Victoria. Any recommendations from the Advisory Committee to form a governance structure with DEPI as the lead agent will need to be and should be approved by DEPI management.

A5. Next Steps

- TBLD and agency group visited BW land to assess MTB trail opportunities as discussed in meeting (completed 26/8/14).
- TBLD prepare workshop summary with schematic trails plan and costing (completed 11/9/14).
- MS to set date for meeting with PWG (Oct 2014)

Part B Agency & Technical Site Visit

Tuesday 26 August 2014, 9.00am – 12.00pm, COPACC – Cnr Rae and Gellibrand Streets, Colac

Attendance:

| | |
|-----------------------|---|
| Michael Swanson (MS) | Business Development Officer, Colac Otway Shire |
| Craig Clifford (CC) | DEPI |
| Craig Woobridge (CW) | DEPI |
| Norm Douglas (ND) | Forrest MTB Group |
| Khan Beckett (KB) | Barwon Water |
| Glen Berrill (GB) | Director, Thompson Berrill Landscape Design |
| Andrea Szymanski (AS) | Senior Landscape Architect, Thompson Berrill Landscape Design |

B1. Overview

MTB trails were identified that can achieve the objectives for diversification and improvement of the MTB trail offer in Forrest. Opportunities were then evaluated on site for sustainability. Detailed on site discussion and evaluation was completed and key areas for potential MTB trails identified in the site meetings. Key areas for trails in Forrest and Yaughar are shown, keyed and described on the attached Dwgs FMTB 04 and 05 respectively.

Preliminary estimates for the opportunities identified were discussed and agreed to with the technical working group, for the purpose of forward planning for funding applications and grant applications. Please note the estimates were reflective of the agreed opinion of the technical and experienced experts who attended the meeting, and are as noted as preliminary and not based on actual lengths or detailed costings, which would be subject of detailed investigations. The works identified in the OPC were regarded as high priority works (short term of 1- 2 years) and if implemented, would be of tangible, immediate and significant benefit to the Forrest MTB facility, resulting in a more attractive and well balanced recreational offer to the MTB community, in keeping with the objectives of the Forrest MTB strategy.

B2. Potential trail opportunities evaluated in Forrest

B2.1 Forrest Trail Head

- Improve trail head facilities, signage, interface with the skills park and lead in to the main trail to the MTB trails
- Design and Install a drinking fountain and bottle fill station as a priority, as town water is available on the site
- Design and implement improvements to the main entry point to the MTB trails from the carpark with a highly visible, creative, sculptural and contemporary entry 'gateway' so that visitors are 100% aware of their arrival and immersion the start of the Forrest MTB experience, themed around a 'Forrest MTB identity'
- Design and implement a well designed wash down facility at the Forrest Trail Head site, with careful consideration of design, management of environmental impacts, water use and disposal, water supply, drainage, weed management and maintenance of facility
- Design and implement improved signage thought the Forrest and Yaughar trail network including exciting, colourful and contemporary orientation signage and graphics and 3D maps to excite the visitor to continue to seek out and use the trail signage
- Design and implement improved way finding and reassurance MTB trail signage for better, safer and more enjoyable use of the trails

B2.2 Skills Park

- Design and implementation of a large and exciting skill park to cater to all ages and skills of visitors, to allow riders to hone their skills, test their abilities and learn new skills
- Locate the skills park adjacent to the trail head carpark and shelter so that carers and friends can supervise people on the skills park
- Integrate the skills park sensitively into the established treed area without harm to the trees
- Actively update and promote the provision of a skills park in all literature, new site maps, on line links and Forrest MTB web sites, as this will be a major draw card for attracting MTB visitors to Forrest
- Design and implementation of the new skills park as a priority new Forrest MTB project

B2.3 Rollercoaster

- Review Rollercoaster current design and alignment to decrease speed as it is a fast ride and can cause accidents, and is not conducive to the 'first ride' experience for MTB visitors
- Diversify the alignment to make the ride more interesting and use the terrain of the long valley for more challenge but retaining the green classification
- Design and implementation of the trail upgrade as a priority Forrest MTB project

B2.4 Follow the Dog

- IMBA guidelines identify this trail as a blue run due to its terrain and degree of technical difficulty – but due to its physically demanding qualities (riders out there struggle) it's regarded more of a black run.
- Technical team agreed FTD could therefore be upgraded to a black trail to diversify the MTB offer in Forrest.
- Design and implementation of the trail upgrade as a priority Forrest MTB project

B2.5 'Forrest Flow' (working title)

- An exciting opportunity for a new 2.5 - 3.0 km single-track flow trail was identified for the large east-facing slope of forest, down from the BW water treatment facility. This trail would be blue coded to provide a long flowing single track, with excellent constant gradient to provide a challenging and enjoyable along downhill ride close to town, with loops back to the township and the trailhead.
- This new trail opportunity perfectly meets the major objective of the strategy to enhance the intermediate to advanced trail offer closer to Forrest township, for convenience and diversified MTB product
- The trail could also have many advanced black 'B' lines that could also offer the advanced rider an exciting and challenging downhill ride, perfectly meets the major objective of the strategy to enhance the advanced trail offer closer to Forrest township, for diversification of the higher end of the MTB market
- This trail would commence on the outskirts of town, with the entry conveniently located directly opposite the trail head and carpark for ease of access and entry, and establish a loops
- The new trail would be downstream of the dam wall, so not located in an environmentally sensitive area. BW gave in principal support to the alignment, subject to further discussion of detail
- Design and implementation of the new trail as a priority new Forrest MTB project

B2.6 Red Carpet

- An exciting opportunity to review Red Carpet to diversify the existing alignment to make the ride more interesting and use the steeper side terrain of the long slope to create more challenging black "B" runs for advanced riders
- Design and implementation of the trail upgrade as a priority Forrest MTB project

B3. Potential opportunities evaluated in Yaugher

B3.1 Tunnel Underpass

- The strategy identified a major risk issue being the requirement for a MTB tunnel underpass just before Boundary Road, providing riders the opportunity to safely cross the Colac - Forrest Road from the Forrest - Birregurra Rail Trail to the Yaugher trail system
- The tunnel would be extensively used during the major MTB events to allow the road to remain open and reduce risk of riders and cars
- The tunnel will tie into new trail up the side of Boundary Road
- Design and implementation of the tunnel as a priority Forrest MTB project

B3.2 'Boundary rider' (working title)

- The strategy identified a major risk issue being the requirement for MTB riders to ride up and down Boundary Road, which is very uninteresting and a hard and exhausting ride for all users, which is a major disincentive for riders to use Yaugher from Forrest. It is also dangerous as cars use the road at high speeds which can be very dangerous with bikes weaving across the roadway, especially younger and less experienced riders
- An exciting opportunity exists to create a new climbing and descent trail running beside Boundary Road, on DEPI managed land, creating a safe and enjoyable alternative to Boundary Road
- Design and implementation of the new trail as a priority new Forrest MTB project

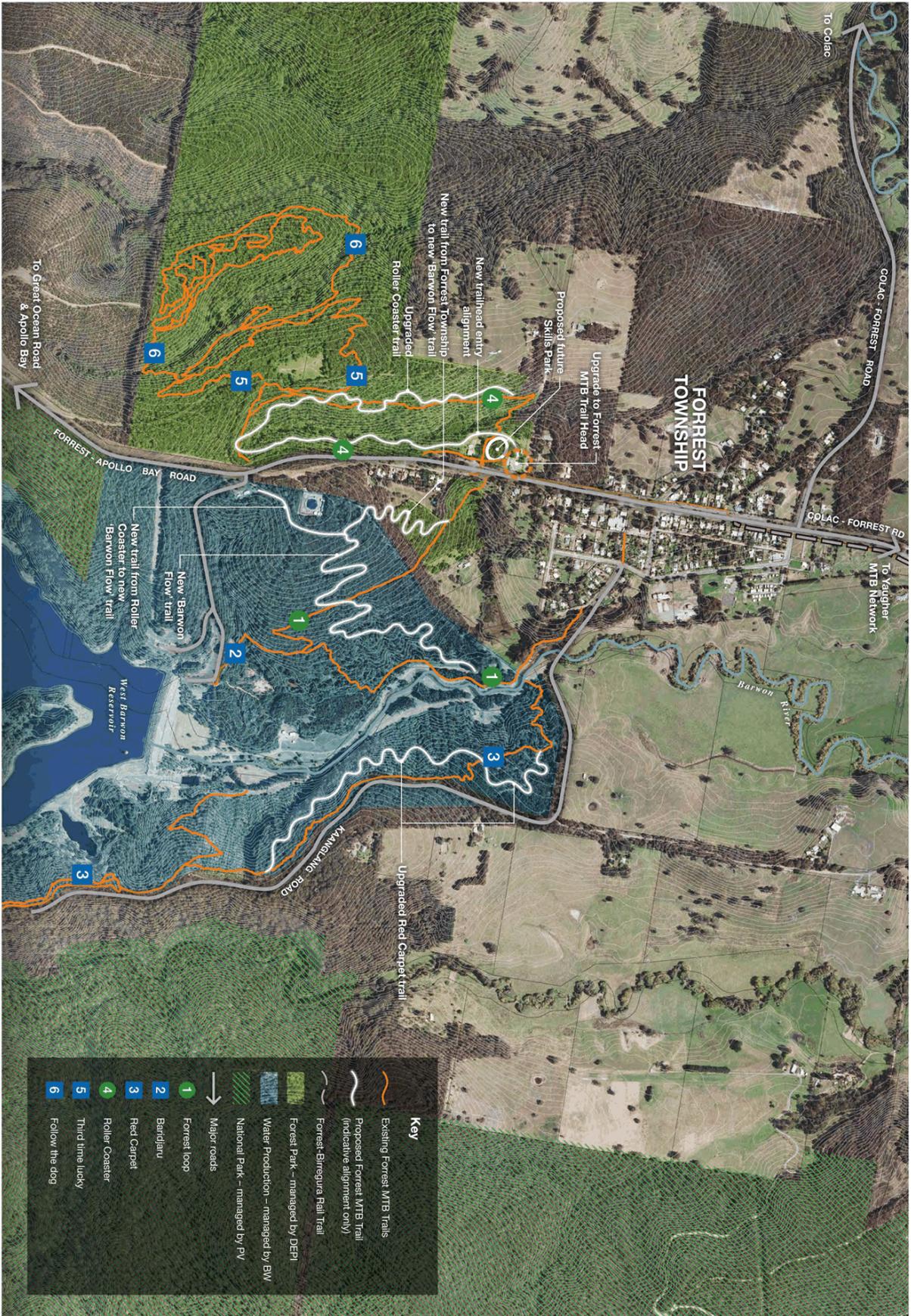
B3.3 Equestrian

- The least used and longest trail at Yaugher is Vista, with estimates of fewer than 15 riders a year. The trail has recently been significantly modified for fire access.
- An synergistic opportunity exists to convert Vista to a dedicated equestrian trail, giving the horse riding community an excellent long ride
- The horse riding community asked for consideration in the consultation phase of rationalisation of the existing horse trail in Yaugher, as they are discontinuous and confusing. This proposed trail will link into the existing equestrian trail network and create a dedicated and exciting ride for the horse riding community
- Undertake site investigation to determine requirements for changed trail use and determine requirements for implementation

B4. Opinion of Probable Cost (OPC)

Refer to attached Opinion of Probable Cost, which outlines the approximate budgets corresponding to the identified opportunities. These costs were discussed and agreed to in principal with the technical working group in the workshop and following the site visit.

THOMPSON BERRILL LANDSCAPE DESIGN P/L



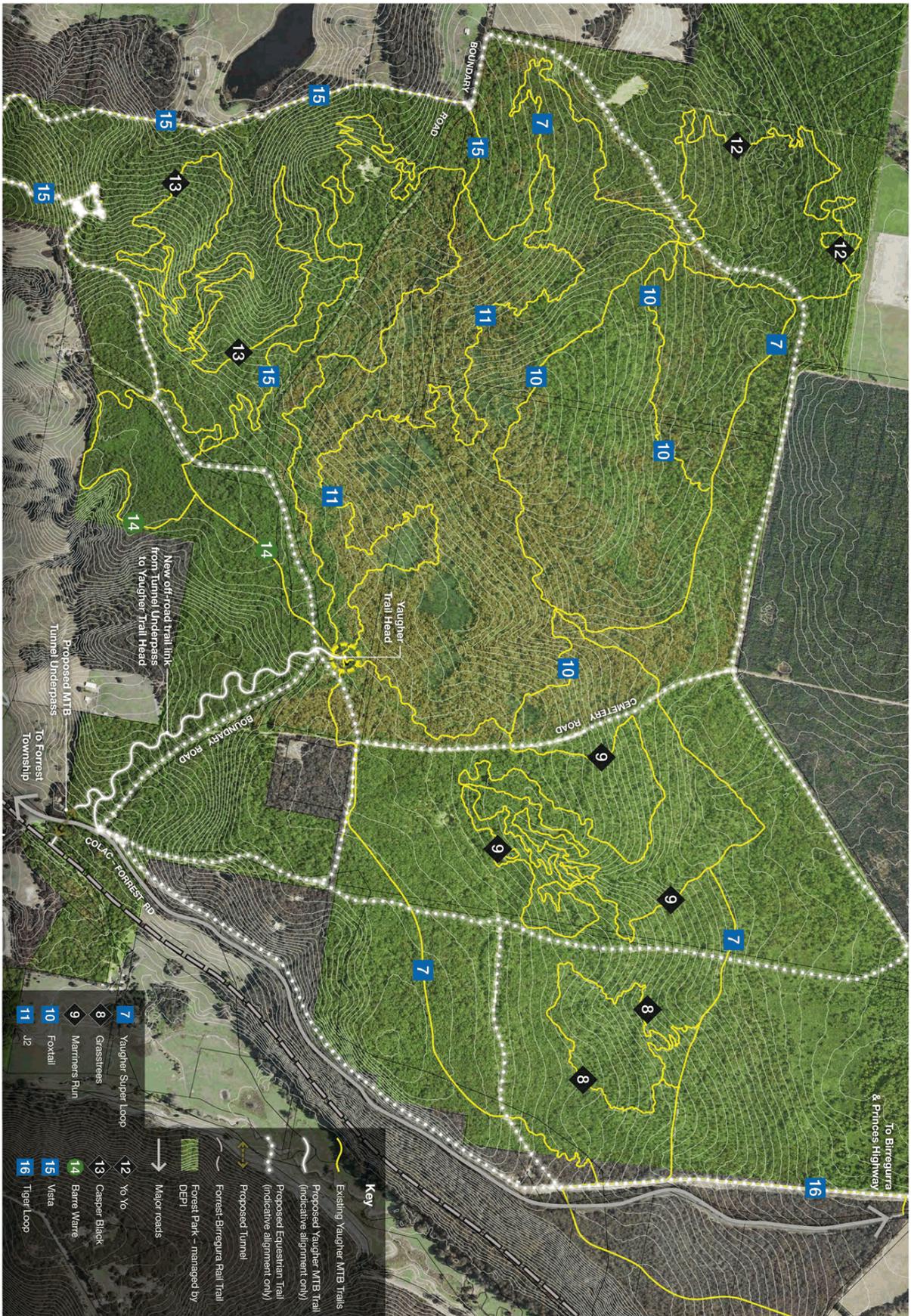
Key

- Existing Forest MTB Trails
- Proposed Forest MTB Trail (indicative alignment only)
- Forest-Barrigulla Rail Trail
- Forest Park - managed by DERL
- Water Production - managed by BW
- National Park - managed by PV
- Major roads
- Forest loop
- 1 Berghuru
- 2 Red Carpet
- 3 Roller Coaster
- 4 Third time lucky
- 5 Follow the dog
- 6

SCHEMATIC PROPOSED FORREST TRAILS PLAN
FORREST MTB DRAFT STRATEGIC TRAILS PLAN

DWG No. FMTB-04
 SEPTEMBER 2014
 1:5000 @ A1

THOMPSON BERRILL LANDSCAPE DESIGN P/L



SCHEMATIC PROPOSED YAUCHER TRAILS PLAN
FORREST MTB DRAFT STRATEGIC TRAILS PLAN

DWG No. FMTB-05
 SEPTEMBER 2014
 1:10000 @ A1

Appendix D.
Preliminary Opinion of Probable Cost

**Forrest MTB Trails Strategy
Preliminary Opinion of Probable Cost (POPC) V2**

Prepared by TBLD P/L for Colac Otway Shire October 2014

Note: This preliminary opinion of probable cost has been prepared for planning purposes only.

| No. | Description | No. | Item | TOTAL |
|--|---|-----|------|------------------|
| TRAIL WORKS | | | | |
| 1.0 TRAIL ENHANCEMENT WORKS | | | | |
| 1.1 | Rollercoaster track upgrade and enhancement works, including development of A & B lines, improved drainage, improved track surface, etc. <i>Subject to trail detailed design</i> | 1 | Item | \$40,000 |
| 1.2 | Red Carpet track upgrade and enhancement works, including development of A & B lines, improved drainage, improved track surface, etc. <i>Subject to trail detailed design</i> . | 1 | Item | \$40,000 |
| 1.3 | Follow the Dog track upgrade and enhancement works, including development of A & B lines, improved drainage, improved track surface, etc. <i>Subject to trail detailed design</i> . | 1 | Item | \$40,000 |
| 1.4 | Yaugher trail enhancements and upgrade works. <i>Subject to trail detailed design</i> . | 1 | Item | \$40,000 |
| Subtotal to Trail Enhancement Works | | | | \$160,000 |
| 2.0 NEW TRAIL WORKS | | | | |
| 2.1 | Barwon Flow' – establish new blue trail within the Forrest MTB Trail Network, providing new trails suitable for families to intermediate rider ability, in close proximity to the Forrest Township. New trails to be developed are located on Barwon Water owned land and managed by DEPI, as per the current MTB trail arrangement. <i>Subject to future trail detailed design</i> . | 1 | Item | \$120,000 |
| 2.2 | Boundary Rider' (adjacent to Boundary Road) – establish a new off road trail link from the proposed Tunnel Underpass to the existing Yaugher Trail Head. <i>Subject to future detailed design</i> . | 1 | Item | \$120,000 |
| Subtotal to New Trail Works | | | | \$240,000 |
| 3.0 NEW SKILLS PARK | | | | |
| 3.1 | Establish a MTB skills development park adjacent to the Forrest Trail Head site. Skills park to provide diverse skill development for all levels of riders, located adjacent to the carpark, creatively signed and located to provide a welcoming character for all trail users, with a functional layout and provide sufficient information and direction for visitors at the site. <i>Subject to future detailed design</i> . | 1 | Item | \$50,000 |
| Subtotal to Skills Park | | | | \$50,000 |
| 4.0 NEW TRAIL SIGNAGE | | | | |
| 4.1 | Upgrade of trail signage, including directional, wayfinding and interps, across the Forrest and Yaugher MTB trail network, e.g. iconic welcoming signage, trail entry gateway, key orientation and directional signage, IMBA rider standards and guidelines, types of skills to be explored within the park, etc. <i>Subject to future detailed design</i> . | 1 | Item | \$100,000 |
| Subtotal to New Trail Signage | | | | \$100,000 |
| SUBTOTAL TRAIL WORKS | | | | \$550,000 |
| CONTINGENCIES 10% | | | | \$55,000 |
| TOTAL TRAIL WORKS | | | | \$605,000 |
| 5.0 TUNNEL UNDERPASS | | | | |
| 5.1 | As discussed and identified with Colac Otway Shire, undertake a feasibility study to identify potential options and alignment for On site discussions identified the crossing in the area just north of Yaugher Road. <i>Subject to future feasibility study, engineering and detailed design</i> . | 1 | Item | \$150,000 |
| Subtotal to Tunnel Underpass | | | | \$150,000 |



Assembly of Councillors Record

This Form MUST be completed by the attending Council Officer and returned IMMEDIATELY to Document Management Co-ordinator for filing. A copy of the completed form must be provided to the Executive Officer to the CEO, Mayor & Councillors for reporting at the next Ordinary Council Meeting.

Assembly Details:

Date: 4 February 2015

Time: 5:15pm – 6:00pm

Assembly Location: Central Reserve – Hockey Rooms

In Attendance:

Councillors: Cr Brian Crook

...../...../...../.....

Officer/s: Ian Seuren – Manager Recreation, Arts and Culture

...../...../...../.....

Matter/s Discussed: See attached meeting agenda.

.....
(some e.g's. Discussion s with property owners and/or residents, Planning Permit Application No. xxxx re proposed development at No. xx Pascoe Street, Apollo Bay, Council Plan steering committee with Councillors and officers.)

Conflict of Interest Disclosures: (refer page 5)

Councillors: Not applicable

...../...../...../.....

Officer/s: Not applicable

...../...../...../.....

Left meeting at: At conclusion of the meeting at 6:00pm

Completed by: Ian Seuren – Manager, Recreation, Arts and Culture



Central Reserve Advisory Committee

Colac Hockey Rooms

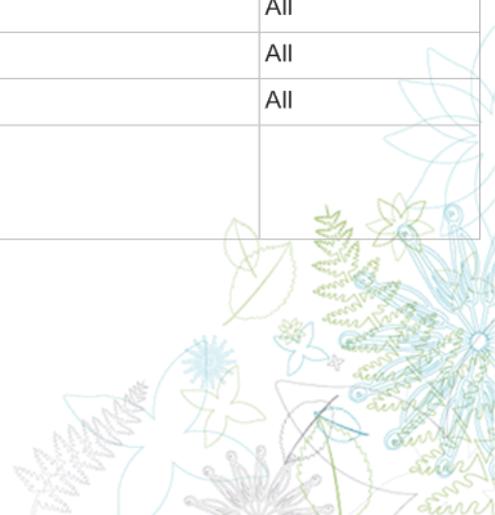
Date: 4 February 2015

Time: 5:15pm

AGENDA

| | | |
|-----|---|------------|
| 1. | ATTENDEES
Ian Seuren (Colac Otway Shire)
Cr Brian Crook (Colac Otway Shire)
Matthew Gibson (Colac Football & Netball Club Inc.)
Andrea Graham (Colac Netball Club Inc.)
Graham Tevelein (Colac Youth & Recreation Club)
Rob Osborne (Colac & District Cricket Association Inc.)
Peter Lucas (Colac & District Football Netball League)
Val Russell (Colac & District F/L Netball Association Inc.)
Sam McDonald (Colac Auskick)
Matthew Walsh (Little Athletics Colac)
Terry Stephens (Colac & District Dog Obedience Club)
Simon Dewar (Colac Secondary College)
John Clarke (Colac Otway Rovers)
Steve Cheater (City Bowls Club Colac) | |
| 2. | APOLOGIES | |
| 3. | CONFIRMATION OF MINUTES FROM PREVIOUS MEETING | Cr Crook |
| 4. | FACILITIES AND GROUNDS MAINTENANCE REPORTS | Ian Seuren |
| 5. | MASTER PLAN IMPLEMENTATION UPDATE <ul style="list-style-type: none"> • Design services • Community engagement for design • Community contribution | Cr Crook |
| 6. | BLUEWATER FITNESS CENTRE REDEVELOPMENT | Ian Seuren |
| 7. | COLAC SECONDARY COLLEGE OVAL UPDATE | Ian Seuren |
| 8. | UPDATE FROM USER GROUPS | All |
| 9. | GENERAL BUSINESS | All |
| 10. | ITEMS FOR FUTURE MEETINGS | All |
| 11. | NEXT MEETING <ul style="list-style-type: none"> • Wednesday 1 April 2015 | |

XYZ Meeting – 00/00/2013





Assembly of Councillors Record

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Assembly Details:

Date: 12 / 02 / 2015

Time: 7.30 pm

Assembly Location: Colac Botanic Cafe

(some e.g's. COPACC, Colac Otway Shire Offices, 2 - 6 Rae Street, Colac, Shire Offices – Nelson Street, Apollo Bay

In Attendance:

Councillors: Cr Woodcroft /...../...../.....

Officer/s: Greg Anderton / Laurence Towers /...../.....

Matter/s Discussed: Correspondence In and Out, Treasurer's Report, Gardener's Report, Potting Report, General Business (Rose Arbour Event, Plant sale and Picnic Day, Book launch)

(some e.g's. Discussion s with property owners and/or residents, Planning Permit Application No. xxxx re proposed development at No. xx Pascoe Street, Apollo Bay, Council Plan steering committee with Councillors and officers.)

Conflict of Interest Disclosures: (refer page 5)

Councillors:/...../.....

...../...../.....

Officer/s:/...../.....

...../...../.....

Left meeting at:

Completed by: Kristy Cochrane

\\colacdocs\user documents\kcochrane\Offline Records (CL)\Assembly of Councillors - Friends of the Colac Botanic Gardens - 20150212.docx



Assembly of Councillors Record

This Form MUST be completed by the attending Council Officer and returned IMMEDIATELY to Document Management Co-ordinator for filing. A copy of the completed form must be provided to the Executive Officer to the CEO, Mayor & Councillors for reporting at the next Ordinary Council Meeting.

Assembly Details:

Date: 16 / 02 / 2015
Time: 9.00 am

Assembly Location: Colac Regional Selling Centre

(some e.g's. COPACC, Colac Otway Shire Offices, 2 - 6 Rae Street, Colac, Shire Offices – Nelson Street, Apollo Bay

In Attendance:

Councillors: Cr Buchanan / / /
..... / /

Officer/s: Phil Corluka / Greg Anderton / Graeme Riches / Kristy Cochrane
/ /

Matter/s Discussed: Safety at the Saleyards, Development of the Business Plan for the Colac Regional Saleyards, Position with Directions Paper, Flooring options for the yards, Priority List for Works/Improvements, External Promotion of the Colac Regional Saleyards, Update – Regional Livestock Selling Centres Discussions, Operating Report – Saleyards Throughput Figures, Other Business, General Business

(some e.g's. Discussion s with property owners and/or residents, Planning Permit Application No. xxxx re proposed development at No. xx Pascoe Street, Apollo Bay, Council Plan steering committee with Councillors and officers.)

Conflict of Interest Disclosures: (refer page 5)

Councillors: / / /
..... / /

Officer/s: / / /
..... / /

Left meeting at:

Completed by: Kristy Cochrane

| | | |
|--|---|---|
|  <p>Agenda</p> | <h1>Councillor Workshop</h1> <p>Wednesday, 18 February 2015
COPACC Rehearsal Room
1.00pm to 5.30 pm</p> | |
| | <p>ATTENDEES:
Cr Lyn Russell, Cr Stephen Hart (arrived 1:52pm, absent 4:12 -4:15pm), Cr Frank Buchanan, Cr Terry Woodcroft(absent 2:47pm – 2:51pm and 4:58pm – 5:10pm), Cr Brian Crook.</p> <p>Sue Wilkinson (CEO)
Carmen Lawrence (A/GM, Corporate & Community Services)
Doug McNeill (A/GM, Sustainable Planning & Development)
Phil Corluka (GM, Infrastructure & Services)</p> <p>Apology: Colin Hayman</p> | |
| | <h2>Agenda Topics</h2> | |
| 1.03 pm | Declaration of Interest | |
| 1.05pm – 1.30pm | Website Redevelopment Project | T McLennan, Margaret Giudice, Stephen Northey |
| 1.30pm – 2.05 pm | Mid Year Review (no paper) | A Roberts |
| 2.05pm – 2.15pm | Council Plan Review | M Giudice |
| 2.17pm – 2.44 pm | Responding to Community Enquiries | S Wilkinson |
| 2.45pm – 2.50pm | Request Cards (no paper) | R Munro |
| 2.53pm – 3.04pm | Meeting Procedure Review (no paper) | S Wilkinson |
| 3.04pm – 3.16pm | Break | |
| 3.16pm – 4.00pm | Post Election Policy Review (no paper) | T White |
| 4.00pm – 4.48pm | Economic Development Strategy | T White |
| 4.50pm – 5.28pm | General Business | |

COUNCILLOR BRIEFING SESSION



Colac Otway
SHIRE

Councillor Briefing Session

COUNCILLOR BRIEFING SESSION OF THE COLAC-OTWAY SHIRE COUNCIL held at the Senior citizens Centre, Apollo Bay on 25 February 2015 at 10.00am

| ITEM | RESPONSIBLE OFFICER | ACTION DUE DATE | | | | | | | | |
|--|---|--|-----------------------|-------------------|----------------------------|-----|---------------------|---|--|--|
| <p>1. ATTENDEES:
 Cr. Frank Buchanan (Mayor)
 Cr. Brian Crook
 Cr. Stephen Hart
 Cr. Michael Delahunty
 Cr Lyn Russell
 Cr Terry Woodcroft</p> <p>Sue Wilkinson, CEO</p> <p>Part: Carmen Lawrence, A/General Manager, Corporate & Community Services
 Phil Corluka, General Manager, Infrastructure & Services
 Doug McNeill, A/General Manager, Sustainable Planning & Development
 Rhonda Deigan, Executive Officer</p> <p>2. APOLOGIES:
 Colin Hayman</p> <p>Briefings:</p> <ul style="list-style-type: none"> CEO's Performance Review – 10.00am Budget - Outline Business Cases – 11.30am <p>3. DISCLOSURE OF CONFLICT OF INTEREST</p> <table border="1" data-bbox="274 1249 943 1512"> <tr> <td>Cr Woodcroft:</td> <td>OM152502-8 & OM152502-18 Contract 1510 Supply & fitting of tyres & batteries (MAV)</td> </tr> <tr> <td>Nature of Disclosure:</td> <td>Indirect Interest</td> </tr> <tr> <td>Type of Indirect Interest:</td> <td>78B</td> </tr> <tr> <td>Nature of Interest:</td> <td>One of the local suppliers involved in contract 1510 is a sponsor for a community group that Cr Woodcroft manages</td> </tr> </table> <p><i>Crs Russell, Delahunty and Hart flagged a potential conflict of interest in item OM152505-10 Planning Scheme Review.</i></p> <p>5. Agenda</p> <p>Discussed.</p> | Cr Woodcroft: | OM152502-8 & OM152502-18 Contract 1510 Supply & fitting of tyres & batteries (MAV) | Nature of Disclosure: | Indirect Interest | Type of Indirect Interest: | 78B | Nature of Interest: | One of the local suppliers involved in contract 1510 is a sponsor for a community group that Cr Woodcroft manages | | |
| Cr Woodcroft: | OM152502-8 & OM152502-18 Contract 1510 Supply & fitting of tyres & batteries (MAV) | | | | | | | | | |
| Nature of Disclosure: | Indirect Interest | | | | | | | | | |
| Type of Indirect Interest: | 78B | | | | | | | | | |
| Nature of Interest: | One of the local suppliers involved in contract 1510 is a sponsor for a community group that Cr Woodcroft manages | | | | | | | | | |

COUNCILLOR BRIEFING SESSION

| ITEM | RESPONSIBLE OFFICER | ACTION DUE DATE |
|--|---------------------|-----------------|
| <p>6. GENERAL BUSINESS</p> <ul style="list-style-type: none"> • Great Ocean Road Closures • School-based apprenticeships • Increased rail services • Forrest Men's Shed • Mooleric Road Quarry <p>Crs Delahunty and Russell declared a conflict of interest in this item and left the meeting from 2.57pm to 3.04pm.</p> <ul style="list-style-type: none"> • Rezoning of land in Connor Street (Bulla Dairy Foods) • Apollo Bay Harbour Report • Western Region Coastal Action Plan • 2015/2016 Budget • COPACC operations • Family Day Care Services. <p>Meeting Closed at 3.28pm</p> | | |

| | |
|--|---|
|  <p>Agenda</p> | <h1 style="text-align: right;">Strategic Planning Workshop</h1> <p style="text-align: right;">Wednesday, 4 March 2015
Colac Community Library & Learning Centre
10.00am – 4.00pm</p> |
| | <p>ATTENDEES:
Cr Frank Buchanan (Mayor), Cr Lyn Russell, Cr Stephen Hart, Cr Terry Woodcroft, Cr Brian Crook</p> <p>Bruce Turner (Facilitator)
Sue Wilkinson (CEO)
Doug McNeill (A/GM, Sustainable Planning & Development)
Phil Corluka (GM, Infrastructure & Services)</p> <p>Apology:
Colin Hayman (GM, Corporate & Community Services)</p> |
| | <h2>Agenda Topics</h2> |
| 10.00 am | Welcome |
| 10.30 am | Future Issues
Priority Setting |
| 1.00 pm | Lunch |
| 1.30 am | Working together: how to strengthen relationships |
| | Conclusion |
| 4.00 pm | Close |

| Councillor Workshop | |
|--|---|
| 
Colac Otway
SHIRE
Agenda | Wednesday, 11 March 2015
COPACC Meeting Room
9.00 am to 5.30 pm |
| | ATTENDEES:
Cr Frank Buchanan (Mayor), Cr Lyn Russell (absent from 12.25pm to 3.00pm),
Cr Stephen Hart, Cr Chris Smith (from 10.30 to 12.25pm), Cr Terry Woodcroft
(absent 12.45pm to 3.00pm), Cr Michael Delahunty (from 10.30am –
10.35am), Cr Brian Crook

Sue Wilkinson (CEO)
Colin Hayman (GM, Corporate & Community Services) – until 3.30pm
Doug McNeill (A/GM, Sustainable Planning & Development)
Phil Corluka (GM, Infrastructure & Services)

Apology: Nil |
| Agenda Topics | |
| 9.00 am | Declaration of Interest
Dr Delahunty – Item 2 - Planning Committee
Meeting Agenda |
| 9.00am – 9.50am | Planning Committee Briefing Session |
| 9.50am – 10.00am | Harrington Park Management Options Ian Seuren |
| 10.00am – 10.15am | Customer Service Targets Nikki Karpeles/Tamzin McLennan |
| 10.15am – 10.30am | Break |
| 10.30am – 12.25pm | Planning Committee Meeting |
| 12.30pm – 1.00pm | Lunch |
| 1.00pm – 1.40pm | STIP – Alternative Project Tony White / Katrina Kehoe |
| 1.40pm – 1.50pm | Old Beechy Rail Trail Licence Agreements Ian Seuren |
| 1.50pm – 2.45pm | Apollo Bay C73 – Panel Report Blraithin Butler / Isis Planning – dave Merrett |
| 2.45pm – 3.00pm | Break |
| 3.00pm – 3.30pm | COPACC - confidential Ian Seuren/Ashley Roberts/Carmen Lawrence/Adam Lehmann/Paula Gardiner |
| 3.30pm – 5.07pm | Outline Business Cases – confidential
Cr Woodcroft delared a conflit of interest re line
item 125 and left the meeting between 3.50pm to
4.04pm EMT/Adam Lehmann/Ashley Roberts/Paula Gardiner/Ian Seuren |
| 5.07pm – 5.30 pm | General Business |

**173 AGREEMENT
Underpass Agreement
(Section 173 of the Planning and Environment Act ("the Act"))**

THIS DEED OF AGREEMENT is made the 6th day of March 2015 pursuant to Section 173 of the Planning and Environment Act 1987 ("the Act").

BETWEEN: COLAC-OTWAY SHIRE COUNCIL of 2-6 Rae Street, Colac in the State of Victoria (hereinafter called "Council")

AND Simon David Scott of 960 Irrewillipe Road, Barongarook West, (hereinafter called "Simon")

AND Norman Harold Lowe of 415 Barrys Road, Barongarook West, (hereinafter called "Norman")

RECITALS:

- A Simon is the owner of the land more particularly described as Lot 1, TP 219407L (960 Irrewillipe Road, Barongarook West), (hereinafter called "Simon's Land") and desires to construct a underpass under Irrewillipe Road, Barongarook West to enable stock to cross under that road.
- B Norman is the owner of the land more particularly described as Lot 3, TP 643654A (415 Barrys Road, Barongarook West), (hereinafter called "Norman's Land") and gives consent to Simon to construct a underpass under Irrewillipe Road, Barongarook West and encroaching within Norman's Land to enable stock to cross under that road.
- C Council, will consent to the construction of the underpass subject to the following conditions:

DEFINITIONS

"Simon" shall be deemed to include the Simon's successors, assignees and transferees and the obligations imposed upon and assumed by Simon in respect to the Simon's Land shall also be binding on its successors, transferees, purchasers, mortgagees, assigns and any person obtaining possession of whole or part of the Simon's Land ("Simon's Successors") as if each of Simon's Successors had separately executed this Agreement

"Norman" shall be deemed to include the Norman's successors, assignees and transferees and the obligations imposed upon and assumed by Norman in respect to the Norman's Land shall also be binding on its successors, transferees, purchasers, mortgagees, assigns and any person obtaining possession of whole or part of the Norman's Land ("Norman's Successors") as if each of Norman's Successors had separately executed this Agreement.

AGREEMENT

In consideration of Councils' approval and Simon causing an underpass to be built under Irrewillipe Road, Barongarook West and permitting Simon to use the underpass for the passage of livestock the Council, Simon and Norman agree that:

- I. The right to use of the underpass is limited to Simon and persons authorized by Simon.
- II. Norman does not have right of use of the underpass or the right to authorize other persons to use it.
- III. The underpass shall be constructed in accordance with plans submitted by AGB Engineering Pty Ltd being Drawing Reference sheet M1081-DR-C01, M1081-DR-C02 and M1081-DR-C03 dated 19 February 2015 ("the works") as amended to accord with Councils' requirements and which, as amended shall have met the approval of Council.
- IV. The materials and works required to fully construct the underpass shall be supplied and carried out by a contractor approved by Councils' General Manager - Infrastructure at Simon's expense, subject to any agreed contribution by Council, and shall be maintained by the contractor at Simon's expense for a period of Four (4) months after completion.

DAC-125929-90-1-V1

- V. Simon shall obtain all relevant approvals and/or permits and ensure the works, including the fenced approaches and walkways, are constructed in accordance with the plans and to the satisfaction of Council.
- VI. Simon shall be responsible for the maintenance, repair and drainage of the underpass and associated guard rail.
Simon shall reimburse Council within one month should it incur any costs or expenses in repairing, maintaining or draining the underpass or associated infrastructure, should Simon fail to do so.
Council will not be required to undertake inspections of the underpass or associated infrastructure.
Council will be responsible for inspecting, maintaining and repairing the road and associated road infrastructure under which the underpass crosses, in accordance with Council's Road Management Plan.
Council will accept liability for the cost of repairs attributable to a specially authorized overloaded vehicle travelling over the underpass, prior notification which will be given by Council to Simon.
Simon shall notify Council of any such known damage within 28 days.
- VII. Simon and Norman shall advise any future purchaser of the their land or of any part thereof in writing of the existence and contents of this agreement in the event of any sale of any part of their respective landholdings prior to registration of this Agreement on the titles to Simon's Land and to Norman's Land.

Simon and Norman shall advise Council in writing of any such change in ownership of their respective Lands or any part thereof within 28 days of such change.
- VIII. Simon shall;
A. wholly indemnify and hold harmless Council and Norman against any and all demands actions and claims or suits for damages brought against Council or Norman by any entity in respect of the construction, existence and/or use of the underpass; and
B. protect himself, Council and Norman against any legal action which may result from the design construction and/or usage of the underpass or its approaches by obtaining an adequate insurance cover by a reputable insurer for an amount agreed to by Council and in default of such an agreement for an insured amount nominated by an independent broker agreed to by the parties, or in default of such agreement an amount nominated by the President for the time being of the Law/Institute of Victoria.
- IX. If Council is of the opinion that the underpass is in need of cleaning or repair (for which matters Simon is responsible) a notice may be served on Simon directing that such cleaning and/or repairs as specified in the notice shall be carried out by Simon within a period of 28 days after service of the notice.
- X. In default of compliance by Simon with such notice Council may carry out the cleaning and/or repairs and charge the cost thereof to Simon. Such cost may be recoverable in a court of competent jurisdiction.
- XI. The underpass shall:
A. only be used for the passage of livestock and , ancillary thereto, dogs and persons involved in the passage of the livestock with or without appropriate farm machinery such as motor bikes or quad bikes;
B. not be used for the housing of animals, vehicles or farm machinery, or for the storage of produce or material of any type for any period of time.
- XII. No attachments shall be made to any part of the underpass without prior approval in writing from Council.
- XIII. Simon acknowledges that the continued use of the underpass is subject at all times to compliance with the terms of this Agreement. In the event of structural failure or evidence of structural weakness of the underpass; Simon shall be responsible for rectifying the faults or removing the underpass and reinstating the road to proper condition. In the event of the road being widened and/or realigned in the future, Council would bear the cost of extending or relocating the underpass.

DAC-125929-90-1-V1

- XIV. No compensation shall be claimed from or paid by Council upon termination of the usage of the underpass in accordance with the terms of this agreement.
- XV. This agreement shall be for a period of twenty years commencing on 6 March 2015 and terminating on 28 February 2035 unless terminated pursuant to any provision hereof or extended by agreement between all parties hereto.
- XVI. A notice (including, without limitation a document) shall be deemed to have been given when it is received by the person to whom it is addressed or is delivered to the address of that person stated in the Agreement or last communicated in writing by that person to the person giving the notice, whichever is the earlier.
- XVII. This Agreement supersedes any and all prior arrangements, agreements or undertakings and shall be binding notwithstanding any prior arrangements, agreements or undertakings made which may conflict with or be at variance with this agreement or any correspondence or document relating to the subject matter of this agreement which may have passed between parties hereto.
- XVIII. Upon termination of the agreement, Simon will be responsible for removing the underpass and reinstating the road to proper condition.
- XIX. Norman will not prevent the use by Simon and by persons authorized by Simon of that part of Norman's Land reasonably required for access in accordance with this Agreement across that part of Norman's Land to and from the underpass and for the maintenance of the underpass for so long as this Agreement is registered upon the title to Norman's Land.
- XX. The Council, Simon and Norman shall do all things necessary (including signing any further agreement, acknowledgment or document) to enable the Council to enter a memorandum of this Agreement on the Certificates of Title to Simon's Land and to Norman's Land in accordance with Section 181 of the Act.
- XXI. The Agreement shall commence on the date that it bears and shall end:
 - A. with the written agreement of the parties; or
 - B. otherwise by operation of the Act.

IN WITNESS whereof the parties hereto have hereunto set their hands and seals the day and year first set herein before written.

COLAC-OTWAY SHIRE COUNCIL

**THE COMMON SEAL of COLAC OTWAY
SHIRE** was hereto affixed in accordance
with its Local Law No. 4

.....
Chief Executive Officer

SIGNED SEALED AND DELIVERED)
by Simon David Scott in the presence of)

Witness

SIGNED SEALED AND DELIVERED)
by Norman Harold Lowe in the presence of)

Witness

DAC-125929-90-1-V1





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**REGISTER SEARCH STATEMENT (Title Search) Transfer of
Land Act 1958**

Page 1 of 1

VOLUME 08034 FOLIO 495

Security no : 124054068594S
Produced 13/02/2015 03:52 pm

LAND DESCRIPTION

Lot 1 on Title Plan 219407L (formerly known as part of Crown Allotment 24A
Parish of Barongarook).
PARENT TITLE Volume 06793 Folio 535
Created by instrument 2607000 24/11/1953

REGISTERED PROPRIETOR

Estate Fee Simple
Sole Proprietor
SIMON DAVID SCOTT of RMB 9580 IRREWILLIPE RD BARONGAROOK 3249
X233329A 28/12/2000

ENCUMBRANCES, CAVEATS AND NOTICES

MORTGAGE AF196123V 10/07/2007
AUSTRALIA AND NEW ZEALAND BANKING GROUP LTD

Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section
24 Subdivision Act 1988 and any other encumbrances shown or entered on the
plan set out under DIAGRAM LOCATION below.

DIAGRAM LOCATION

SEE TP219407L FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

NIL

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: 960 IRREWILLIPE ROAD BARONGAROOK WEST VIC 3249

DOCUMENT END



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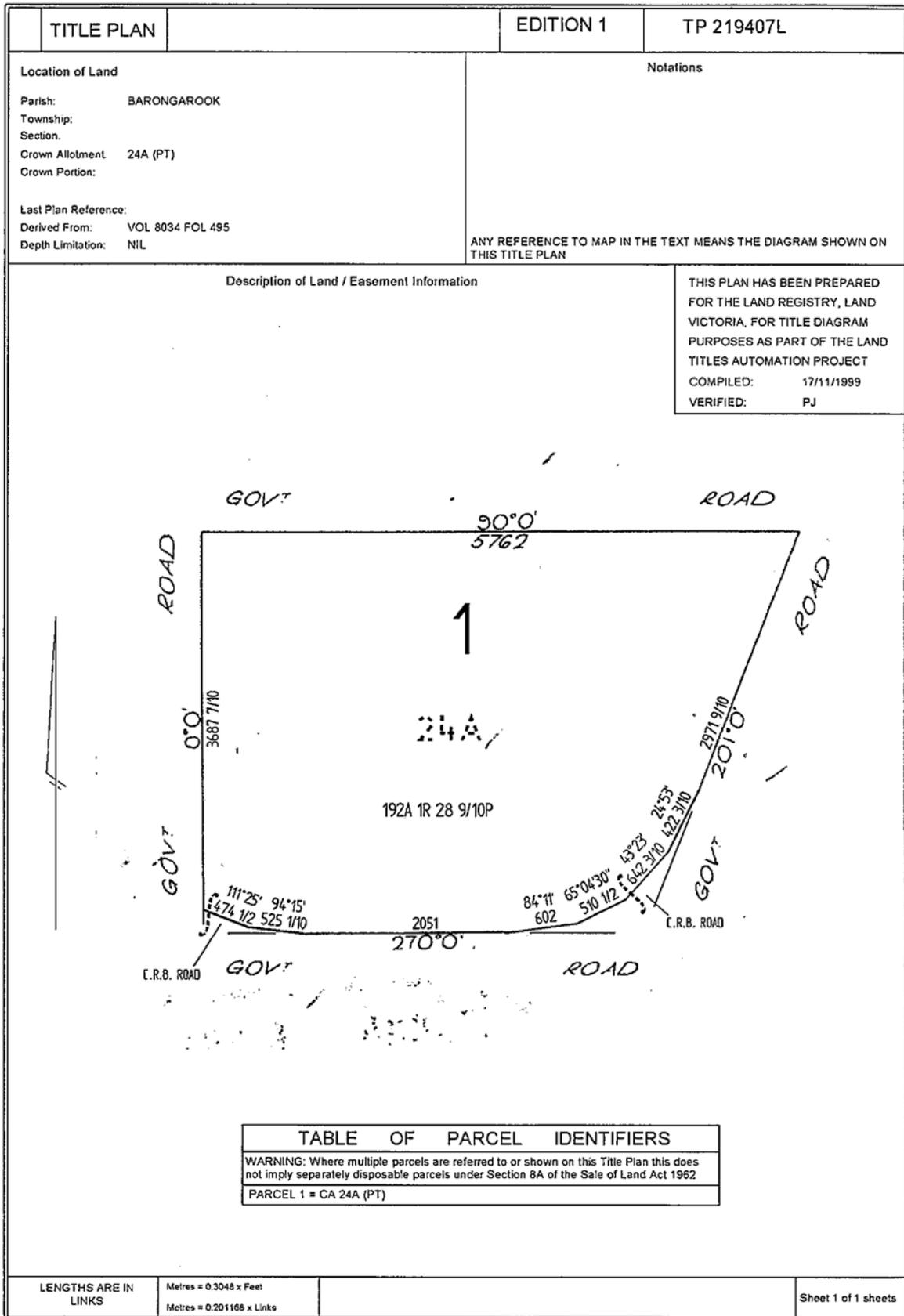
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| Document Type | plan |
| Document Identification | TP219407L |
| Number of Pages
(excluding this cover sheet) | 1 |
| Document Assembled | 13/02/2015 15:54 |

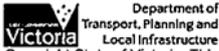
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REGISTER SEARCH STATEMENT (Title Search) Transfer of
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Page 1 of 1

VOLUME 11546 FOLIO 207

Security no : 124054068639T
Produced 13/02/2015 03:54 pm

LAND DESCRIPTION

Lots 2,3 and 4 on Title Plan 643654A.
PARENT TITLE Volume 09689 Folio 692
Created by instrument AL618954A 12/01/2015

REGISTERED PROPRIETOR

Estate Fee Simple
Sole Proprietor
NORMAN HAROLD LOWE of 415 BARRY'S ROAD BARONGAROOK WEST VIC 3249
AC478798R 17/11/2003

ENCUMBRANCES, CAVEATS AND NOTICES

CHARGE AC478799P 17/11/2003
MARY GRACE LOWE

Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section 24 Subdivision Act 1988 and any other encumbrances shown or entered on the plan set out under DIAGRAM LOCATION below.

DIAGRAM LOCATION

SEE TP643654A FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

| NUMBER | | STATUS | DATE |
|-----------|----------------------|------------|------------|
| AL618954A | DISCHARGE OF CHARGE | Registered | 15/01/2015 |
| AL639486P | WITHDRAWAL OF CAVEAT | Registered | 22/01/2015 |

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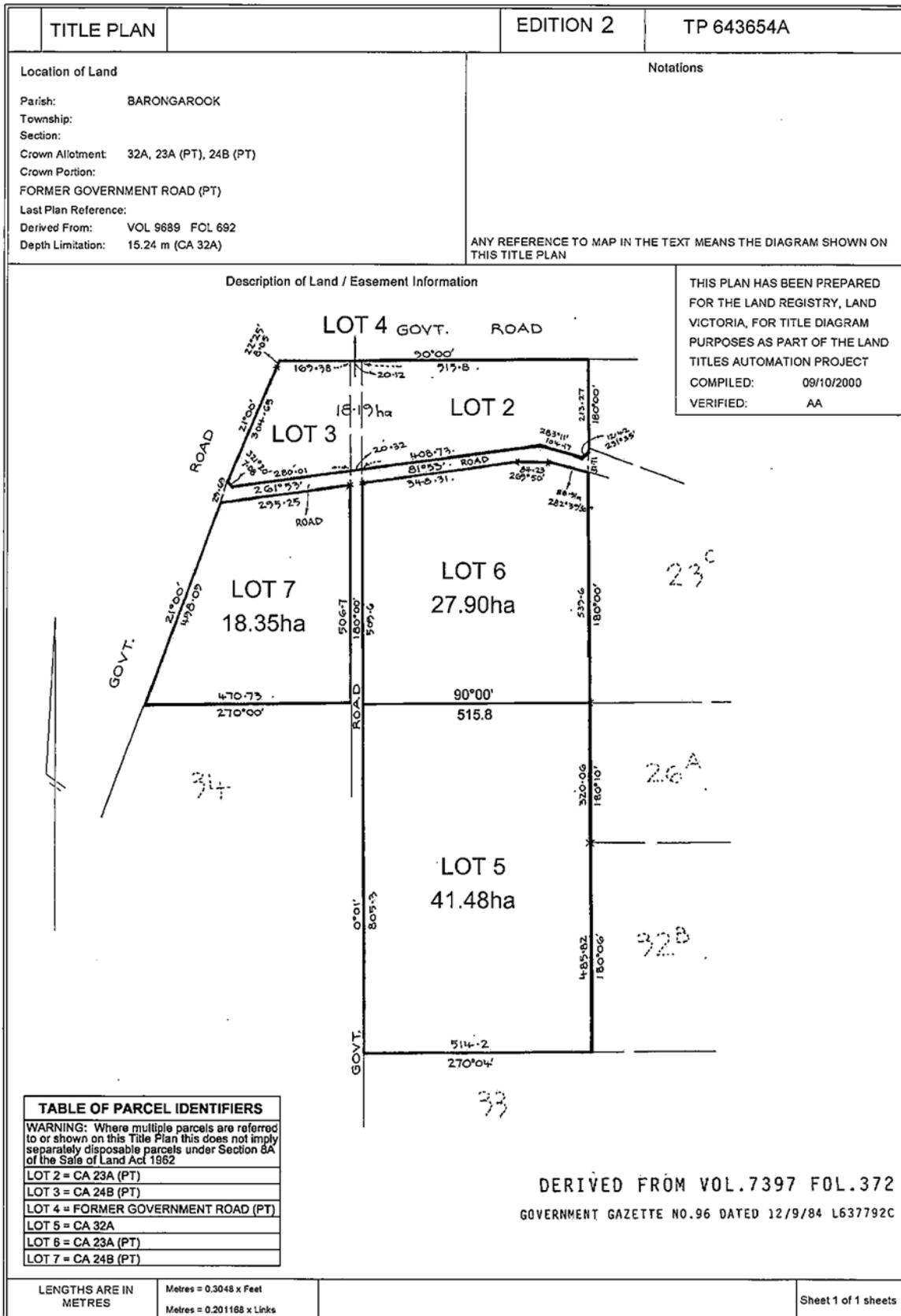
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| Document Identification | TP643654A |
| Number of Pages
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D15/17192

FORMAL STANDARD AGREEMENT
(Section 173 of the Planning and Environment Act)



THIS DEED OF AGREEMENT is made the 1st day of February 2015

BETWEEN

COLAC-OTWAY SHIRE COUNCIL of 2-6 Rae Street, Colac in the State of Victoria (hereinafter called "Council")

AND

J J & F E Parsons of 1780 Irrewillipe Road, Irrewillipe East, Dairy Farmers (herein after called "the User")

RECITALS:

A J J & F E Parsons are the owners of certain land abutting both sides of the Road being the land more particularly described as Lot 1, TP 249498 345 Carlisle Road, Irrewillipe East) and Lot 1, TP 131270 (1425 Irrewillipe Road, Irrewillipe East), (hereinafter called "the Land") and desires to construct a stock underpass under Irrewillipe Road, Irrewillipe East to enable stock to cross under that road.

B Council, will consent to the construction of the stock underpass subject to the following conditions:

In consideration of Councils' approval to the User causing a stock underpass to be built under Irrewillipe Road, Irrewillipe East and permitting the User to use the stock underpass the User agrees that:

- I. The stock underpass ("the works") shall be constructed in accordance with plans submitted by AGB Engineering Pty Ltd being Drawing Reference sheet E1986-DR-C01, E1986-DR-C02 and E1986-DR-C03 dated 21 October 2014 as amended to accord with Councils' requirements and which, as amended shall have met the approval of Council.
- II. The materials and works required to fully construct the stock underpass shall be supplied and carried out by a contractor approved by Councils' General Manager - Infrastructure at the User's expense, subject to any agreed contribution by Council, and shall be maintained by the contractor at the User's expense for a period of Four months after completion.
- III. The User shall obtain all relevant approvals and/or permits and ensure the Works, including the fenced approaches and walkways, are constructed in accordance with the plans and to the satisfaction of Council.
- IV. The User shall be responsible for the maintenance, repair and drainage of the stock underpass and associated guard rail. The User shall reimburse Council within one month should it incur any costs or expenses in repairing, maintaining or draining the stock underpass or associated infrastructure, should the User fail to do so. Council will not undertake routine inspections of the underpass or associated infrastructure. Council will be responsible for inspecting, maintaining and repairing the road and associated road infrastructure under which the underpass crosses, in accordance with Council's Road Management Plan. Council will accept liability for the cost of repairs attributable to a specially authorised overloaded vehicle travelling over the underpass, prior notification which will be given by Council to the User. The User shall notify Council of any such known damage within 28 days.
- V. The User shall advise any future purchaser of the Land of any part thereof in writing of the existence and contents of this agreement. The User shall advise Council in writing of any such change in tenure of ownership of the Land or any part thereof within 28 days of such change.
- VI. The User shall;

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- A. wholly indemnify and hold harmless Council against any and all demands actions and claims or suits for damages brought against Council by any body or party in respect of the construction, existence and/or use of the stock underpass; and
 - B. protect itself and Council against any legal action which may result from the design construction and/or usage of the stock underpass or its approaches by obtaining an adequate insurance cover by a reputable insurer for an amount agreed to by Council and in default of such an agreement for an insured amount nominated by an independent broker agreed to by the parties, or in default of such agreement an amount nominated by the President for the time being of the Law /Institute of Victoria.
- VII. If Council is of the opinion that the stock underpass is in need of Cleaning or repair for those matters for which the User is responsible a notice may be served on the User directing that such cleaning and/or repairs as specified in the notice shall be carried out by the User within a period of 28 days after service of the notice.
- In default of compliance by the User with such notice Council may carry out the repairs and charge the cost thereof to the user. Such cost may be recoverable in a court of competent jurisdiction.
- VIII. The stock Underpass shall:
- A. only be used for the passage of stock and by persons authorised by the User to assist with the passage of stock for which sole purpose this permission is granted;
 - B. not be used for the housing of animals, vehicles or farm machinery, or for the storage of produce or material at any time.
- IX. No attachments shall be made to any part of the stock underpass without prior approval in writing from Council.
- X. The User acknowledges that its continued use of the stock underpass is subject at all times to compliance by the User with terms of this Agreement. In the event of structural failure or evidence of structural weakness of the underpass, the User shall be responsible for rectifying the faults or removing the underpass and reinstating the road to proper condition. In the event of the road being widened and/or realigned in the future, Council would bear the cost of extending or relocating the underpass.
- XI. No compensation shall be claimed from or paid by Council upon termination of the usage of the stock underpass in accordance with the terms of this agreement by mutual consent of Council and the User.
- XII. The User's rights to use the stock underpass as set out in this agreement shall pass on to its successors in title subject to the provisions of this agreement.
- XIII. This agreement shall be for a period of five years commencing on 1 March 2015 and terminating, subject to any clause contained herein on 1 March 2020..
- XIV. "Provided the User is not in breach of this Agreement, the User may elect by notice in writing to be delivered to Council no less than three months prior to the expiration of the initial term of Agreement and any extension(s) of the term thereof extend the period of this Agreement for a further five years. The length of the Agreement shall therefore be five years with three further option periods of five years each, ie 5x5x5x5. Extension of this agreement pursuant to the exercise of an option period shall be deleted, so that at the end of the first period, there is only a further 5x5x5 years to run, after ten years then a further 5x5, and so forth".
- XV. "A notice (including, without limitation a document) shall be deemed to have been given when it is received by the person to whom it is addressed or is delivered to the address of that person stated in the Agreement or last communicated in writing by that person to the person giving the notice, whichever is the earlier".
- XVI. This Agreement supersedes any and all prior arrangements, agreements or undertakings and shall be binding notwithstanding any prior arrangements, agreements or undertakings made which may conflict

D15/17192

with or be at variance with this agreement or any correspondence or document relating to the subject matter of this agreement which may have passed between parties hereto.

XVII. Continued use of the underpass after the term of agreement will be subject to further agreement at that time. If there is no renewal of the agreement, the User will be responsible for removing the underpass and reinstating the road to proper condition.

IN WITNESS whereof the parties hereto have hereunto set their hands and seals the day and year first set herein before written.

COLAC-OTWAY SHIRE COUNCIL

The Common Seal of the Colac-Otway Shire Council was hereto affixed in accordance with its Local Law No. 4.

.....
Councillor

.....
Councillor

.....
Chief Executive Officer

SIGNED SEALED AND DELIVERED
by the said Users in the presence of


.....

.....


.....
Witness

D15/17192



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**REGISTER SEARCH STATEMENT (Title Search) Transfer of
Land Act 1958**

Page 1 of 1

VOLUME 09085 FOLIO 634

Security no : 124054068265A
Produced 13/02/2015 03:41 pm

LAND DESCRIPTION

Lot 1 on Title Plan 131270A.
PARENT TITLE Volume 07046 Folio 008
Created by instrument F663447 16/04/1975

REGISTERED PROPRIETOR

Estate Fee Simple
Joint Proprietors
JOSHUA JAMES PARSONS
FAITH ELIZABETH PARSONS both of 1780 IRREWILLIPE ROAD IRREWILLIPE VIC 3249
AL460591M 31/10/2014

ENCUMBRANCES, CAVEATS AND NOTICES

MORTGAGE AL460592K 31/10/2014
BENDIGO AND ADELAIDE BANK LTD

Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section 24 Subdivision Act 1988 and any other encumbrances shown or entered on the plan set out under DIAGRAM LOCATION below.

DIAGRAM LOCATION

SEE TP131270A FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

| NUMBER | | STATUS | DATE |
|-----------|-----------------------|------------|------------|
| AL460590P | DISCHARGE OF MORTGAGE | Registered | 31/10/2014 |
| AL460591M | TRANSFER | Registered | 31/10/2014 |
| AL460592K | MORTGAGE | Registered | 31/10/2014 |

-----END OF REGISTER SEARCH STATEMENT-----

Additional information: (not part of the Register Search Statement)

Street Address: 1425 IRREWILLIPE ROAD IRREWILLIPE EAST VIC 3249

DOCUMENT END

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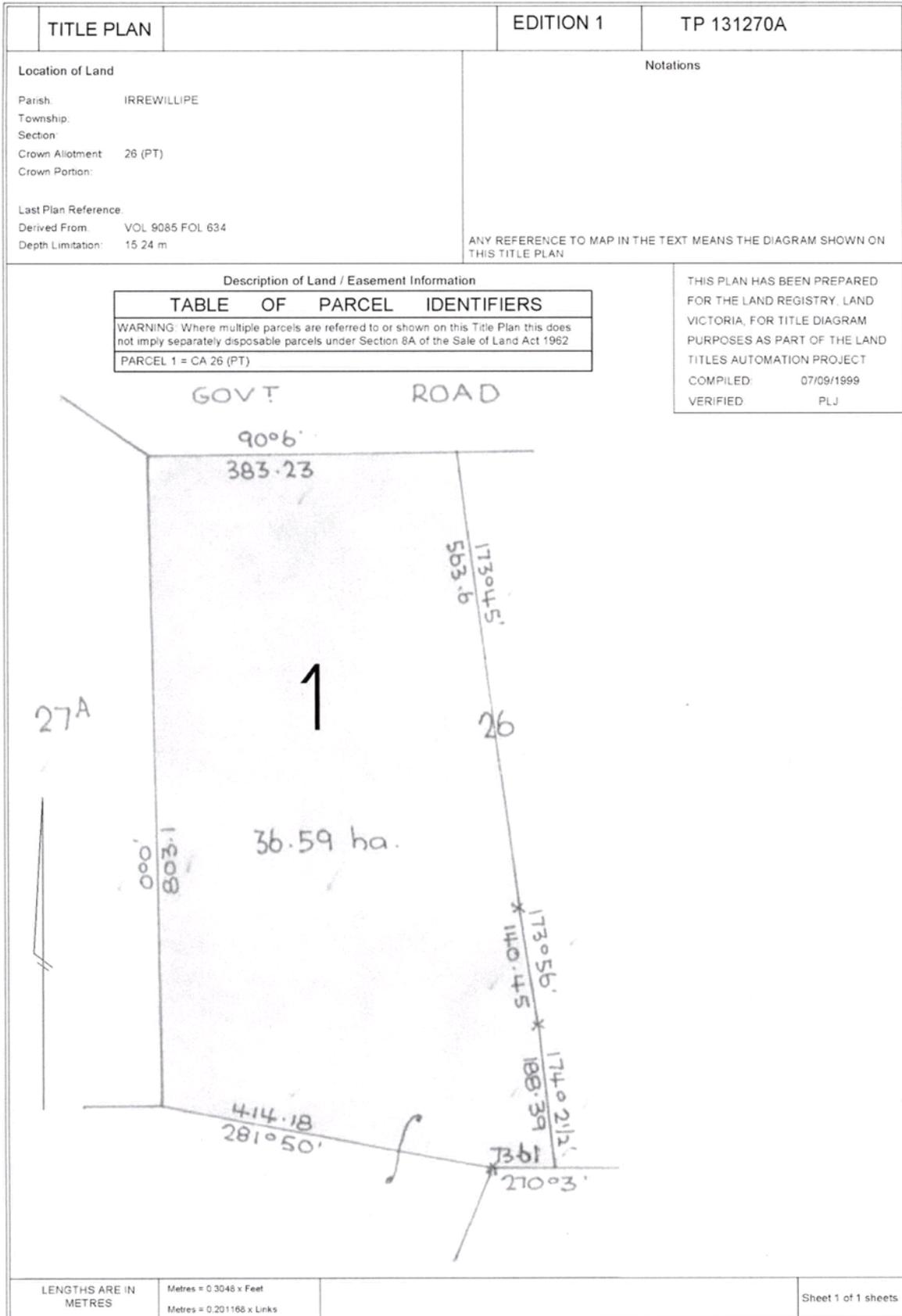
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**REGISTER SEARCH STATEMENT (Title Search) Transfer of
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Page 1 of 1

VOLUME 04382 FOLIO 320

Security no : 124054068364S
Produced 13/02/2015 03:45 pm

LAND DESCRIPTION

Lots 1,2 and 3 on Title Plan 249498K.
PARENT TITLE Volume 00916 Folio 001
Created by instrument 0970448 27/10/1920

REGISTERED PROPRIETOR

Estate Fee Simple
Joint Proprietors

JOSHUA JAMES PARSONS

FAITH ELIZABETH PARSONS both of 345 CARLISLE ROAD IRREWILLIPE EAST VIC 3249
AG676964M 06/08/2009

ENCUMBRANCES, CAVEATS AND NOTICES

MORTGAGE AH132579S 30/03/2010
BENDIGO AND ADELAIDE BANK LTD
TRANSFER OF MORTGAGE AL236362F 17/07/2014

CAVEAT AH018890F 05/02/2010

Caveator

PHILIP LESTER RICHES

Capacity SEE CAVEAT

Lodged by

SLM LAW

Notices to

SLM LAW of 119 MURRAY STREET COLAC VIC 3250

Any encumbrances created by Section 98 Transfer of Land Act 1958 or Section 24 Subdivision Act 1988 and any other encumbrances shown or entered on the plan set out under DIAGRAM LOCATION below.

DIAGRAM LOCATION

SEE TP249498K FOR FURTHER DETAILS AND BOUNDARIES

ACTIVITY IN THE LAST 125 DAYS

NIL

DOCUMENT END

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| TITLE PLAN | EDITION 1 | TP 249498K | | | | | | | | | | |
|---|---|---|-----------------------------|--|---|--|-----------------------|--|------------------------|--|--|--|
| Location of Land
Parish: IRREWILLIPE
Township:
Section:
Crown Allotment: 15(PT), 27A(PT)
Crown Portion:
FORMER GOVERNMENT ROAD (PT)
Last Plan Reference:
Derived From: VOL 4382 FOL 320
Depth Limitation: NIL | | Notations

ANY REFERENCE TO MAP IN THE TEXT MEANS THE DIAGRAM SHOWN ON THIS TITLE PLAN | | | | | | | | | | |
| Description of Land / Easement Information | | THIS PLAN HAS BEEN PREPARED FOR THE LAND REGISTRY, LAND VICTORIA FOR TITLE DIAGRAM PURPOSES AS PART OF THE LAND TITLES AUTOMATION PROJECT
COMPILED: 16/12/1999
VERIFIED: BE | | | | | | | | | | |
| | | | | | | | | | | | | |
| <table border="1" style="margin: auto;"> <thead> <tr> <th colspan="2">TABLE OF PARCEL IDENTIFIERS</th> </tr> </thead> <tbody> <tr> <td colspan="2">WARNING: Where multiple parcels are referred to or shown on this Title Plan this does not imply separately disposable parcels under Section 8A of the Sale of Land Act 1962</td> </tr> <tr> <td colspan="2">PARCEL 1 = CA 15 (PT)</td> </tr> <tr> <td colspan="2">PARCEL 2 = CA 27A (PT)</td> </tr> <tr> <td colspan="2">PARCEL 3 = FORMER GOVERNMENT ROAD (PT)</td> </tr> </tbody> </table> | | | TABLE OF PARCEL IDENTIFIERS | | WARNING: Where multiple parcels are referred to or shown on this Title Plan this does not imply separately disposable parcels under Section 8A of the Sale of Land Act 1962 | | PARCEL 1 = CA 15 (PT) | | PARCEL 2 = CA 27A (PT) | | PARCEL 3 = FORMER GOVERNMENT ROAD (PT) | |
| TABLE OF PARCEL IDENTIFIERS | | | | | | | | | | | | |
| WARNING: Where multiple parcels are referred to or shown on this Title Plan this does not imply separately disposable parcels under Section 8A of the Sale of Land Act 1962 | | | | | | | | | | | | |
| PARCEL 1 = CA 15 (PT) | | | | | | | | | | | | |
| PARCEL 2 = CA 27A (PT) | | | | | | | | | | | | |
| PARCEL 3 = FORMER GOVERNMENT ROAD (PT) | | | | | | | | | | | | |
| LENGTHS ARE IN LINKS | Metres = 0.3048 x Feet
Metres = 0.201168 x Links | Sheet 1 of 1 sheets | | | | | | | | | | |

